

Agenda

Planning Committee

Wednesday, 6 July 2022 at 7.30 pm

New Council Chamber, Town Hall, Reigate



This meeting will take place in the Town Hall, Castlefield Road, Reigate. Members of the public, Officers and Visiting Members may attend remotely or in person.

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Members:

S. Parnall (Chairman)

M. S. Blacker
J. Baker
J. S. Bray
P. Chandler
Z. Cooper
P. Harp
A. King

J. P. King
S. A. Kulka
S. McKenna
R. Michalowski
C. Stevens
D. Torra
S. T. Walsh

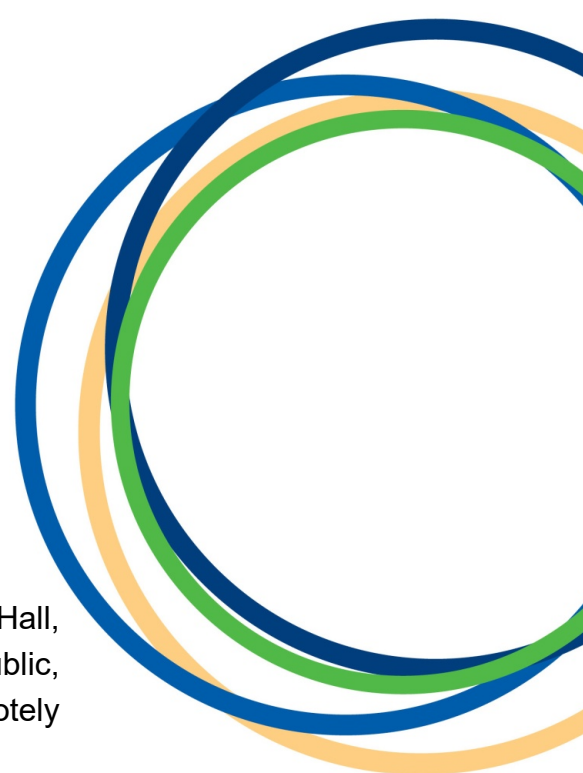
For enquiries regarding this agenda;

Contact: 01737 276182

Email: democratic@reigate-banstead.gov.uk

Published 29 June 2022

Reigate & Banstead
BOROUGH COUNCIL
Banstead | Horley | Redhill | Reigate



Substitutes:

Conservatives:	R. Absalom, H. Avery, J. Hudson, N. C. Moses, M. Tary and R. S. Turner
Residents Group:	G. Adamson and G. Hinton R. Harper and N. D. Harrison
Green Party:	J. Booton, V. Chester, J. C. S. Essex, A. Proudfoot, S. Sinden and R. Ritter
Liberal Democrats	M. Elbourne

Mari Roberts-Wood
Head of Paid Service

1. Minutes (Pages 7 - 12)

To confirm as a correct record the Minutes of the previous meeting.

2. Apologies for absence

To receive any apologies for absence.

3. Declarations of interest

To receive any declarations of interest.

4. Addendum to the agenda (To Be Tabled)

To note the addendum tabled at the meeting which provides an update on the agenda of planning applications before the Committee.

PLANNING APPLICATIONS:

NOTES:

1. The order in which the applications will be considered at the meeting may be subject to change.
2. Plans are reproduced in the agenda for reference purposes only and are not reproduced to scale. Accordingly dimensions should not be taken from these plans and the originals should be viewed for detailed information. Most drawings in the agenda have been scanned, and reproduced smaller than the original, thus affecting image quality.

To consider the following applications :

5. 21/03303/F - Titan House, Crossoak Lane, Salfords (Pages 13 - 62)

The demolition of existing buildings (2) and the erection of two any industrial processes (class e (g) (iii)), general industrial (use class b2) storage and/or distribution (use class b8) units with ancillary office accommodation, together with other associated parking, servicing landscape and infrastructure.

6. 21/03215/F - Redhill Ambulance Station, Pendleton Road, Redhill (Pages 63 - 110)

Demolition of existing ambulance station and ancillary buildings, construction of 8 dwelling houses with associated access and parking. As amended on 31/01/2022 and on 30/05/2022.

7. 22/00181/F - Oakwood Sports Centre, Balcombe Road, Horley (Pages 111 - 138)

Creation of a Community 3G Football Turf Pitch (FTP) (11020 sq. metres) with associated features including: 3G football turf pitch (7460 sq. metres); 4.5m high ball stop fencing with entrance gates to form an enclosure around FTP perimeter; 1.2m high and 2.0m high pitch barriers with entrance gates internally within fenced FTP enclosure; 2.6m high maintenance equipment storage container (15 sq. metres) within fenced FTP enclosure; 15.0m high LED floodlights (6no.) around FTP perimeter; 4.0m high LED amenity light (1no.) along pedestrian access; Hard-standing areas for pedestrian access and circulation, portable goals storage, as well as vehicular access (807 sq. metres); 0.75m high grass flood defence crest around FTP perimeter (formed with recycled soils from the FTP construction (2753 sq. metres). (No change of use.) As amended on 21/04/2022 and on 13/05/2022.

8. 21/02108/F - 64 & rear of 62 Shelveys Way, Tadworth (Pages 139 - 178)

Demolition of 64 Shelveys Way and the erection of 3 x 4 bedroom dwellings and 1 x 3 bedroom dwelling. As amended on 08/02/2022 and on 23/05/2022.

9. 21/00429/CU - Land and City Families Trust, Old Pheasantry, Merrywood Grove, Lower Kingswood (Pages 179 - 216)

Change of use of part of the building to a school.

10. 22/00939/F - 103B High Street, Banstead (Pages 217 - 236)

Extension of first floor at rear to form 2 self-contained units of accommodation.

11. 21/03311/F - Alvis House, Park Road, Banstead (Pages 237 - 282)

A change of use of land to class c3, the removal of the existing areas of hardstanding, retention and restoration of bunker 4, the demolition of the remaining structures, and redevelopment to provide ten detached dwellings accessed via an internal circuit road framing a central water body. To include associated works for the purpose of landscaping. As amended on 25/03/2022 and on 12/04/2022.

12. 22/00557/F - 80 Croydon Road, Reigate (Pages 283 - 310)

Demolition of existing single-storey permanent structures (used as garages and storage) and the erection of 2No. self-built semi-detached 3-bedroom family dwellings with associated access, external amenity spaces, refuse storage and car and cycle parking.

13. Any other urgent business

To consider any item(s) which, in the opinion of the Chairman, should be considered as a matter of urgency.



Our meetings

As we would all appreciate, our meetings will be conducted in a spirit of mutual respect and trust, working together for the benefit of our Community and the Council, and in accordance with our Member Code of Conduct. Courtesy will be shown to all those taking part.



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Notice is given of the intention to hold any part of this meeting in private for consideration of any reports containing "exempt" information, which will be marked accordingly.

BOROUGH OF REIGATE AND BANSTEAD

PLANNING COMMITTEE

Minutes of a meeting of the Planning Committee held at the New Council Chamber - Town Hall, Reigate on 8 June 2022 at 7.30 pm.

Present: Councillors J. Baker, M. S. Blacker, P. Chandler, Z. Cooper, P. Harp, A. King, J. P. King, S. A. Kulka, S. McKenna, R. Michalowski, S. Parnall, C. Stevens, D. Torra, S. T. Walsh and N. D. Harrison (Substitute).

1. ELECTION OF CHAIRMAN

RESOLVED that Councillor Parnall; having been proposed by Councillor Walsh, and seconded by Councillor Blacker, be elected as Chairman of the Planning Committee for the 2022-23 municipal year.

2. ELECTION OF VICE-CHAIRMAN

RESOLVED that Councillor Blacker; having been proposed by Councillor Michalowski, and seconded by Councillor J King, be elected as Vice-Chairman of the Planning Committee for the 2022-23 municipal year.

3. MINUTES

RESOLVED that the minutes of the previous meeting held on 27 April 2022 be approved as a correct record.

4. APOLOGIES FOR ABSENCE

An apology for absence had been received from Councillor Bray, Councillor Harrison attended as her substitute.

5. DECLARATIONS OF INTEREST

There were no declarations of interest.

6. ADDENDUM TO THE AGENDA

RESOLVED that the addendum be noted.

7. 21/03303/F - TITAN HOUSE, CROSSEAK LANE, SALFORDS

The Committee considered an application at Titan House, Crossoak Lane, Salfords for the demolition of existing buildings (2) and the erection of two any industrial processes (class e (g) (iii)), general industrial (use class b2) storage and/or distribution (use class b8) units with ancillary office accommodation, together with other associated parking, servicing landscape and infrastructure.

Jim Blackmore, Vice-Chair at Salfords and Sidlow Parish Council, spoke in objection to the application stating that the Parish Council had met with the developers however they were unable to concur on matters. Their main concerns were the impact on residents in Empire Villas and traffic. The site was on the border of Horley and Salfords and Sidlow and both councils had objected to this proposal. The site was allocated for business use however they questioned the scale of

Agenda Item 1

Planning Committee
8 June 2022

Minutes

development and whether it was needed or appropriate considering the recently developed North Gatwick Gateway site which was situated opposite. The scale of the development was not in keeping with the area. Residents in Empire Villas would be affected by the size and scale of the development, notably their severe reduction in light compared to the existing site layout and it was felt that these residents would suffer a serious detrimental impact. Traffic was currently over capacity at the A23/Cross Oak Lane junction and the lanes to the east of the site, the NPPF was quoted. When the already permitted Westvale Park housing was to be completed this would exacerbate the situation. The type of traffic using this site would be more HGV based. The impact on the road network would be severe and the following condition was suggested should the Committee approve the application. "To require regulation of site traffic being prevented from using the access routes through The Acres and Langshott developments and the country lanes in the wider Salfords and Sidlow area."

Paul Shuker, the Agent, spoke in support of the application, stating the applicant has worked collaboratively with key officers, statutory consultees, and local residents. The project would deliver a sustainable economic led repurposing of an existing allocated site and would continue and retain an economic base at this allocation. The applicant fully endorses the case officer's report and its conclusions in that it fully satisfies the principal planning policies in the local plan and the NPPF. The continued concerns were acknowledged, however they had been addressed through redesign and re-consultations. These have all been validated independently by statutory consultees and technical experts in their respected fields. The issue of sunlight and daylight had also been considered. Current proposals had been reduced by 10% from that originally submitted, despite the larger scheme still satisfying BRE guidance. The proposals have been benchmarked against other employment schemes in the defined employment area and were found to be consistent in scale, character and setting. The negligible amenity impact is further reduced by the retention of existing green infrastructure around the perimeter of site. The design sought to retain existing embedded bio-diversity. At the same time this green infrastructure retains the local character of Bonehurst Road. Surrey County Council independently reviewed the evidence and found the scheme to be acceptable on highway policy grounds. The proposal fully satisfied Policy TAP1 of the Local Plan. The economic benefits to the borough were outlined.

Following a vote, it was **RESOLVED** that the application be deferred to the next meeting of the Committee so that reasons for refusal can be considered.

8. **21/03185/F - OLDBURY ENGINEERING LTD, 8 - 12 BALCOMBE ROAD, HORLEY**

The Committee considered an application at Oldbury Engineering Ltd, 8 - 12 Balcombe Road, Horley for the Demolition of all existing building and erection of a detached building containing 6 apartments with associated access, parking for car and cycles, refuse storage and amenity space. As amended on 22/02/2022 and on 16/05/2022.

Reasons for refusal were proposed by Councillor Stevens and seconded by Councillor Baker, whereupon the Committee voted and **RESOLVED** that planning permission be **REFUSED** on the grounds that:

1. The proposed development by virtue of its modern flat roofed design, high eaves adjacent to its neighbour and second floor bulk would appear jarring and incongruous with the adjacent properties and fail to reflect the local distinctiveness of the area, harmful to its character. This would be contrary to Policy DES1 of the Development Management Plan 2019 and advice contained within the Local Distinctiveness Design Guide SPD 2021 and National Planning Policy Framework.
2. The proposed development by virtue of the small private and communal outdoor areas would fail to provide adequate amenity space for its occupants thereby failing to achieve acceptable quality living conditions contrary to policy DES5 of the Development Management Plan 2019 and advice contained within the National Planning policy Framework.
3. The proposed development fails to provide sufficient parking to meet the parking standards set out in Annex 4 of the Development Management Plan which, without any evidence to the contrary would lead to the increased potential for on-street parking in areas where demand for spaces is high, leading to amenity issues for existing and potential residents contrary to Policies DES1 and TAP1 of the Development Management Plan 2019.

9. 21/02438/F - SALFORDS VILLAGE STORE, 21 BRIGHTON ROAD, SALFORDS

The Committee considered an application at Salfords Village Store, 21 Brighton Road, Salfords for the Demolition of existing convenience store building and redevelopment involving the erection of a convenience store (Class E) with associated parking and landscaping. As amended on 15/03/2022, 16/03/2022 and on 28/04/2022.

Jim Blackmore, Vice-Chair of Salfords and Sidlow Parish Council, spoke in objection to the application, raising concerns regarding the suitability of the site for a busy convenience store; highway safety in relation to vehicle movements at this busy junction associated with deliveries; the accident record in proximity to site and noise and disturbance to neighbours associated with deliveries and late night shoppers. It was explained that there was little onsite parking provided and parking in Honeycrook Lane was restricted by yellow lines and dropped kerbs. The plans showed that there were traffic islands in the middle of the A23 Brighton Road and Honeycrook Lane both of which could restrict the traffic going into and out of the site, in particular HGVs. The swept path analyses in the Mayer Brown Stage 1 Road Safety Audit showed the difficulty long and/or multi-axle lorries would have getting into and out of the site. This was a significant matter at this junction, which has a long history of road accidents. This analysis was not shown in the committee report. The report did not recognise the need to avoid noise nuisance to the neighbouring residential properties and only comments on internal plant at the south of the building. It was felt that there should be a condition that no noisy machinery be located near the neighbouring residential properties or it must have adequate sound attenuation. With deliveries arriving any time between 6am and 10 pm, delivery vehicles should be required to enter and leave in forward gear and avoid reversing warning sounders. Whenever possible the engines of the delivery vehicles should be switched off.

Agenda Item 1

Planning Committee
8 June 2022

Minutes

Sarah Isherwood, the Agent, spoke in support of the application stating the site currently contained a dilapidated convenience store which had been vacant for some time. It had been vandalised and was detracting from the character and appearance of the surrounding area. The proposal sought to bring the site back into use as a new store to serve the local community which was accessible on foot and would reduce the need to travel, improving the sustainability of the area for existing and future residents. This point was supported by some local residents and there was support relating to job creation and economic growth. Concerns were raised regarding highway safety however following discussions with Surrey County Council Highways team, amendments were made to the proposals including the submission of a road safety audit to demonstrate that the proposals would not have an impact on highway safety. An additional planning condition was proposed to secure a Delivery and Servicing Management Plan which would include specific details on the management of the delivery bay. This condition ensured the Local Planning Authority could assess and approve the details around how the Co-Op would operate the site based on the principles set out within the application. Subject to the conditions set out, Surrey County Council Highways did not raise any objection to the proposals. The proposals brought back into use a brownfield site within the local centre to provide a new Co-Op store to support the local community where there was currently no convenience store in easy walking distance. The development would improve the character and appearance of the area by providing a replacement building of an appropriate scale and design. No objections were raised by the Council's technical consultees.

RESOLVED that planning permission be **GRANTED** subject to conditions as per the recommendation.

Officers to consider the need for asbestos conditions in future given the doubling of legislation, an informative was preferred.

10. FIRST HOMES INTERIM POLICY STATEMENT

The Head of Planning explained that this item had been deferred from the meeting held on 6 April 2022 in order to seek clarification on whether it should be considered initially by the Overview and Scrutiny Committee. Following advice from the Monitoring Officer, it was concurred that this was a matter for the Planning Committee.

An overview of the First Homes Interim Policy Statement was given, and it was explained that this was a Government policy, which as a local authority, Reigate and Banstead Borough Council were required to fulfil. The policy explained how affordable housing was to be introduced via the First Homes scheme.

The scheme would offer first time buyers (with associated criteria attached) the opportunity to purchase homes at a 30% discount. The 30% discount would remain in place for a property in the scheme for perpetuity. Local authorities could choose to apply a discount that was greater than 30% if it wished, however this would take away funding for other affordable homes that could be offered, and this authority did not wish to pursue this option.

The capped price for a home in the First Homes scheme would be £250,000 and this would apply to 1 and 2 bed properties. In terms of criteria of buyers, the Council

could set its own local connections policy, as well as an Armed Forces personnel connection. Other criteria were also set out.

It was questioned as to how future sales would be managed in light of the 30% discount remaining for perpetuity. It was explained that the Council's housing officers would work to manage subsequent sales and purchases. It was acknowledged that the Council would incur costs for undertaking this and this would take up officer time.

In respect of the item not being considered by the Overview and Scrutiny Committee, it was explained that this was not within the scope for that Committee.

It was requested that a report be brought back by officers, to this Committee, to consider the Policy's implementation within the next year.

RESOLVED that:

- I. Note the requirements of this new national planning housing policy, and the need to apply it in the borough as set out in this Interim Policy Statement;
- II. Note the recommended local eligibility criteria;
- III. Grant delegated authority to the relevant Heads of Service alongside portfolio holders to amend this Interim Policy Statement as required to reflect lessons learnt through its implementation; and
- IV. Officers to bring report back to Committee within a year to review implementation.

11. ANY OTHER URGENT BUSINESS

There was none.


The Meeting closed at 10.28 pm

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Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	06 July 2022
	REPORT OF:	HEAD OF PLANNING
	AUTHOR:	Michael Parker
	TELEPHONE:	01737 276339
	EMAIL:	Michael.parker@reigate-banstead.gov.uk
AGENDA ITEM:		WARD: Horley East And Salfords

APPLICATION NUMBER:	21/03303/F	VALID:	17/01/2022
APPLICANT:	F74 D Chateau S.A.R.L C/O Salmon Property Horley L	AGENT:	Lambert Smith Hampton
LOCATION:	TITAN HOUSE CROSSOAK LANE SALFORDS SURREY RH1 5EX		
DESCRIPTION:	The demolition of existing buildings (2) and the erection of two any industrial processes (class e (g) (iii)), general industrial (use class b2) storage and/or distribution (use class b8) units with ancillary office accommodation, together with other associated parking, servicing landscape and infrastructure.'		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

This application was deferred from the Planning Committee meeting of 8 June. The item was deferred as the motion to grant was not carried but there were no reasons for refusal put forward at the meeting.

Since that meeting the applicant has provided a sunlight and daylight report, a highway note on the no turn left condition (condition 25) and 5 amended plans showing a further reduction in height of the building.

Daylight and Sunlight Report:

The report considers two scenarios, 1 – excluding evergreen hedge along northern boundary and 2 – including evergreen hedge along northern boundary.

In scenario 1 - the Vertical Sky Component (VSC), records 91% BRE compliance and is considered very good for the area. Windows that demonstrate transgressions beyond the BRE Guidelines are considered remote and the majority record very good levels of daylight availability and No-Sky Line (NSL). In terms of sunlight Annual Probable Sunlight Hours (APSH) records 97% BRE compliance. The

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

isolated transgression records annual sunlight levels far exceeding the BRE's 25% target value.

In scenario 2 – the Vertical Sky Component (VSC) results record full BRE compliance (100%), commensurate with the BRE Guidelines, The NSL daylight assessment records 98% BRE compliance. The single transgression is considered isolated and localised. The sunlight assessment, the APSH, records full BRE compliance (100%). Therefore officers are satisfied that this report further demonstrates that the impact to the neighbouring residents will be acceptable.

Amended plans:

Amended plans have been provided which show a further 0.5 metres reduction in the height of the buildings. The height of the parapet has been reduced from 13.7m to 13.2m and the eaves along the norther boundary have been further reduced from 12.65 to 12.15m. This has further reduced the bulk of the building along the northern boundary and as is clearly shown on the updated cross section drawings the proposal now comfortably passes both the 25 and 45 degree test. These changes further reduce any potential impact on the neighbouring properties and combined with the Daylight and Sunlight Report demonstrates that the proposed development would have an acceptable relationship with the neighbouring properties to the north of the site.

Transport note:

This report re-emphasises that the proposal would result in a reduction in vehicle traffic accessing site and travelling on the local highway network when compared to the existing use of the site. As a result of the development there would be 105 fewer movements in the AM peak hour, 114 fewer movements in the PM peak hour and 378 movements over the daily period. This analysis and impact was agreed with the Highway Authority. The moving of the access further along Cross Oak Lane also results in an improvement to traffic flow due to reduced likelihood of junction being blocked and potential accidents or delays. Lastly the report states that the imposition of the condition so as to not allow such vehicles to turn left out of the access would not change the peak hour traffic data analysis – because it was not recorded and not expected in the future that HGVs would come from the east down Cross Oaks Lane. From officer point of view the requirement of a condition was requested by SCC on highway safety grounds. They are the technical statutory consultees and it has been demonstrated that this condition would have no impact. Officers do note that concern was raised regarding the routing of heavy vehicles not just to the east of the site but also other narrow country lanes in the area and residential estates such as The Acres and Westvale. Officers have therefore recommended to update condition 25 to ensure that the subsequent routing strategy also ensures that other inappropriate routes are avoided.

These changes and additional information, in the view of officers, further strengthen officer's position that this scheme is entirely acceptable and there are no substantive reasons as to why this application should be refused.

Members should also note the significant economic benefits this proposal will bring to the Borough. There is a whole section in the submitted Planning Statement (Section 7) that deals with this. In short the scheme is designed to meet latent

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

demand for E (g) (iii), B2 and B8 uses in the Coast to Capital economic area and is a response to known demand for high quality space that is well connected to Gatwick Airport and the existing infrastructure in Gatwick Diamond. The scheme would create (blended) around 160 direct jobs during operation as well as provide local job opportunities during the demolition and construction phases. The Planning Statement also emphasises that significant weight needs to be placed on paragraph 81 of the NPPF. Paragraph 81 of the NPPF states that:

“Planning policies and decisions should help create the conditions in which businesses can invest, expand and adapt. Significant weight should be placed on the need to support economic growth and productivity, taking into account both local business needs and wider opportunities for development. The approach taken should allow each area to build on its strengths, counter any weaknesses and address the challenges of the future. This is particularly important where Britain can be a global leader in driving innovation, and in areas with high levels of productivity, which should be able to capitalise on their performance and potential.”

The applicant also notes that “This is further supported given that this is an allocated site and was acquired by the applicant on the basis of the favourable planning policy position of the subject site and the desire by the Capital to Coast LEP to encourage new employment in this key growth hub around Gatwick and the wider economic triangle.”


The economic benefits of the scheme are not set out in detail within the committee report because officers found the scheme to be entirely policy compliant and within a site which is designated for such types of developments and therefore it wasn't considered necessary. However members are reminded of the need to take in to account the economic benefits identified by the applicant in their decision making.

The previous Officer report is set out below and the recommendation remains that planning permission should be granted with conditions subject to the completion of a S106. The changes to the original report set out in the addendum for the June meeting and changes to the conditions/drawings discussed above are shown in *italics* in the below report.

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	08 June 2022
	REPORT OF:	HEAD OF PLANNING
	AUTHOR:	Michael Parker
	TELEPHONE:	01737 276339
	EMAIL:	Michael.parker@reigate-banstead.gov.uk
AGENDA ITEM:	7	WARD: Horley East And Salfords

APPLICATION NUMBER:	21/03303/F	VALID:	17/01/2022
APPLICANT:	F74 D Chateau S.A.R.L C/O Salmon Property Horley L	AGENT:	Lambert Smith Hampton
LOCATION:	TITAN HOUSE CROSSOAK LANE SALFORDS SURREY RH1 5EX		
DESCRIPTION:	The demolition of existing buildings (2) and the erection of two any industrial processes (class e (g) (iii)), general industrial (use class b2) storage and/or distribution (use class b8) units with ancillary office accommodation, together with other associated parking, servicing landscape and infrastructure.'		
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SUMMARY

The application seeks full permission for the demolition of existing buildings (2) and the erection of two any industrial processes (class e (g) (iii)), general industrial (use class b2) storage and/or distribution (use class b8) units with ancillary office accommodation, together with other associated parking, servicing landscape and infrastructure.

There is no in principle objection to the scheme. The site is located within the urban area, the site is not statutory or locally listed and is in a designated a Principal Employment Area.

The site is located on the eastern side of the A23 and at the southern end of the urban area of Salfords. The site was previously occupied by Titan Travel. To the east of the site is the railway line. To the north of the site are residential properties accessed along Empire Villas. The site accesses onto Cross Oak Lane, close to the signalised junction with the A23. To the east, is a restricted height bridge which runs beneath the railway line. There are trees and boundary vegetation on most

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

boundaries of the site, notably the boundaries with the A23, with the existing buildings set well back from the A23 and Cross Oak Lane and Empire Villas and this is a feature which prevails along much of this stretch of the A23

It is accepted that the scheme would result in a significant change to the existing character and nature of the site however, overall, it is considered that the proposal achieves a good standard of design and a development which is in keeping with the scale and character of surrounding industrial developments and would not have a detrimental impact on the character of the area.

Whilst neighbouring properties would experience a significant change as a result of the development, the proposals would not give rise to a serious detriment to their living conditions and thus comply with policy DES1 of the DMP and the general provisions of the NPPF (para 127) which seeks to ensure that developments provide a high standard of amenity for existing and future occupants.

Subject to conditions the proposal is considered to be acceptable with regard to highway safety and capacity, parking, trees, ecology, Gatwick safeguarding, sustainable construction, crime and flooding/drainage.

This proposal is therefore considered to be acceptable in planning terms so is recommended for approval.

RECOMMENDATION(S)

Planning permission is **GRANTED** subject to conditions.

Subject to the completion of all documentation required to create a planning obligation under Section 106 of the Town and Country Planning Act 1990 (as amended to secure:

- (i) Contribution of £6150 towards auditing of the travel plan
- (ii) The Council's legal costs in preparing the agreement

Planning permission is **GRANTED** subject to conditions.

In the event that a satisfactorily completed obligation is not received by 8 November 2022 or such longer period as may be agreed, the Head of Places and Planning be authorised to refuse permission for the following reason

1. Without a completed planning obligation the proposal fails to provide adequate contribution towards auditing of the Travel Plan and is therefore contrary to the National Planning Policy Framework 2021 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

Consultations:

Aerodrome Safeguarding (Gatwick Airport): no objection subject to conditions

Environment Agency: no objection

Environmental Health (contamination): no objection subject to conditions

Environmental Health (air quality): no objection subject to adequate dust management

Highway Authority: The County Highway Authority has assessed the application on safety, capacity and policy grounds and has raised no objection subject to conditions and £6150 contribution towards auditing of Travel Plan.

Horley Town Council: objects

i) The current transport infrastructure will not support the proposal both at Cross Oak Lane and at the junction of Cross Oak Lane with the A23 especially when taking into account that the Westvale Park development access road joins into this junction;

ii) Prior to any approval, a thorough traffic survey along the A23 and its junctions, from the Chequers roundabout in Horley to the junction with Three Arch Road in Earlswood, be carried out and the results established;

iii) Should the Local Planning Authority be minded to approve the application, HTC request a condition be imposed to require traffic regulation of site traffic being prevented from using access routes through the Acres and Langshott developments and country lanes in the wider Salfords and Sidlow Parish Council area.

Network Rail: no objection from planning perspective

Regulatory Support Services: no objection subject to conditions

Reigate Society: concerns regarding the impact on residential properties along Empire Villas and impact on traffic at Cross Oak Lane/Bonehurst Road junction

Salfords and Sidlow Parish Council: concerns regarding impact on residents in Empire Villas in terms of light, outlook and privacy and impact from plant/pollution and operations of the site. Concerns about the B8 use proposed in terms of type of employment provided. Concerns about impact on local road network.

Surrey County Council Minerals and Waste: no objection

Surrey County Council Lead Local Flood Authority:

Satisfied that the proposed drainage scheme meets the national guidance and technical standards. Condition recommended to secure further finalised details of drainage strategy and implementation of drainage strategy.

Surrey Wildlife Trust:

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

No objection subject to conditions securing mitigation measures

Thames Water: no objection in relation to waste water network and sewage treatment works infrastructure capacity. A number of informatives recommended.

Representations:

31 responses, 30 objections and 1 representation, have been received raising the following issues:

Issue	Response
Alternative location/ proposal preferred	See paragraph 6.2-6.4
Crime fears	See paragraph 6.42-6.46
Drainage/sewage capacity	See paragraph 6.39-6.41
Harm to Conservation Area	The site is not within a Conservation Area
Harm to Green Belt/countryside	The site is within the designated urban area
No need for the development	See paragraph 6.2-6.4
Harm to wildlife habitat	See paragraph 6.34-6.36
Health fears	See paragraph 6.15-6.25
Inconvenience during construction	See paragraph 6.15-6.25
Inadequate parking	See paragraph 6.26-6.30
Increase in traffic and congestion	See paragraph 6.26-6.30
Hazard to highway safety	See paragraph 6.26-6.30
Overdevelopment	See paragraph 6.5-6.14
Out of character with surrounding area	See paragraph 6.5-6.14
Poor design	See paragraph 6.5-6.14
Loss of buildings	See paragraph 6.2-6.4
Noise & disturbance	See paragraph 6.15-6.25
Loss of/harm to trees	See paragraph 6.31-6.33
Overlooking and loss of privacy	See paragraph 6.15-6.25
Loss of private view	Not a material planning consideration
Overshadowing	See paragraph 6.15-6.25
Overbearing relationship	See paragraph 6.15-6.25
Concerns about subsidence damage from construction	This is not a material planning consideration but a matter

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

	between the applicant and neighbouring properties. The proposal would require building regulations approval
Property devalue	Not a material planning consideration
Concern about the retention of the Philips Research Laboratories Plaque	See paragraph 6.51

1.0 Site and Character Appraisal

- 1.1 The site is located on the eastern side of the A23 and at the southern end of the urban area of Salfords. The site was previously occupied by Titan Travel. To the east of the site is the railway line. To the north of the site are residential properties accessed along Empire Villas. The site accesses onto Cross Oak Lane, close to the signalised junction with the A23. To the east, is a restricted height bridge which runs beneath the railway line. There are trees and boundary vegetation on most boundaries of the site, notably the boundaries with the A23, with the existing buildings set well back from the A23 and Cross Oak Lane and Empire Villas and this is a feature which prevails along much of this stretch of the A23.
- 1.2 The site is designated as a Principal Employment Area – Salfords, it is also within the designated urban area and the site adjoins Flood Zone 2 and 3 to the south of the site.

2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: A pre-application submission was made to the Council regarding this development (PAM/21/00190). This has given the Council the opportunity to set out the key areas of consideration and areas where further information would be required and also to highlight areas of concerns.
- 2.2 Improvements secured during the course of the application: Additional contamination information, tree reports, ecology information, additional information regarding highway/parking concerns and amended plans lowering parapet on northern elevation. Details of location of temporary school also provided.
- 2.3 Further improvements to be secured through conditions or legal agreement: Further details of Materials, CTMP, CMS, Travel Plan, Ecology, Trees and landscaping, noise and use, sustainable construction.

3.0 Relevant Planning and Enforcement History

- | | | | |
|-----|--------------|---|---------|
| 3.1 | 07/01810/OUT | Outline planning application for the phased redevelopment of the site to form an office | Granted |
|-----|--------------|---|---------|

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

campus for Titan Travel comprising 2 x 2 storey, 2 x 3 storey and 1 x 4 storey building, associated parking and landscaping. Additional/amended plans received on 13/11/2007 showing parking and landscaping. (Drwg No. 210 Rev C, 203 (Additional Plan), SUDS Assessment (received 13.11.07)) Additional/Amended plans received on 15/04/2008 to show Site plans, elevations, phasing and demolition plans (Drwg no (Amended plans 884 200C, 884 201C, 884 202, 884 203B, 884 210G, 884 211C, 884 212D, 884 220D, 884 222C, 884 230D, 884 232D) (Additional plans MV01, MV02, MV03, MV04)) Amended plans received on 12/05/2008 to show Amended site layout plan (Drwg no 210 H).

- | | | | |
|-----|--------------|---|--------------|
| 3.2 | 22/00486/DED | Demolition of titan house, gate house, electricity house. As amended on 16/03/2022, 23/03/2022 and on 06/04/2022. | No objection |
|-----|--------------|---|--------------|

Site to south of Cross Oak Lane - Goya Developments Former Philips Research Laboratories South Site Crossoak Lane Salfords Surrey RH1 5HA:

- | | | | |
|-----|--------------|--|---------|
| 3.3 | 18/01180/F | The redevelopment of the site to include four employment buildings incorporating 5 units for open B1(b), B1(c), B2 and B8 use comprising 15,831sqm GEA with associated parking and landscape planting. As amended on 30/07/2018, 15/08/2018 and on 04/10/2018. As amended on 19/10/2018 and on 23/10/2018. | Granted |
| 3.4 | 19/01370/S73 | The redevelopment of the site to include four employment buildings incorporating 5 units for open B1(b), B1(c), B2 and B8 use comprising 15,831sqm GEA with associated parking and landscape planting. - Application to remove condition 23 of permission 18/01180/F which restricts the amount of B8 floorspace. As amended on 06/09/2019 | Granted |
| 3.5 | 19/02199/S73 | The redevelopment of the site to include four employment buildings incorporating 5 units for open B1(b), B1(c), B2 and B8 use comprising 15,831sqm GEA with associated parking and landscape planting. - Application to remove condition 23 of | Granted |

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

permission 18/01180/F which restricts the amount of B8 floorspace. Variation of conditions 1, 7, 11 and 15 of permission 19/01370/S73. Amendment to approved plans.

4.0 Proposal and design approach

- 4.1 This is a full application seeking permission for the demolition of existing buildings and the erection of two industrial processes (class e (g) (iii)), general industrial (use class b2) storage and/or distribution (use class b8) units with ancillary office accommodation, together with other associated parking, servicing landscape and infrastructure.'
- 4.2 The proposed development comprises
- 8,152.3 sq.m (GEA) across two (2) industrial process, general industrial and/or storage and distribution units, these units (A and B) would be positioned at the northern end of the site and would spread across the majority of the width of the site;
 - The proposal includes 892 sq.m (GIA) ancillary office in mezzanines (12% of space) across the two units and contained in two storeys;
 - Unit A will provide two loading bays with 3 dock levellers, Unit B will provide 2 loading bays
 - Core parking at 88 staff and visitor car parking spaces located to the south of units A and B, with 28 additional flexible shared parking) taking total to 119 spaces if required;
 - 5 (or 6%) accessible car parking spaces;
 - 5% (or 4 double posts, 2 per unit) electric vehicle charging points (EVCP) with the car parking areas with the remainder of the staff car parking prepared with duct work for future EVCP upgrade;
 - Policy compliant secure cycle spaces (20 spaces);
 - 11 individual trees are to be removed (with 1 small tree in the groups) of 42 existing individual trees (4 are advised to be removed regardless of development – Category U), and replaced with 37 native trees and native ornamental (21 extra heavy standard, 5 conifer/pines and 6 x 1+1 transplants) that will be replanted in the new remodelling landscaping areas (this replaces 11+1 trees to be removed) this represent above a 3:1 ratio and results in an increase level of native trees on site;
 - The proposed tree planting will result in a 60% increase in overall tree coverage from 42 mixed species trees to 67 native and native ornamental trees;
 - Delivering minimum BREEAM Very Good rated buildings;
 - Retention and reposition of the 'Phillips Laboratories' Blue Plaque celebrating the world leading research and development that took place at the site post war; and
 - Photovoltaic solar panels are proposed on the roofs of the two units and the level of provision will exceed Part L of the Building Regulations and the Energy Strategy.

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

- 4.3 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:
Assessment;
Involvement;
Evaluation; and
Design.
- 4.4 Evidence of the applicant's design approach is set out below:

Assessment	Section 2.0, 2.1 and 3.0 set out the context of the location, existing site and setting of the site.
Involvement	The design and access statement is silent on this matter but the Planning Statement at para. 6.63 and 6.64 and submitted Statement of Community Involvement (December 2021) outline the consultation process with identified stakeholders and the outcome of the process.
Evaluation	The proposed design has been led by the needs of the applicant, pre-application discussion, advice from the tree officer and stakeholders such as commercial property agents
Design	<p>Section 4.0 to 10.0 set out the design proposals. The site layout has been developed to respond to the site constraints and the practical considerations of a development of this nature. The sizes and arrangements of the units have been developed with the design team and commercial property agents to meet local demands and prospective tenant's requirements.</p> <p>Great care has been taken in positioning the building to best screen the residential properties to the north from the site activities and light spillage. The building frontages face onto Cross Oak Lane making them clearly visible for the access road. The office accommodation provides feature on the frontage and, as they face south, allows for good daylight and sunlight to the office areas.</p> <p>The elevations submitted with this application indicate general appearance of the proposed building. The forms are simple and well-proportioned suited to buildings of this nature, with clear internal volumes to the warehouses and three storey offices to the front keeping the warehouses free of any intrusions.</p>

- 4.5 Further details of the development are as follows:

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

Site area	1.64 hectares
Existing use	Office and general industry buildings
Proposed use	Class E (g) iii, B2 B8
Existing parking spaces	168 cars spaces, 68 Light goods vehicles / public carrier vehicles
Proposed parking spaces	88 car parking space (with potential for additional 31 spaces – total 119), 10 HGV loading/parking spaces.
Existing floor area	4786 m2
Proposed floor area	7805 m2 (net gain 3142m2)

5.0 Policy Context

5.1 Designation

Urban Area

Principal Employment Area - Salfords

Flood Zone 2 and 3 - to south of site

5.2 Reigate and Banstead Core Strategy

CS1 (Sustainable Development)

CS2 (Valued landscapes and the natural environment)

CS5 (Valued people and economic development)

CS10 (Sustainable Development)

CS11 (Sustainable Construction)

CS12 (Infrastructure Delivery)

CS17 (Travel options and accessibility)

5.3 Reigate and Banstead Local Plan: Development Management Plan

EMP4 (safeguarding employment land and premises),

EMP5 Local Skills and training opportunities),

DES1 (Design of new development),

DES8 (Construction Management),

DES9 (Pollution and contaminated land),

TAP1 (Access, parking and servicing),

CCF1 (Climate change mitigation),

CCF2 (Flood Risk),

NHE2 (Protecting and enhancing biodiversity and areas of geological importance),

NHE3 (Protecting trees, woodland areas and natural habitats),

INF3 (Electronic communication networks),

EMP4 (safeguarding employment land and premises),

EMP5 Local Skills and training opportunities),

DES1 (Design of new development),

DES8 (Construction Management),
DES9 (Pollution and contaminated land),
TAP1 (Access, parking and servicing),
CCF1 (Climate change mitigation),
CCF2 (Flood Risk),
NHE2 (Protecting and enhancing biodiversity and areas of geological importance),
NHE3 (Protecting trees, woodland areas and natural habitats),
INF3 (Electronic communication networks)

5.4 Other Material Considerations

National Planning Policy Framework

National Planning Practice Guidance

Supplementary Planning Guidance

A Parking Strategy for Surrey
Parking Standards for Development
Local Character and Distinctiveness
Design Guide SPD
Climate Change and Sustainable
Construction SPD

Other

Human Rights Act 1998
Community Infrastructure Levy
Regulations 2010
The List of Historic Parks and
Gardens

6.0 **Assessment**

6.1 The main issues to consider are:

- Principle of development
- Design and impact on character of the area
- Neighbour amenity
- Access, parking and traffic generation
- Trees
- Ecology
- Sustainable construction
- Flooding and Drainage matters
- Crime
- Community Infrastructure Levy (CIL) and requested contributions
- Other matters

Principle of development

6.2 The site is located within the Salfords Principal Employment Area. Policy EMP1 states that "Planning permission will be granted for change of use to

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

offices, industrial, and storage and distribution, and for the development of new, upgraded or extended floor space within these uses.”

- 6.3 The existing site and buildings is not statutory or locally listed and therefore there is no objection to its demolition, which has already been allowed under a prior approval application.
- 6.4 There is an identified need for all employment uses within the available evidence and therefore the proposal to re-develop the site for employment uses is acceptable in principle.

Design and impact on character of the area

- 6.5 It is acknowledged that the proposed redevelopment of the site is significant in scale. Units A and B would measure 13.7m to top of parapet and approximately 15.2m at highest point (in centre of roof). Together unit A and B would spread across almost the full width of the site at approximately 127.5. remainder of the site would be made up of hardstanding and landscaping, including the retention of the tree buffer along the western boundary with the main road. The proposed development would therefore result in a significant change to the quantum of development on the site.
- 6.6 However, the proposal needs to be assessed in the context of the fact that the site is designated as a Principal Employment Area and its brownfield status where it is important to make efficient use of the site. The character and land uses in the surrounding area which are of industrial and commercial developments, particularly the recently developed site to the south of the site and the nature of the proposed use, which necessitates buildings of a certain scale and design.
- 6.7 In terms of the layout and scale of the buildings this is considered typical for a modern industrial/commercial development with large buildings measuring up to 15m tall and a significant area of hardstanding for both HGV and car parking. The density study submitted by the applicant shows the proposal site in the context of the wider Salfords Industrial Estate to the north and south of the site, as well as comparison with the recently development site (Goya site) immediately to the south. The Density study clearly shows that the scale and density of the proposal is in line with the general pattern of development within the Industrial Estate. In terms of the comparison with the Goya site the proposed density would be 48.2% compared to 48.8%, the proposed site coverage would be 44% compared to 43% and the proposed heights (clear internal height) would be in keeping with the site to south which ranges from 10.5m to 12.5m. It is therefore considered that considered in this context the proposal would not be out of keeping with the general character of the wider industrial estate.
- 6.8 The proposed design of the units would be fairly simple and utilitarian but this is the nature of such buildings and given the location of the site within a designated industrial estate it is considered that the proposed buildings could not be refused on this ground. Officers have asked the applicant to consider

ways in which they could add interest and break up the elevations. The applicant has indicated that they are willing to review the finish of the elevations in terms of cladding and colours proposed. It is considered that were the application approved a condition could be added to secure further details.

- 6.9 Another important factor on this site is the existing treed nature of the western boundary and south-western corner of the site. The impact on trees is discussed in more detail below but in terms of the location of the building the closest part of the proposal would be approximately 15.3m from Bonehurst Road, which is commensurate with the spacing left by the development to the south (approx. 15.5m). This spacing ensures that the trees along the western boundary can be retained and provides an adequate set back from the road to limit the visual impact of the proposal on the streetscene.
- 6.10 In terms of the potential impact on the wider landscape the application is accompanied with a Strategic Landscape and Visual Impact Assessment (SLVIA). Regarding potential landscape impacts, the SLVIA confirms that there are no known registered or designated heritage assets on the site, and that its current landscape quality and condition is classified as 'Poor', with no protective landscape designations. Accordingly, the SLVIA concludes that the proposed site design would result in a negligible to slight beneficial effect on the landscape features and character and a negligible effect on the land value, hydrology, cultural heritage, land use and topography.
- 6.11 The SLVIA also considers the visual effects of the proposals by identifying viewpoints and the potential impact on the surrounding residential properties, motorists and surrounding industrial development, referred to as visual receptors. In summary, the SLVIA concludes that, the proposals will result in negligible impacts on the residential and employment development and motorists in the vicinity of the site.
- 6.12 Overall, the SLVIA concludes that, through the management and enhancement of the existing vegetation will help screen the development and integrate it within the surrounding landscape to the east and west, whilst improving the landscape value to the north and south.
- 6.13 Therefore, whilst the scheme would result in a significant change to the existing character and nature of the site it is considered that the proposal achieves a good standard of design and a development which is in keeping with the scale and character of surrounding industrial developments and would not have a detrimental impact on the character of the area.
- 6.14 Conditions are recommended to secure finalised levels details, and further details of the proposed external materials, boundary treatments and hard landscaping.

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

Neighbour amenity

- 6.15 Development Management Policy DES1 expects all new development to provide an appropriate environment for future occupants whilst not adversely impacting upon the amenity of occupants of existing nearby buildings, including by way or overbearing, obtrusiveness, overshadowing, overlooking and loss of privacy.
- 6.16 The site borders a road to the west, the railway to the east and a road with industrial/commercial units to the south. As such there would be no concerns with the relationship to these neighbouring land uses.
- 6.17 To the north is a residential road know as Empire Villas which has the potential to be impacted by the proposal in terms of overbearing impact, loss of light and loss of privacy. In terms of privacy there would be no north facing windows to the building ensuring no opportunity for overlooking.
- 6.18 In terms of overbearing impact and loss of light to the nearest properties no. 1 to 8 Empire Villas would be located a minimum of approximately 25m from the closest part of the building and these properties front on to Empire Villas so the rear of their properties would be unaffected in terms of outlook. The applicant has demonstrated that the new building would pass the 25 degree test which indicates that the impact on light would be acceptable. Given this and the distance between these dwellings and the proposed building and the retained screening it is not considered that the proposal would result in an unacceptable loss of light or overbearing impact.
- 6.19 No.11 Empire Villas, located at the eastern end of Empire Villas would be the nearest dwelling, at approximately 11.9m from the proposed building. The house is however orientated east-west and therefore the main outlook for this property is not towards the building. When 45 degree line is made from the top of the proposed building the line would not conflict with any of the nearest east or west facing windows indicating that there would not be an unacceptable loss of light. There is a side window facing the proposed building which would fail the 25 degree test however from a site visit it was clear that this window was a secondary window and therefore the room it serves would not be unacceptable impacted. Whilst the proposed building would be significant in height it is considered that the distance of over 10metres to the side of no.11 combined with the east – west orientation would ensure that there is not an unacceptable loss of outlook or overbearing impact.
- 6.20 No.28 Beechwood Villas would have a similar relationship to no.11 but the spacing would be greater. As such for similar reasons as no.11 the impact is considered to be acceptable.
- 6.21 In terms of the possible impact of the proposal on the dwellings to north from matters such as noise and light spill the Council has consulted its Environmental Health consultants Regulatory Support Services (RSS). They have reviewed the proposed plans and submitted technical reports and has

raised no objection subject to conditions to limits impact on the residential properties to the north in terms of noise and lighting. One of the recommended conditions relates to Construction and Environmental Management Plan. These requirements will be added in to the Construction Management Statement (CMS) as discussed further below.

- 6.22 In terms of contamination the applicant has submitted a Phase 1 and Phase 2 report. The contamination officer has reviewed the information and has advised that "For the sites redevelopment application, the contaminated land conditions would still apply as further Phase 2 intrusive investigations reporting would need to be submitted in relation to that application". As such conditions are recommended to secure further information.
- 6.23 With regard to Air Quality the Council's Environmental Health Officer has raised no concern having reviewed the submitted Air Quality Assessment in terms of the proposed use of the site. They have asked that a suitable dust management scheme during construction is secured by condition. This will be covered within the CMS as set out below.
- 6.24 In terms of inconvenience during the construction period due to the proximity of the site to the residential dwellings to the north of the site it is accepted that there is potential for noise and disruption to these properties. It is not a reason to refuse an application given the temporary nature of the construction but it is considered that the potential impact could be minimised through the inclusion of a Construction Management Statement (CMS) which can secure further information in relation to matters such as working hours and procedures in place to reduce dust and noise emissions. The CMS can be secured by condition. As set out in the below transport section a condition is also recommended to secure further details of construction traffic, parking and storage management through a Construction Transport Management Plan (CTMP).
- 6.25 Taking the above into account, whilst neighbouring properties would experience a significant change as a result of the development, the proposals would not give rise to a serious detriment to their living conditions and thus comply with policy DES1 of the DMP and the general provisions of the NPPF (para 127) which seeks to ensure that developments provide a high standard of amenity for existing and future occupants.

Access, parking and traffic generation

- 6.26 Development Management Plan Policy TAP1 requires all types of development to provide safe and convenient access for all road users taking account of cumulative impacts, which would not unnecessarily impede the free flow of traffic, or compromise pedestrians or other transport modes. Traffic resulting from a development must not materially impede traffic congestion on the highway network or increase the risk of accidents.
- 6.27 The National Planning Policy Framework at paragraph 109 confirms that "Development should only be prevented or refused on highways grounds if

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.”

6.28 There would be new access proposed as part of the development and the parking provision would change from 168 cars spaces, 68 Light goods vehicles / public carrier vehicles to 88 car parking space (with potential for additional 31 spaces – total 119), and 10 HGV loading/parking spaces, reflecting the lower on site employment generating potential of the proposed use.

6.29 Surrey County Council as the County Highway Authority (CHA) has considered this additional information and has provided the following comments:

“The proposed development would lead to a reduction in existing two way movements from 136 movements in the am peak and 142 movements in the pm peak and 752 during the day to 31 two way movements in the am peak and 28 two way movement in the pm peak and 374 movements during the day. As such there has been no junction assessment carried out due to the reduction in movements.

The reduction is due to the proposed change of use from a predominantly office based use with some workshops to a Use Class E (g) (iii) and B2 and B8. It is recommend that a condition is imposed that the site does not become an office based development as this has not been proposed and therefore assessed.

The proposed quantum of parking for any use applied for is adequate and comparable to what historically has occurred on the site. In addition there are parking restrictions on Cross Oak Lane between its junction with Bonehurst Road and the rail bridge to the east of the access. Those parking restrictions would prevent inappropriate parking on the highway.

The layout of the site would be able to accommodate large vehicles likely to use the site, such vehicles would be able to enter and leave the site in forward gear.

The proposed access has been subject to a road safety audit, which recommended some modifications to the purposed access that can be incorporated into the access at the detailed design stage for subsequent Section 278 agreement works to build the access. One such concern at the safety audit is tall vehicles colliding with the bridge to the east. I have recommended a condition for the developer to submit a routing strategy to be submitted to and agreed in writing with the Local Planning Authority.”

6.30 In light of the above comments from the CHA the application is considered to have an acceptable highways and parking impact and is therefore considered to be compliant with policy TAP1.

Impact on trees

- 6.31 The applicant has included an Arboricultural Impact Assessment and Landscape Plan. The Tree Officer provided the following initial comments:
“The Arboricultural Impact Assessment from Landmark Trees is well detailed and thorough and explains how the majority of site trees can be retained during the proposed development and explains the removal of those trees that will need to be taken out.

The Tree Protection Plan demonstrates the protection of the retained trees. The line of Tree Protection Fencing seems to stop though before the eastern end of the line along the north side boundary and misses off the eastern part of G3 – this must be either rectified or justified before the plan can be approved.

The Tree Protection Plan also shows parts of the root protection area of some retained trees that are outside of the protective fence line. These areas are shown to be protected by ‘Ground Protection’ with the finished surface explained as of a ‘no-dig’ construction in the AIA. This is briefly and generally explained in the AIA and it looks to be proposed that the ‘no-dig’ base will be laid down as initial ground protection with the surface renewed when construction is completed. This sounds like a useful proposal, but it is not clear how this will work as part of the whole site – for example there may be level changes – also, the demolition and construction machinery and the post-development site traffic is likely to be heavy. It is suggested that the details on this could be provided via a conditioned AMS – which is so, however, if this detail is critical to the proposal, then it would be better to have a final specification up front with validation from an engineer and arboriculturist to ensure that it will all work and fit together on the ground.

The proposed landscape plan is detailed and extensive and should provide landscape screening and habitat value to the site. There is a concern that the new planting along the northwest and north side boundaries may struggle to establish in such proximity to the proposed building.”

- 6.32 Following the submission of additional information the Tree Officer has provided the following comments:

“In respect to earlier comments the ground protection position and function is now clear. However, it is what comes next that doesn’t yet seem to be finalized. The AIA document says at 8.2.5 that ‘Where sections of hard surfacing are required in close proximity to trees, it is recommended that ‘No Dig’ surfacing be employed in accordance with BS5837...and AAIS 1996[APN1]’. It also says at 8.2.6 that ‘If the RPA of a tree is encroached by underground service routes then BS5837...and NJUG Volume 4 provisions should be employed. If it is deemed necessary, further arboricultural advice must be sought’.

These are commonplace issue with trees and development, and it looks as though the ‘no-dig’ surfacing could be quite extensive given the amount of ground protection over areas of RPA not within the protective fencing. The

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

above are not Method Statements though and it is therefore not clear how this will work on site. The above points are understood – it is known what it means – but it is general and therefore it is not certain what will be done. It is common for the specifics of these matters to be designed post consent; however, these operations can be the most disruptive and can lead to irreparable damage to the roots systems of retained trees and their rooting environment if not properly carried out, and sometimes it just doesn't work.

As per my previous comments: 'It is suggested that the details on this could be provided via a conditioned AMS – which is so, however, if this detail is critical to the proposal, then it would be better to have a final specification up front with validation from an engineer and arboriculturist to ensure that it will all work and fit together on the ground'."

- 6.33 In light of the above comments from the Tree Officer and subject to conditions the application is considered to have an acceptable impact on the existing site and surrounding trees and the proposal is therefore compliant with policy NHE3 of the Development Management Plan 2019.

Ecology

- 6.34 The planning application is supported by a Preliminary Ecological Appraisal (PEA). The report concludes that the site has relatively low ecological significance with no impact on statutory or non-statutory site and unlikely to be any impact on protected species. Surrey Wildlife Trust (SWT) has considered the submitted report and following further information regarding bats has advised that the reports and surveys have been produced in line with best practice and therefore subject to a condition securing a Construction Environmental Management Plan (CEMP) and Landscape Ecological Management Plan (LEMP) they consider that the impact would be acceptable.
- 6.35 In terms of net gain in biodiversity the submitted documents show that the scheme will not provide a net gain. In such circumstances policy NHE2(b) does allow for a contribution towards off site provision to off-set this impact. The applicant has made clear that they are willing to agree to such a contribution however the Council currently has no mechanism to allow for an off-site contribution, with no projects or sites currently identified for this. It is noted that the NPPF (para 180 d) requires that when determining planning application Local Planning Authorities should apply the following principle "development whose primary objective is to conserve or enhance biodiversity should be supported; while opportunities to improve biodiversity in and around developments should be integrated as part of their design, especially where this can secure measurable net gains for biodiversity or enhance public access to nature where this is appropriate." However the NPPF does not require a measurable net gain and policy NHE2 5b. states that schemes will be expected to "be designed, wherever possible, to achieve a net gain in biodiversity." In this case the applicant has set out that it is not possible to achieve a net gain in biodiversity and given the national and local policy position it is not considered that this could form a reasonable reason for

refusal. The proposal does however include a number of on-site enhancement measures and the LEMP condition recommend by SWT would secure further details of these measures as well as future maintenance.

- 6.36 Subject to the conditions discussed it is considered that the scheme would comply with policy NHE2 of the DMP.

Sustainable Construction

- 6.37 The proposal is required to meet policy CS11 of the Core Strategy and policy CCF1 of Development Management Plan. This requires the development to meet BREEAM 'very good' and to include renewable or low-carbon energy generation to provide 10% of the expected energy usage of the development.

- 6.38 The submitted reports confirm that the building is aiming to meet the BREEAM 'very good' rating and that it would be able to achieve a 19.9% reduction in carbon emissions through PV panels. Conditions are recommended to secure implementation.

Flooding and Drainage matters

- 6.39 The site is in Flood Zone 1 and is therefore at a low risk of fluvial flooding but the road and land to the south of the site is within flood zone 1 and 2. The site is designated at being at risk of surface water flooding. A Flood Risk Assessment and Drainage strategy document has been submitted with the application.

- 6.40 The Flood Risk Assessment has been assessed by the Environment Agency who has raised no objection to the proposals and has not recommended any conditions.

- 6.41 The drainage strategy has been considered by Surrey County Council as the Lead Local Flood Authority who has concluded that it meets the requirements of national technical standards. They therefore raise no objection subject to a condition securing finalised details of the drainage strategy and implementation.

Crime

- 6.42 Policy DES1 requires that development: "Creates a safe environment, incorporating measures to reduce opportunities for crime and maximising opportunities for natural surveillance of public places. Developments should incorporate measures and principles recommended by Secured by Design."

- 6.43 The submitted Design and Access Statement has a section on security which states that the following have been designed into the scheme:
- Access and movement has been well considered to create well-defined routes and entrances that provide convenient movement throughout the development, without compromising security.
 - The layout has been developed so different uses do not cause conflict.

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

- All publicly accessible spaces have been orientated in a way that they are all overlooked.
 - A sense of safety will be achieved through the level of human activity that will be present throughout the day.
- 6.44 The development has only one access point into the site, thus providing a secure entrance point and visibility for each of the units, offering natural surveillance for the proposed building units.
- 6.45 The sides and rear of the site, as well as the yard areas will be enclosed by the use of 2.4m high secure weld mesh fencing, which will help mitigate the risk of crime, providing safety for employers and employees, thus preventing the intrusion of trespassers.
- 6.46 In order to secure further information a condition in relation to Secure by Design is recommended.

Community Infrastructure Levy (CIL) and requested contributions

- 6.47 The proposal, being for new industrial and distribution premises, falls outside of the uses which attract a charge based on the Council's adopted Charging Schedule and as such the development would not be liable to pay CIL.
- 6.48 The Community Infrastructure Levy (CIL) Regulations were introduced in April 2010 and state that it is unlawful to take a planning obligation into account unless its requirements are (i) relevant to planning; (ii) necessary to make the proposed development acceptable in planning terms; and (iii) directly related to the proposed development. As such only contributions that are directly required as a consequence of development can be requested and such requests must be fully justified with evidence including costed spending plans to demonstrate what the money requested would be spent on.
- 6.49 In this case, as above, the County Council has requested contributions towards travel plan monitoring (£6,150). These contributions are considered necessary to address and avoid potential unacceptable highways impacts and are proportionate in their scale and kind to the development proposed. A clear justification has been provided by the County Highway Authority. These will be secured through a legal agreement.

Other matters

- 6.50 The site is close to Gatwick airport and therefore it has been examined by Gatwick Airport from an aerodrome safeguarding perspective. No objection has been raised subject to a condition to secure a Bird Hazard Management Plan and details of the proposed solar panels.
- 6.51 Representations have been received asking that the existing commemorative plaque regarding the historic use of the site for the Philips Research Laboratory. The applicant has confirmed they are happy to protect the

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

plaque during construction and retain it on site. A condition is recommended to secure this.

CONDITIONS

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan Type	Reference	Version	Date Received
<i>Roof Plan</i>	2105-PL-1002	P2	21.04.2022
<i>Roof Plan</i>	2105-PL-1006	P2	21.04.2022
<i>Elevation Plan</i>	2105-PL-1015	P4	24.06.2022
<i>Elevation Plan</i>	2105-PL-1016	P4	24.06.2022
<i>Section Plan</i>	2105-PL-0020	P3	21.06.2022
<i>Elevation Plan</i>	2105-PL-0021	P4	24.06.2022
<i>Section Plan</i>	2105-PL-1020	P3	21.06.2022
Site Layout Plan	2105-PL-0001	P3	06.05.2022
Site Layout Plan	2105-PL-0003	P3	06.05.2022
<i>Location Plan</i>	2105-EX-0099	P1	06.06.2022
Landscaping Plan	21-120-P-01	A	10.01.2022
Existing Plans	2105-EX-0021	P1	29.12.2021
Existing Plans	2105-EX-0001	P1	29.12.2021
Existing Plans	2105-EX-0020	P1	29.12.2021
Other Plan	2105-PL-0002	P1	29.12.2021
<i>Floor Plan</i>	2105-PL-1000	P1	29.12.2021
Floor Plan	2105-PL-1001	P1	29.12.2021
Floor Plan	2105-PL-1005	P1	29.12.2021

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

3. No development shall commence, excluding demolition and enabling works, until a Construction Management Statement (CMS), to include details of:
 - a) An introduction consisting of construction phase environmental management plan, definitions and abbreviations and project description and location;
 - b) A description of management responsibilities;
 - c) A description of the construction programme which identifies activities likely to cause high levels of noise or dust;
 - d) Prediction of potential impacts with regard to water, waste, vibration, emissions and odours. Where potential impacts are identified, mitigation measures should be identified to address these impacts.

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

- e) Information about the measures that will be used to protect privacy and the amenity of surrounding sensitive uses; including provision of appropriate boundary protection.
- f) Means of communication and liaison with LPA, neighbouring residents and businesses.
- g) Site working hours and a named person for residents to contact;
- h) Detailed Site logistics arrangements;
- i) Details regarding parking, deliveries, and storage;
- j) Details regarding dust and noise mitigation measures to be deployed including identification of sensitive receptors and ongoing monitoring;
- k) Details of the hours of works and other measures to mitigate the impact of construction on the amenity of the area

Has been submitted to and improved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The condition above is required in order that the development is managed in a safe and considerate manner to help mitigate potential impact on the amenity and safety of neighbours and to accord with Reigate and Banstead Development Management Plan 2019 policy DES8.

4. No development shall take place, excluding demolition and enabling works, until the developer obtains the Local Planning Authority's written approval of details of both existing and proposed ground levels and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

Reason: To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining buildings and to safeguard the visual amenities of the locality including heritage assets with regard to Reigate and Banstead Development Management Plan DES1 and NHE9.

5. Prior to commencement of development, excluding demolition and enabling works, in follow-up to the contamination information already supplied, a contaminated land site investigation proposal, detailing the extent and methodologies of sampling, analyses and proposed assessment criteria required to enable the characterisation of the plausible pollutant linkages identified in the preliminary conceptual model, shall be submitted to the Local Planning Authority. This is subject to the written approval in writing of the Local Planning Authority, and any additional requirements that it may specify, prior to any site investigation being commenced on site. Following approval, the Local Planning Authority shall be given a minimum of two weeks written notice of the commencement of site investigation works. Please note this means a proposal is required to be submitted and approved prior to actually undertaking a Site Investigation.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard the Reigate and Banstead Local

Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

6. Prior to commencement of the development, excluding demolition and enabling works, a contaminated land site investigation and risk assessment, undertaken in accordance with the site investigation proposal as approved that determines the extent and nature of contamination on site and is reported in accordance with the standards of DEFRA's and the Environment Agency's Land Contamination: Risk Management Guidance (2020) and British Standard BS 10175, shall be submitted to the Local Planning Authority and is subject to the approval in writing of the Local Planning Authority and any additional requirements that it may specify. If applicable, ground gas risk assessments should be completed in line with CIRIA C665 guidance.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

7. a. Prior to commencement of the development, excluding demolition and enabling works, a detailed remediation method statement should be produced that details the extent and method(s) by which the site is to be remediated, to ensure that unacceptable risks are not posed to identified receptors at the site and details of the information to be included in a validation report, has been submitted to and approved in writing by the Local Planning Authority, and any additional requirements that it may specify, prior to the remediation being commenced on site. The Local Planning Authority shall then be given a minimum of two weeks written notice of the commencement of remediation works.
 - b. Prior to occupation, a remediation validation report for the site shall be submitted to the Local Planning Authority in writing. The report shall detail evidence of the remediation, the effectiveness of the remediation carried out and the results of post remediation works, in accordance with the approved remediation method statement and any addenda thereto, so as to enable future interested parties, including regulators, to have a single record of the remediation undertaken at the site. Should specific ground gas mitigation measures be required to be incorporated into a development the testing and verification of such systems should have regard to CIRIA C735 guidance document entitled 'Good practice on the testing and verification of protection systems for buildings against hazardous ground gases' and British Standard BS 8285 Code of practice for the design of protective measures for methane and carbon dioxide ground gases for new buildings.

Reason: To demonstrate remedial works are appropriate and demonstrate the effectiveness of remediation works so that the proposed development will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

8. Unexpected ground contamination: Contamination not previously identified by the site investigation, but subsequently found to be present at the site shall be reported to the Local Planning Authority as soon as is practicable. If deemed necessary development shall cease on site until an addendum to the remediation method statement, detailing how the unsuspected contamination is to be dealt with, has been submitted in writing to the Local Planning Authority. The remediation method statement is subject to the written approval of the Local Planning Authority and any additional requirements that it may specify.

Note: Should no further contamination be identified then a brief comment to this effect shall be required to discharge this condition.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and Contaminated Land) and the NPPF.

9. Prior to the commencement of any development works, including demolition and all construction activities, all tree protection measures shall be undertaken in strict accordance with the approved details contained in the submitted Arboricultural Impact Assessment Report ref. WRA/COL/AIA/01 and Arboricultural Method Statement – Demolition Stage ref. WRA/COL/AMS/01 from Landmark Trees. All arboricultural matters will then follow that described in these approved details.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3 and DES1 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard BS 5837:2012 'Trees in relation to design, demolition and construction – Recommendations'

10. No development shall commence, excluding demolition works, until a detailed, scaled and finalized Tree Protection Plan (TPP) and the related Arboricultural Method Statement for the Construction Stage (AMS) is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas (RPAs) of trees, shown to scale on the TPP. This must include details of all service routes, materials and methodology for any excavation and construction within the RPA of retained trees and a schedule of arboricultural supervision and reporting. All works must be carried out in strict accordance with these details when approved.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance

of the area and to comply with policies NHE3, DES1 and DES3 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard 5837:2012 Trees in relation to design, demolition and construction.

11. The development hereby permitted shall not commence, excluding demolition and enabling works, until details of the design of a surface water drainage scheme have been submitted to and approved in writing by the planning authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDS, NPPF and Ministerial Statement on SuDS. The required drainage details shall include:
- a) The results of further infiltration testing completed in accordance with BRE Digest: 365 and confirmation of groundwater levels.
 - b) Evidence that the proposed final solution will effectively manage the 1 in 30 & 1 in 100 (+40% allowance for climate change) storm events, during all stages of the development. The final solution should follow the principles set out in the approved drainage strategy. If infiltration is deemed unfeasible, associated discharge rates and storage volumes shall be provided using a maximum discharge rate of 8.3l/s.
 - c) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels, and long and cross sections of each element including details of any flow restrictions and maintenance/risk reducing features (silt traps, inspection chambers etc.). Confirmation is required of a 1m unsaturated zone from the base of any proposed soakaway to the seasonal high groundwater level and confirmation of half-drain times.
 - d) A plan showing exceedance flows (i.e. during rainfall greater than design events or during blockage) and how property on and off site will be protected from increased flood risk.
 - e) Details of drainage management responsibilities and maintenance regimes for the drainage system.
 - f) Details of how the drainage system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the drainage system is operational.

Reason: To ensure the design meets the technical standards for SuDS and the final drainage design does not increase flood risk on or off site in accordance with, Policy CS10 of the Core Strategy 2014, Policies DES9 and CCF2 of the Development Management Plan 2019 and the 2019 NPPF.

12. Prior to the first occupation of the development, a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority. This must demonstrate that the surface water drainage system has been constructed as per the agreed scheme (or detail any minor variations), provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/areas, flow restriction devices and outfalls), and confirm any defects have been rectified.

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

The drainage system shall therefore be retained and maintained in accordance with the agreed details.

Reason: To ensure the drainage system is constructed to the national Non-Statutory Technical Standards for SuDs in order to mitigate against the risk of surface water flooding with regard to policy INF1 and CCF2 of the Reigate and Banstead Development Management Plan 2019.

13. No development shall commence on site, excluding demolition and enabling works, until a scheme for the soft and hard landscaping (including hard surfacing and any street furniture), including details of existing landscape features to be retained or pruned, has been submitted and approved in writing by the local planning authority. The landscaping scheme shall include details of hard landscaping, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to first occupation of the approved development or in accordance with a programme agreed in writing with the local planning authority

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837. Trees in relation to construction.

Any trees shrubs or plants planted or retained in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason: To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with Reigate and Banstead Borough Development Management Plan 2019 policies NHE3 and DES1, British Standards including BS8545:2014 and British Standard 5837:2012.

14. No pruning, removal or other works to the retained trees and hedges located both within and overhanging the site, shall take place during construction, or for 5 years after completion except with the prior written approval of the Local Planning Authority. Any tree works already approved as part of this consent and any other work undertaken should be done in accordance with British Standard BS 3998:2010 'Tree Work - Recommendations'. If any of the retained trees or hedges, within the site, controlled by this condition, are removed, die, or become damaged or diseased within five years of completion, they shall be replaced before the expiry of one calendar year by tree/s or hedge/s to a planting specification agreed in writing by the Local Planning Authority.

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

Reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3 and DES1 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard BS 5837:2012 'Trees in relation to design, demolition and construction – Recommendations'

15. The development hereby permitted shall not commence, excluding demolition and enabling works, until a Bird Hazard Management Plan has been submitted to and approved in writing by the Local Planning Authority. The submitted plan shall include details of:
- Management of any flat/shallow pitched roofs on buildings within the site which may be attractive to nesting, roosting and "loafing" birds.

The Bird Hazard Management plan shall be implemented as approved upon completion of the roofs and shall remain in force for the life of the buildings. No subsequent alterations to the plan are to take place unless first submitted to and approved in writing by the Local Planning Authority.

Reason: To manage the roofs in order to minimise their attractiveness to birds which could endanger the safe movement of aircraft and the operation of Gatwick Airport.

16. No solar panels are to be installed until full details of the proposed PV scheme has been submitted to and approved in writing by the Local Planning Authority.

The scheme shall be implemented as approved and no subsequent alterations to the approved scheme are to take place unless first submitted to and approved in writing by the Local Planning Authority.

Reason: to ensure the development does not endanger the safe movement of aircraft or the operation of Gatwick Airport through interference with communication, navigational aids and surveillance equipment and glare issues.

17. No development shall take place above slab level until written details of the materials to be used in the construction of the external surfaces, including fenestration and roof and the type and colour of walls and any cladding, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

18. The development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected has been submitted to and approved in writing by the Local Planning Authority. The

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

boundary treatment shall be completed before the occupation of the development hereby permitted.

Reason: In the interest of maintaining the historic and architectural character of the listed building, historic gardens and the visual amenities of the area with regard to Reigate and Banstead Development Management Plan 2019 policies DES1.

19. The development hereby approved shall not be commenced, excluding demolition and enabling works, unless and until a scheme for the protection and retention/reinstatement of the existing commemorative Philips Research Laboratory plaque has been submitted to and approved in writing by the Local Planning Authority. This shall include any necessary report works and details of its proposed location within the site.

Reason: In order to secure a realistic strategy for the preservation of this non-designated heritage asset with regard to policy CS4 of the Reigate and Banstead Core Strategy 2014 and the provisions of the NPPF.

20. No development shall commence, excluding demolition and enabling works, until a Construction Transport Management Plan, to include details of:
- (a) parking for vehicles of site personnel, operatives and visitors
 - (b) loading and unloading of plant and materials
 - (c) storage of plant and materials
 - (d) programme of works (including measures for traffic management)
 - (e) provision of boundary hoarding behind any visibility zones
 - (g) vehicle routing
 - (h) measures to prevent the deposit of materials on the highway
 - (i) before and after construction condition surveys of the highway and a commitment to fund the repair of any damage caused
 - (k) on-site turning for construction vehicles
- has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy DES8 Construction Management of the Reigate and Banstead Local Plan Development Management Plan September 2019.

21. The development shall not be occupied until the proposed belmouth vehicular access to Cross Oak Lane has been constructed in accordance with the approved plan numbered TPHS/276/DR/12.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

22. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with approved plan numbered 2105 PL 001 Rev3 *and/or* 2105 PL 0003 Rev 3 for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking and turning areas shall be retained and maintained for their designated purposes.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

23. The development hereby approved shall not be occupied unless and until each of the proposed parking spaces are provided with a fast charge socket (current minimum requirements - 7 kw Mode 3 with Type 2 connector - 230v AC 32 Amp single phase dedicated supply) and an electrical supply for future fast charge sockets in accordance with the approved plans numbered 2105 PL 001 Rev3 *and/or* 2105 PL 0003 Rev 3 and thereafter retained and maintained to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

24. Notwithstanding the submitted April 2022 Framework Travel Plan (Draft) prior to the occupation of the development a revised Travel Plan shall be submitted taking account of the occupants of the site for the written approval of the Local Planning Authority in accordance with the sustainable development aims and objectives of the National Planning Policy Framework, and Surrey County Council's "Travel Plans Good Practice Guide". And then the approved Travel Plan shall be implemented upon occupation of the site and for each and every subsequent occupation of the development, thereafter maintain and develop the Travel Plan to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

25. *The proposed development shall not be occupied until a routing strategy to prevent tall/heavy vehicles heading east from the access and to access the site from the east and to avoid other narrow country road and residential estates (such as The Acres and Westvale) in the surrounding area has been submitted to and approved in writing with the Local Planning Authority.*

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

26. The development hereby approved shall not be first occupied unless and until the following facilities have been provided in accordance with a scheme to be submitted to and approved in writing for:

(a) The secure parking for bicycles within the development site in line with the minimum number required by the Development Management Plan Annex 4.

And thereafter the said approved facilities shall be provided, retained and maintained to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

27. No development shall commence, *excluding demolition and enabling works*, until a Construction Environment Management Plan (CEMP) has been submitted to and approved in writing by the Local Planning Authority (LPA). The CEMP shall include, but not be limited to:

- a) Map showing the location of all of the ecological features
- b) Risk assessment of the potentially damaging construction activities
- c) Practical measures to avoid and reduce impacts during construction
- d) Location and timing of works to avoid harm to biodiversity features
- e) Responsible persons and lines of communication
- f) Use of protected fences, exclusion barriers and warning signs.
- g) Tree Removal and Bats Strategy and Method Statement

The development shall only be carried out in accordance with the agreed mitigation measures.

Reason: To ensure that any potential impact to protected species is adequately mitigated in accordance with the provisions of the National Planning Policy Framework and policy NHE2 of the Development Management Plan 2019.

28. No development shall commence, *excluding demolition, enabling works and above ground works*, until a Landscape and Ecological Management Plan (LEMP) has been submitted to and approved in writing by the Local Planning Authority (LPA). The LEMP should be based on the proposed impact avoidance, mitigation and enhancement measures specified in the submitted ecology reports and shall include, but not be limited to following:

- a) Description and evaluation of features to be managed

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

- b) Ecological trends and constraints on site that might influence management
- c) Aims and objectives of management
- d) Appropriate management options for achieving aims and objectives
- e) Prescriptions for management actions, together with a plan of management compartments
- f) Preparation of a work schedule (including an annual work plan capable of being rolled forward over a five-year period
- g) Details of the body or organisation responsible for implementation of the plan
- h) Ongoing monitoring and remedial measures
- i) Legal and funding mechanisms by which the long-term implementation of the plan will be secured by the applicant with the management body(ies) responsible for its delivery.
- j) Monitoring strategy, including details of how contingencies and/or remedial action will be identified, agreed and implemented so that the development still delivers the fully functioning biodiversity objectives of the originally approved scheme.
- k) Sensitive lighting plan

The agreed details shall be implemented before occupation of this development, unless otherwise agreed in writing by the LPA, and maintained/monitored in accordance with the agreed details.

Reason: To provide enhancements to the biodiversity of the site in accordance with the provisions of the National Planning Policy Framework and Reigate and Banstead Development Management Plan 2019 policy NHE2.

29. The development hereby approved shall not be first occupied unless and until the renewable energy measures detailed in the submitted Energy Assessment Report by Stroma ref 08-21-88654ES1, dated 13/12/2021, have been implemented, installed and made operational to ensure that the proposal includes renewable or low-carbon energy generation to provide 10% of the expected energy usage of the development.

Any change to the Energy Strategy and timing of implementation shall only be made once an updated report has been submitted to and agreed in writing by the LPA.

Thereafter the scheme development shall be maintained in accordance with the agreed details.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS11 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

30. The development hereby approved shall not be first occupied unless and until evidence has been provided that the development has met a minimum of BREEAM 'very good'.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS11 of the Reigate & Banstead Core Strategy 2014.

31. Full details of the measure taken to implement the recommendations detailed in the Noise Assessment report prepared by Stroma reference SBE Ref: 08-21-88654 – NC 01 and dated 27/10/2021 shall be submitted to and approved in writing the local planning authority. The details as approved by the local planning authority shall be fully installed before the development is occupied and thereafter be permanently retained.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

32. Prior to occupation of any unit a noise assessment shall be carried out to confirm the unit performs in accordance with the approved noise targets. Any additional steps required to mitigate noise shall be detailed and implemented, as necessary. The post completion noise assessment shall be submitted to and approved in writing by the local planning authority. The details as approved shall thereafter be permanently retained.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

33. No externally located plant or equipment shall be installed or operated without the prior written approval of the Local Planning Authority.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

34. The use hereby permitted, or the operation of any plant, machinery equipment or building services plant, shall not commence until an assessment of the cumulative acoustic impact arising from the operation all the plant, machinery or equipment has been submitted to and approved in writing by the local planning authority.

The assessment of the acoustic impact shall be undertaken in accordance with BS 4142: 2014 (or subsequent superseding equivalent) and current best practice and shall include a scheme of attenuation measures to ensure the cumulative rating level of noise emitted from the proposed plant, machinery equipment or building services plant is 5 dB less than background.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

35. The use hereby permitted, or the operation of any plant, machinery, equipment or building services plant, shall not commence until a post-installation noise assessment has been carried out to confirm compliance with the noise criteria. The scheme shall be implemented in accordance with the approved details and attenuation measures, and they shall be permanently retained and maintained in working order for the duration of the use and their operation.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

36. No activities, including loading or unloading, shall be undertaken in the open air after 23:00 hours or before 07:00 hours on any day.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

37. No externally located storage of any materials or waste is permitted.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

38. Prior to occupation a lighting scheme must be submitted for the approval of the Local Planning Authority in accordance with the Institute of Lighting Professional's Guidance notes for the reduction of obstructive light. The scheme must be designed by a suitably qualified person in accordance with the recommendations for environmental zone E3 in the ILP document "Guidance Notes for the Reduction of Obtrusive Light GN01:20.

Before commencement of operation of the approved lighting scheme the applicant shall appoint a suitably qualified member of the institute of lighting professionals (ILP) to validate that the lighting scheme as installed conforms to the recommendations for environmental zone E3 in the ILP document "Guidance Notes for the Reduction of Obtrusive Light GN01:20 the validation report shall be submitted to and approved by the local planning authority and the approved details and attenuation measures shall be permanently retained and maintained in working order for the duration of the use and their operation.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

39. The development shall not be occupied until a scheme demonstrating compliance with the principles of 'Secured by Design' has been submitted to and approved in writing by the Local Planning Authority. The approved details shall be completed before the occupation of the development hereby permitted and shall be permanently maintained as such thereafter.

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

Reason: To ensure that the development provides a secure environment for future residents in accordance with Policy DES1 of the Reigate & Banstead Development Management Plan 2019.

40. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015, (or any Order revoking and re-enacting that Order with or without modification), and Use Classes Order (or any Order revoking and re-enacting that Order with or without modification) the units hereby approved shall be occupied for purposes falling within Use Class E (g) (iii) (any industrial processes) B2 (general industrial and/or B8 (storage and/or distribution) only and shall not be subdivided or used within any other use without the prior written consent of the Local Planning Authority.

Reason: To control the use of the premises in the interests of maintaining an adequate supply of industrial, storage and distribution uses within the designated employment area and with respect to the adequacy of parking provision and potential impacts on the surrounding highway network.

INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.info.
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : Climate Change Information.
3. You are advised that the Council will expect the following measures to be included in the above CMS condition to control noise, pollution and parking:
 - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
 - (e) There should be no burning on site;
 - (f) Only minimal security lighting should be used outside the hours stated above; and

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

(g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - www.ccscheme.org.uk/index.php/site-registration.

4. The applicant is advised that the essential requirements for an acceptable communication plan forming part of the CMS are viewed as: (i) how those likely to be affected by the site's activities are identified and how they will be informed about the project, site activities and programme; (ii) how neighbours will be notified prior to any noisy/disruptive work or of any significant changes to site activity that may affect them; (iii) the arrangements that will be in place to ensure a reasonable telephone response during working hours; (iv) the name and contact details of the site manager who will be able to deal with complaints; and (v) how those who are interested in or affected will be routinely advised regarding the progress of the work. Registration and operation of the site to the standards set by the Considerate Constructors Scheme (<http://www.ccscheme.org.uk/>) would help fulfil these requirements.
5. The permission hereby granted shall not be construed as authority to carry out any works (including Stats connections/diversions required by the development itself or the associated highway works) on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a permit and, potentially, a Section 278 agreement must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works (including Stats connections/diversions required by the development itself or the associated highway works) on the highway will require a permit and an application will need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see: <http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme>. The applicant is also advised that Consent may be required under Section 23 of the Land Drainage Act 1991. Please see: www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/floodingadvice.
6. The developer is advised that as part of the detailed design of the highway works required by the above conditions, the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
7. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned

Agenda Item 5

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

- wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
8. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
 9. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to: <http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.
 10. The Highway Authority has no objection to the proposed development, subject to the above conditions but, if it is the applicant's intention to offer any of the roadworks included in the application for adoption as maintainable highways, permission under the Town and Country Planning Act should not be construed as approval to the highway engineering details necessary for inclusion in an Agreement under Section 38 of the Highways Act 1980. Further details about the post-planning adoption of roads may be obtained from the Transportation Development Planning Division of Surrey County Council.
 11. Notwithstanding any permission granted under the Planning Acts, no signs, devices or other apparatus may be erected within the limits of the highway without the express approval of the Highway Authority. It is not the policy of the Highway Authority to approve the erection of signs or other devices of a non-statutory nature within the limits of the highway.
 12. The permission hereby granted shall not be construed as authority to obstruct the public highway by the erection of scaffolding, hoarding or any other device or apparatus for which a licence must be sought from the Highway Authority Local Highways Service.
 13. Network rail - Due to the close proximity of the proposed development to Network Rail's land and the operational railway, Network Rail requests the applicant / developer engages Network Rail's Asset Protection and Optimisation (ASPRO) team via AssetProtectionLondonSouthEast@networkrail.co.uk prior to works commencing.
 14. Thames Water - A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. We would

Planning Committee
6 July 2022

Agenda Item: 5
21/03303/F

expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer.

15. Thames Water - The proposed development is located within 15 metres of Thames Waters underground assets and as such, the development could cause the assets to fail if appropriate measures are not taken. Please read our guide "working near our assets" to ensure your workings are in line with the necessary processes you need to follow if you're considering working above or near our pipes or other structures

16. Gatwick Safeguarding - Given the nature of the proposed development it is possible that a crane may be required during its construction. We would, therefore, draw the applicant's attention to the requirement within the British Standard Code of Practice for the safe use of Cranes, for crane operators to consult the aerodrome before erecting a crane in close proximity to an aerodrome. Gatwick Airport requires a minimum of four weeks notice. For crane queries/applications please email: lgwcranes@gatwickairport.com

REASON FOR PERMISSION

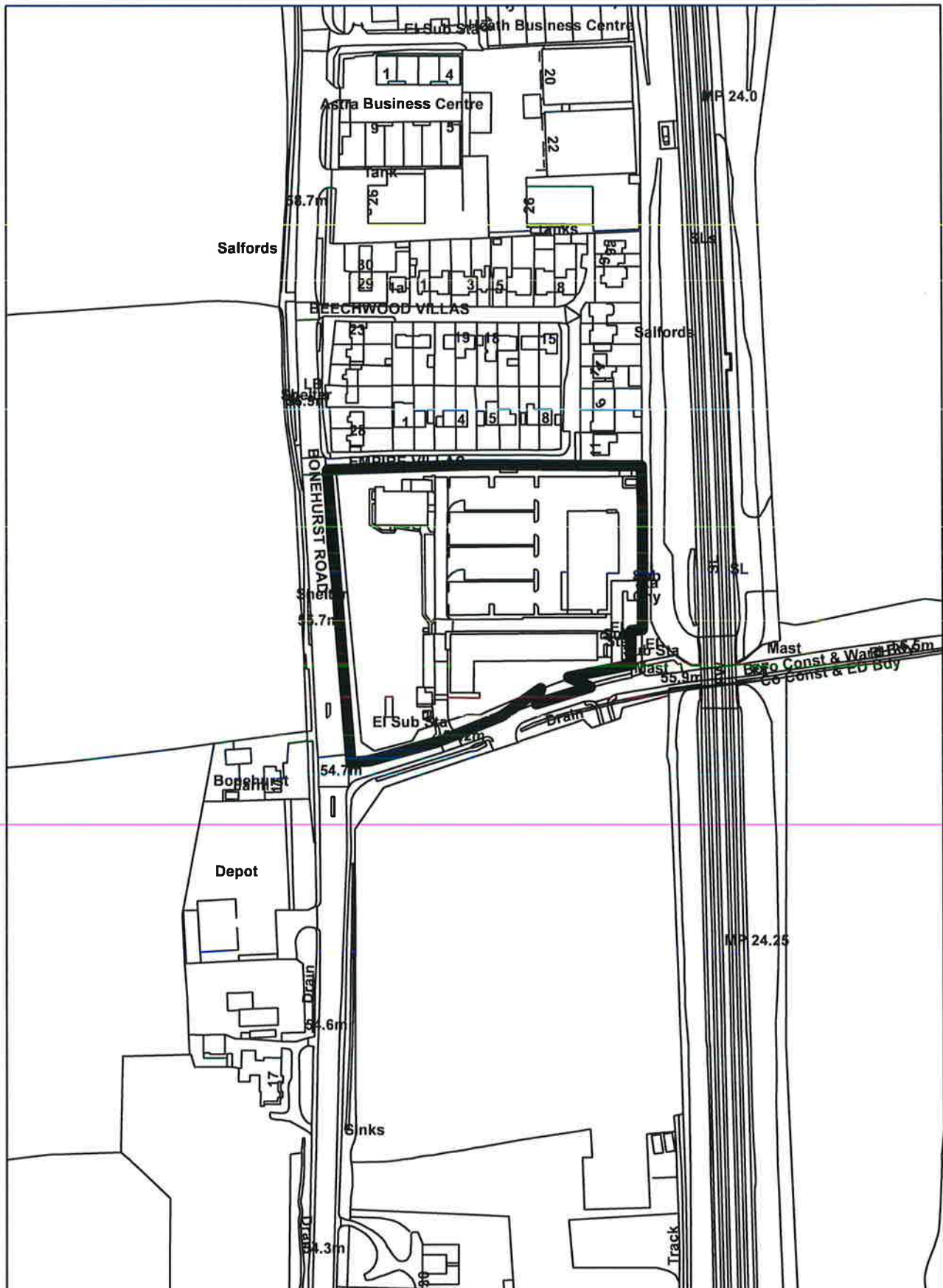
The development hereby permitted has been assessed against the relevant development plan policies as set out in the report and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

Proactive and Positive Statements

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

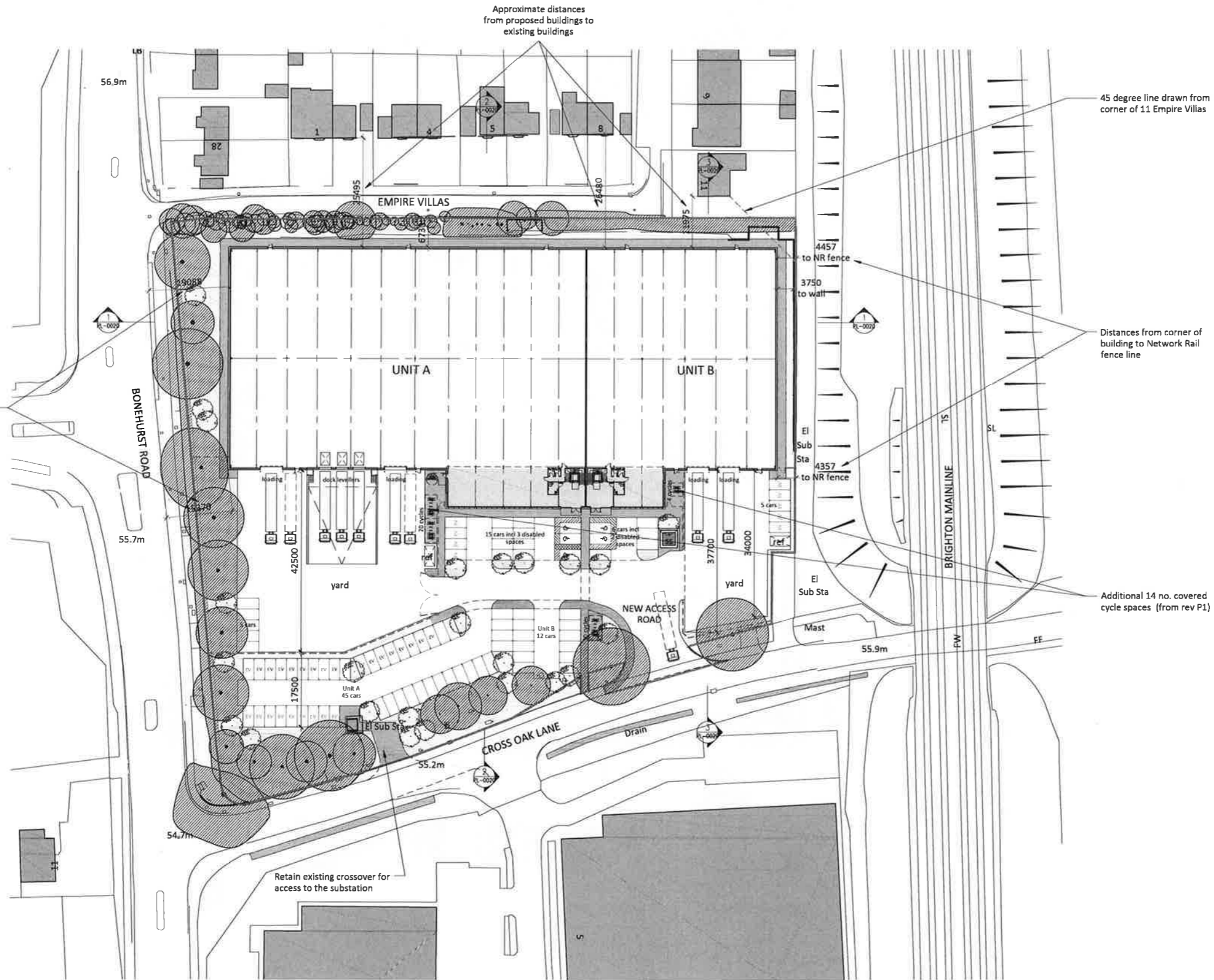
Agenda Item 5

21/03303/F - Titan House, Crossoak Lane, Salfords



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Scale 1:2,500



- KEY:**
- Planning Application Boundary
16,351 m² / 4.04 acres / 1.635 hectare
 - Proposed fence 2.4m high
 - ss Proposed substation location
 - ref Proposed refuse enclosure
 - EV 20% EV charging bays (18no.)
 - EV 20% potential future EV charging bays (18no.)

GROSS INTERNAL AREAS:

Unit	Area
Unit A -	5,091.5 sqm (54,800 sqft)
Warehouse	4,515.5 sqm (48,600 sqft)
incl office undercroft	
Offices on 1st floor	288 sqm (3,100 sqft)
Offices on 2nd floor	288 sqm (3,100 sqft)
Parking incl disabled	65 spaces
Cycle spaces	20 spaces
Unit B -	2,713.5 sqm (29,200 sqft)
Warehouse	2,397 sqm (25,800 sqft)
incl office undercroft	
Offices on 1st floor	158.25 sqm (1,700 sqft)
Offices on 2nd floor	158.25 sqm (1,700 sqft)
Parking incl disabled	23 spaces
Cycle spaces	14 spaces
TOTAL -	7,805 sqm (84,000 sqft)
Parking incl disabled	88 spaces
Cycle spaces	34 spaces

Rev	Notes	Date	By	Auth
P3	cycle parking numbers shown on plan	05.05.2022	WR	WRA
P2	14 cycle spaces added, EV charge points shown	04.04.2022	WR	WRA
P1	Issued for Planning Approval	10.12.2021	WR	WRA
Rev	Notes	dd.mm.yyyy	By	Auth

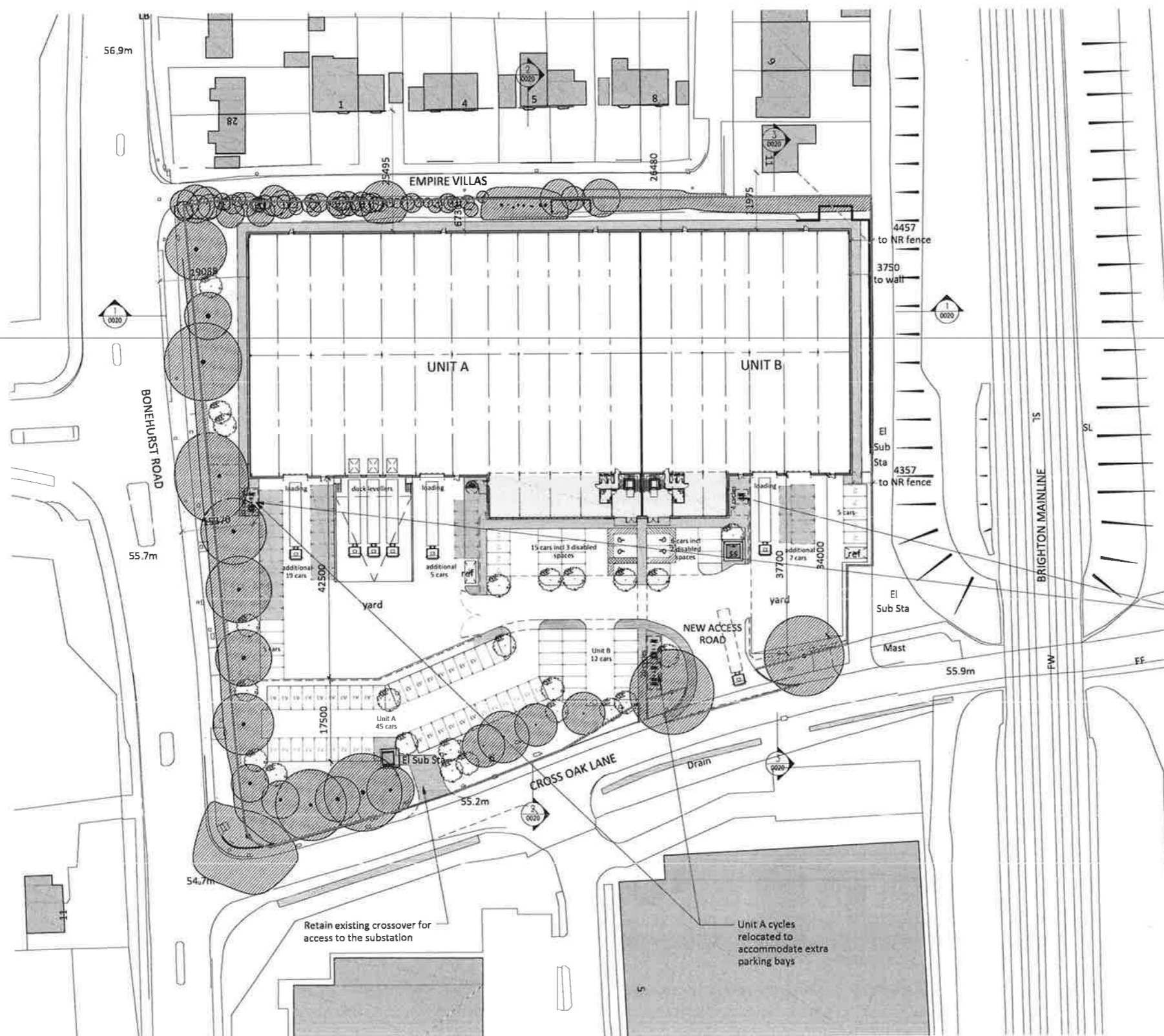


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**Cross Oak Lane
Redhill RH1 5EX**
Proposed Site Plan

Drawing No.: 2105-PL-0001 Rev: P3
scale: 1:1000 @ A3 10 December 2021

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Additional 14 no. covered cycle spaces (from rev P1)

- KEY:**
- Planning Application Boundary
16,351 m² / 4.04 acres / 1.635 hectare
 - - - Proposed fence 2.4m high
 - ss Proposed substation location
 - ref Proposed refuse enclosure
 - EV 20% EV charging bays (24no.)
 - EV 20% potential future EV charging bays (24no.)
 - █ Additional parking bays (allow for 20% EV charge bays [6no.] + 20% future EV charge bays [6no.])

PARKING & CYCLES

Unit A -	
Parking incl disabled	89 spaces
Cycle spaces	20 spaces
Unit B -	
Parking incl disabled	30 spaces
Cycle spaces	14 spaces
TOTAL	
Parking incl disabled	119 spaces
Cycle spaces	34 spaces

P3	cycle numbers added, extra EV charge points shown	05.05.2022	WR	WRA
P2	14 cycle spaces added, EV charge points shown	04.04.2022	WR	WRA
P1	Issued for Planning Approval	10.12.2021	WR	WRA
Rev	Notes	dd.mm.yyyy	By	Auth

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SCALE BAR IN mm

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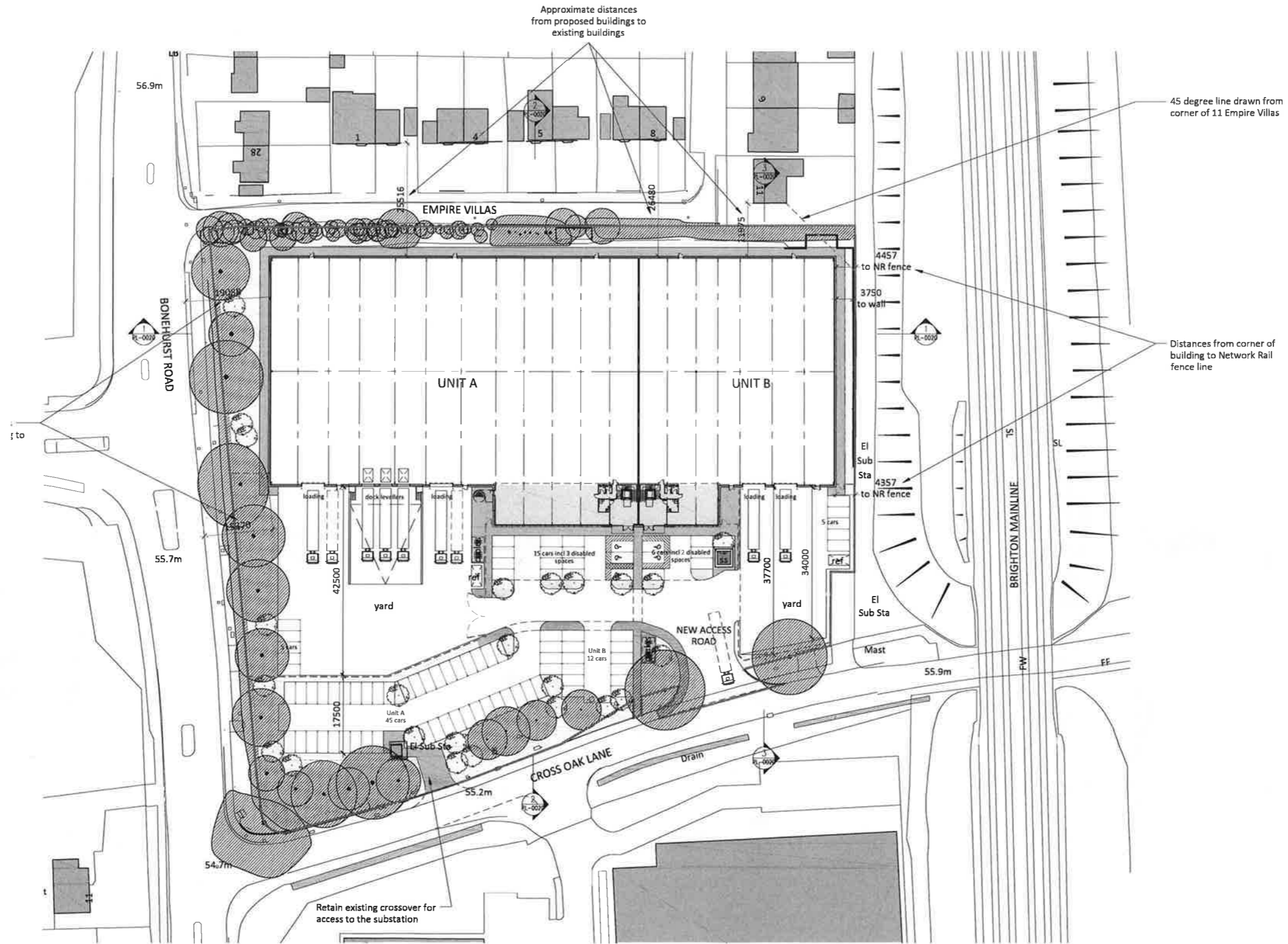
**Cross Oak Lane
 Redhill RH1 5EX**

**Proposed Site Plan with Parking
 Layout for B2 / E (g) (iii) Use**

Drawing No.: 2105-PL-0003 Rev: P3
 scale: 1:1000 @ A3 10 December 2021

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SITE LAYOUT OPTIONS STUDY
 SUBMITTED SITE LAYOUT FOR REFERENCE

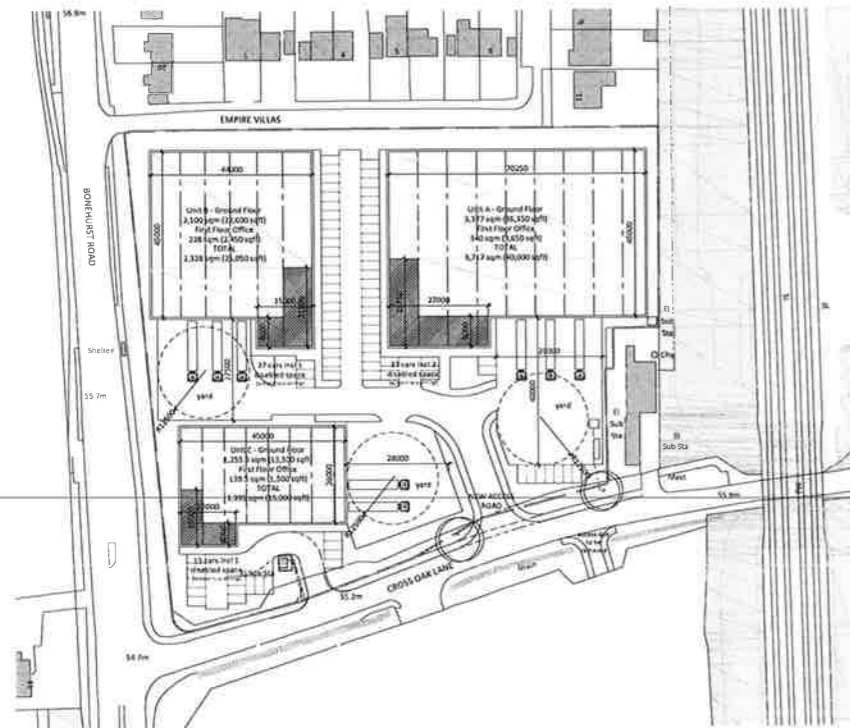


55

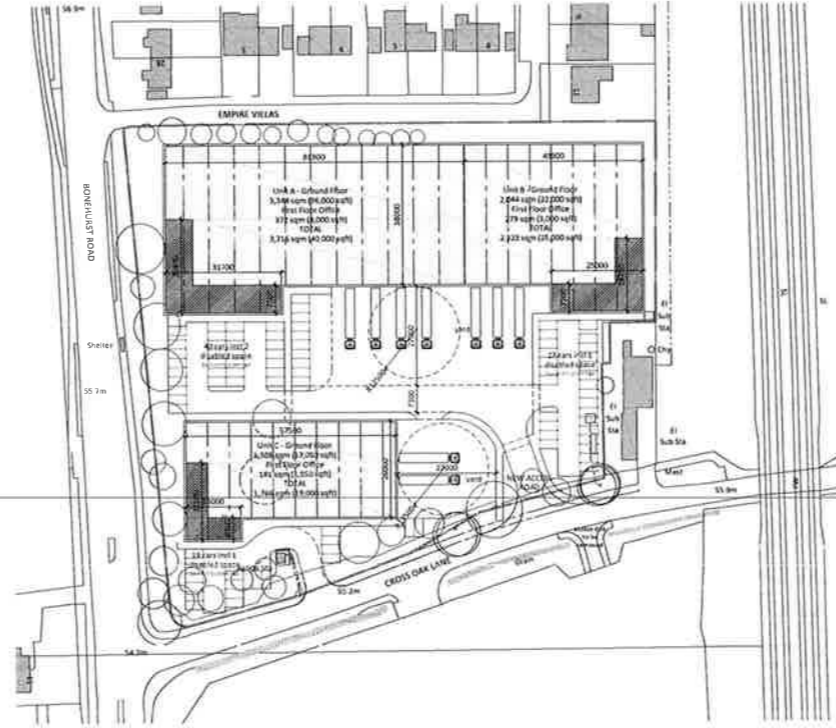
Agenda Item 5

SITE LAYOUT OPTIONS STUDY

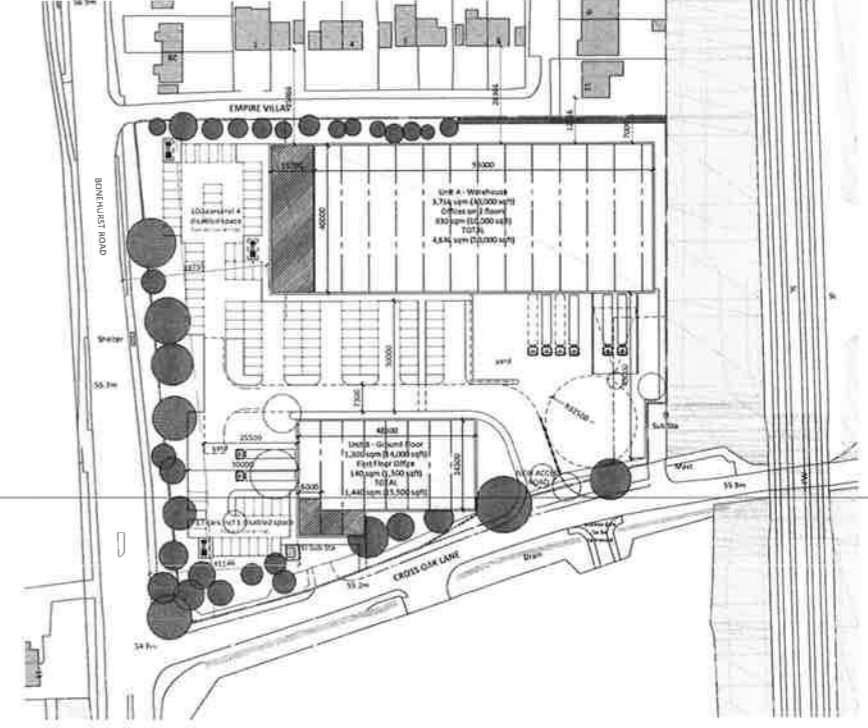
EARLIER SITE LAYOUT STUDIES PRIOR TO SUBMITTING THE PLANNING APPLICATION



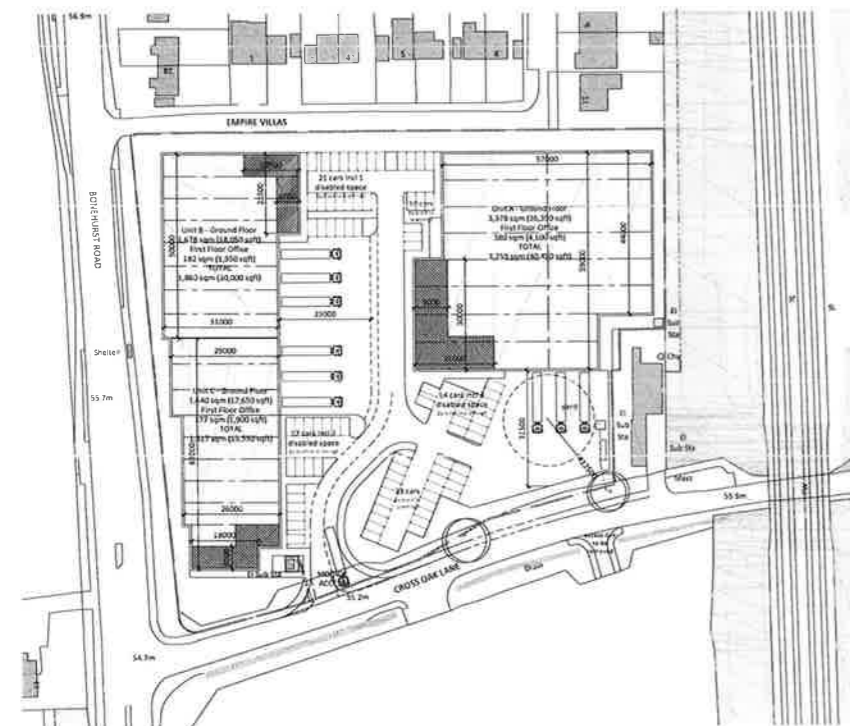
09 February 2021: THREE BUILDINGS - 3 UNITS, 2 ACCESS ROADS, GAP TO EMPIRE VILLAS & CLOSER TO BOUNDARY, LOSS OF TREES TO CROSS OAK LANE & BONEHURST ROAD



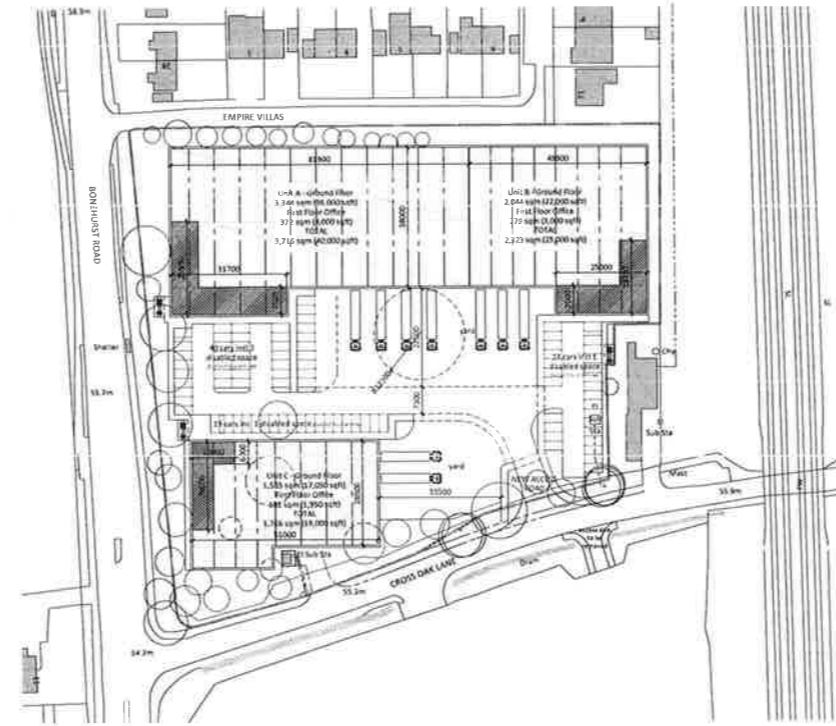
24 February 2021: TWO BUILDINGS - 3 UNITS, 2 ACCESS ROADS, CLOSER TO EMPIRE VILLAS BOUNDARY
LOSS OF TREES TO CROSS OAK LANE



09 July 2021: TWO BUILDINGS - 2 UNITS, NEW ACCESS ROAD, CLOSER TO EMPIRE VILLAS BOUNDARY
RETAIN MORE TO CROSS OAK LANE, CAR PARK ADJACENT EMPIRE VILLAS



11 February 2021: TWO BUILDINGS - 3 UNITS, RETAIN EXISTING ACCESS, GAP TO EMPIRE VILLAS & CLOSER TO BOUNDARY, LOSS OF TREES TO CROSS OAK LANE & BONEHURST ROAD

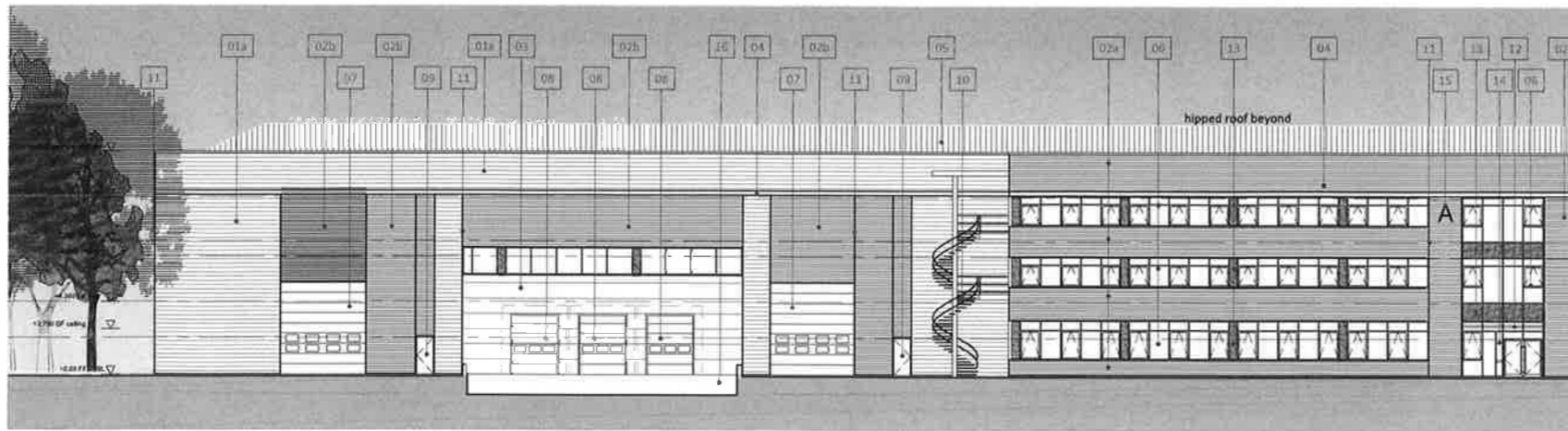


18 March 2021: TWO BUILDINGS - 3 UNITS, NEW ACCESS ROAD, CLOSER TO EMPIRE VILLAS BOUNDARY, RETAIN MOST OF THE TREES TO CROSS OAK LANE & BONEHURST ROAD

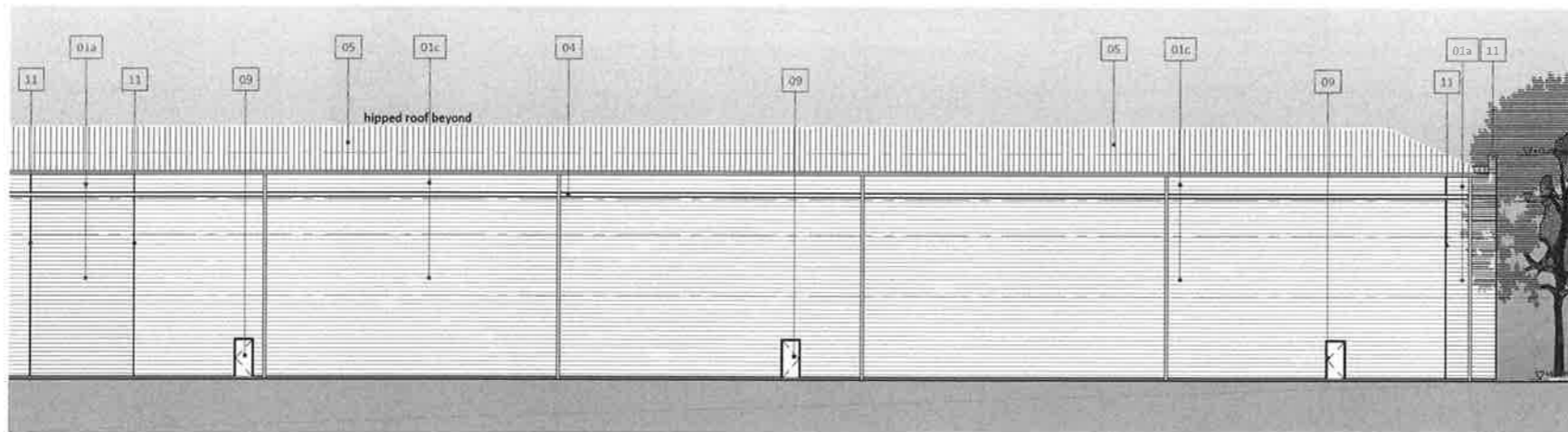


09 July 2021: THREE BUILDINGS - 3 UNITS, NEW ACCESS ROAD, GAP TO EMPIRE VILLAS &, LOSS OF FEWER TREES TO CROSS OAK LANE

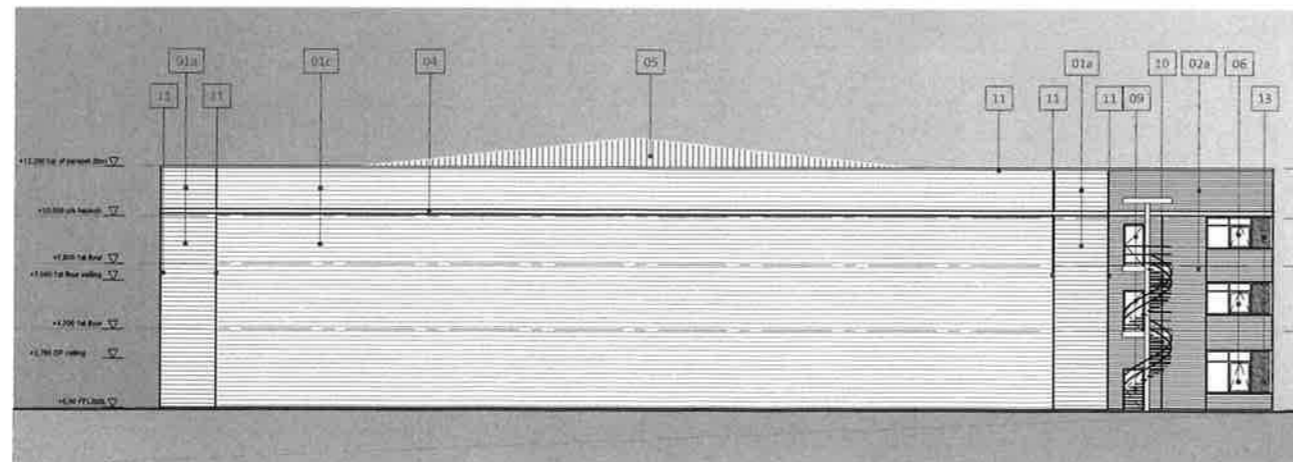
56



SOUTH ELEVATION - UNIT A



NORTH ELEVATION - UNIT A

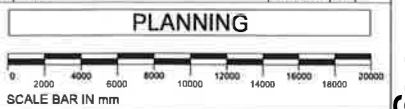


WEST ELEVATION - UNIT A

MATERIALS KEY:

- 01a Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Orion (tbc)
- 01b Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Atlantis (tbc)
- 01c Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Aquarius (tbc)
- 01d Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Sirius (tbc)
- 02a Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zeus (tbc)
- 02b Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Atlantis (tbc)
- 03 Wall cladding: Eurobond Europanel composite cladding cladding - colour Prisma Atlantis (tbc)
- 04 Feature channel flashings: Colour tbc
- 05 Roof cladding: Profiled built up steel cladding - colour: Goosewing Grey
- 06 Strip windows & curtain walling: Frame Colour: Anthracite (RAL 7016)
- 07 Level loading doors: Colour tbc
- 08 Dock leveller doors: Colour tbc
- 09 Metal escape & personnel door: Colour to match adjacent cladding
- 10 Galvanized steel spiral escape stair and roof
- 11 Metal flashings: Colour tbc
- 12 Glazed entrance canopy on ppc metal supports
- 13 Look-a-like glazing spandrel panels
- 14 Metal spandrel panel to match glazing with letter box
- 15 Unit & door numbers
- 16 Concrete dock retaining walls

P4	Sloped roof shown	23.06.2022	WR	WRA
P3	Building height lowered by 0.5m	16.06.2022	WR	WRA
P2	Rear parapet dropped and outboard gutter added	04.04.2022	WR	WRA
P1	Issued for Planning Approval	10.12.2021	WR	WRA
Rev	Notes	dd.mm.yyyy	By	Auth



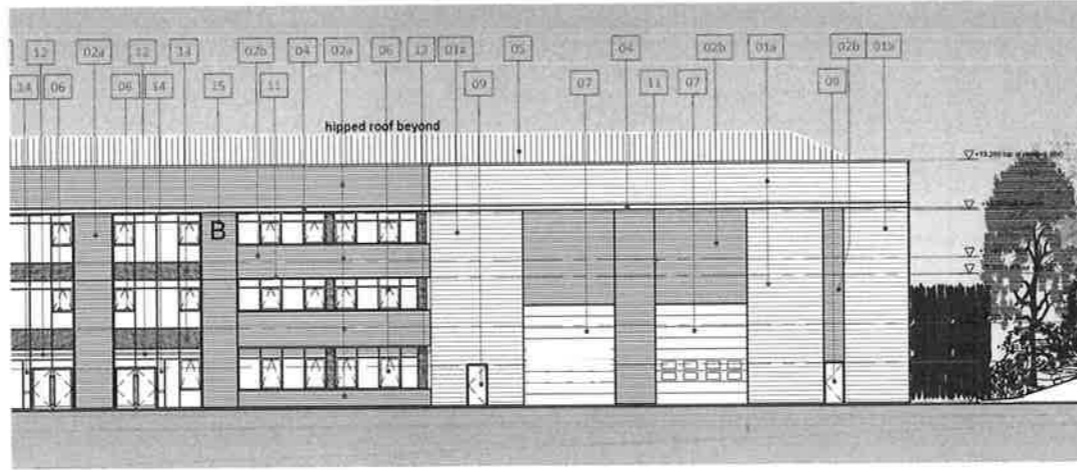
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**Cross Oak Lane
 Redhill RH1 5EX**

**Unit A
 Elevations**

Drawing No.: 2105-PL-1015 Rev: P4
 scale: 1:400 @ A3 / 1:200 @ A1 10 December 2021

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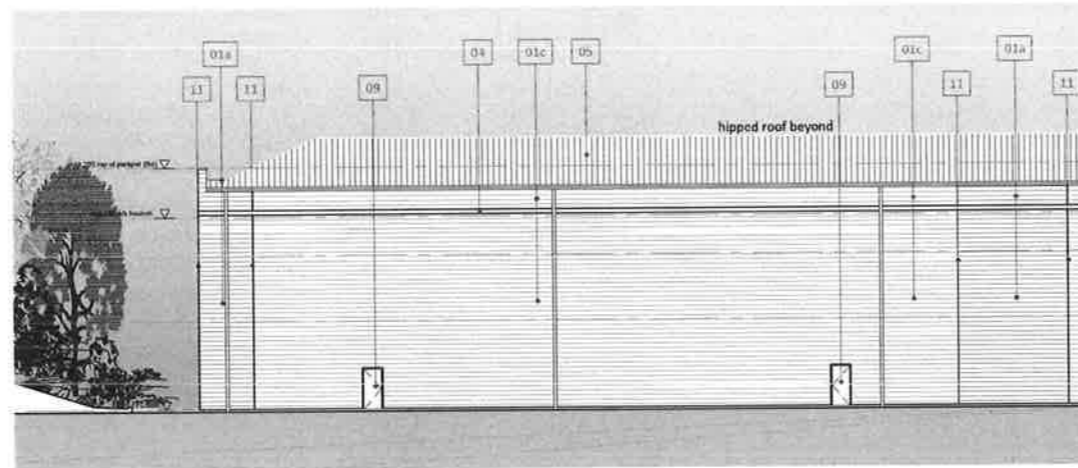
SOUTH ELEVATION - UNIT B

MATERIALS KEY:

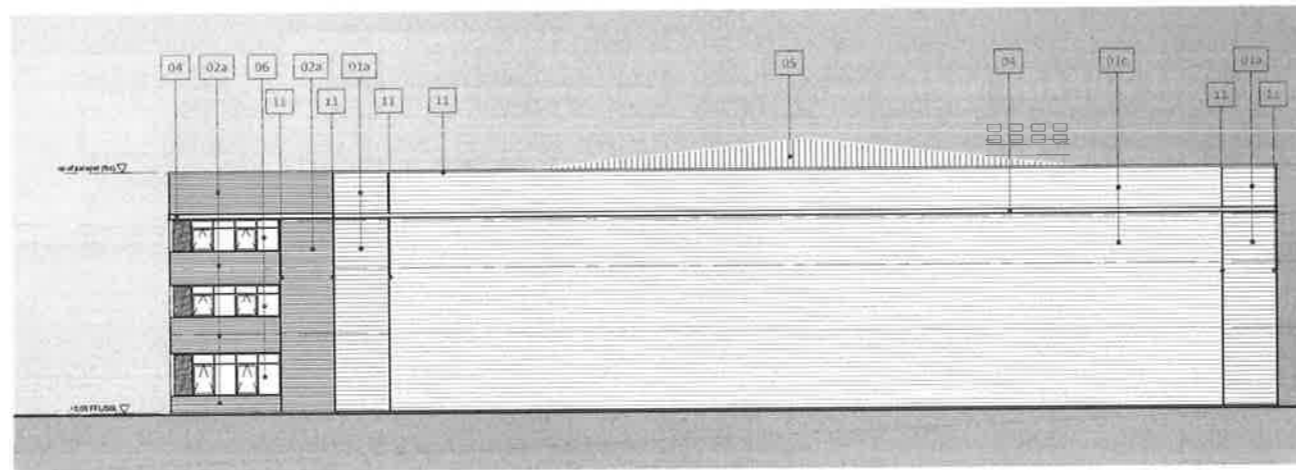
- △ Design 01 001'x
- △ Design 01 002'x
- △ Design 01 003'x
- △ Design 01 004'x
- △ Design 01 005'x

up steel cladding - colour Prisma Atlantis (tbc)

15
16 retaining all



NORTH ELEVATION- UNIT B



EAST ELEVATION - UNIT B

PLANNING

Cross Oak Lane
Redhill RH1 5EX

Unit B
Elevations

Drawing No.: 2105-PL- 1016

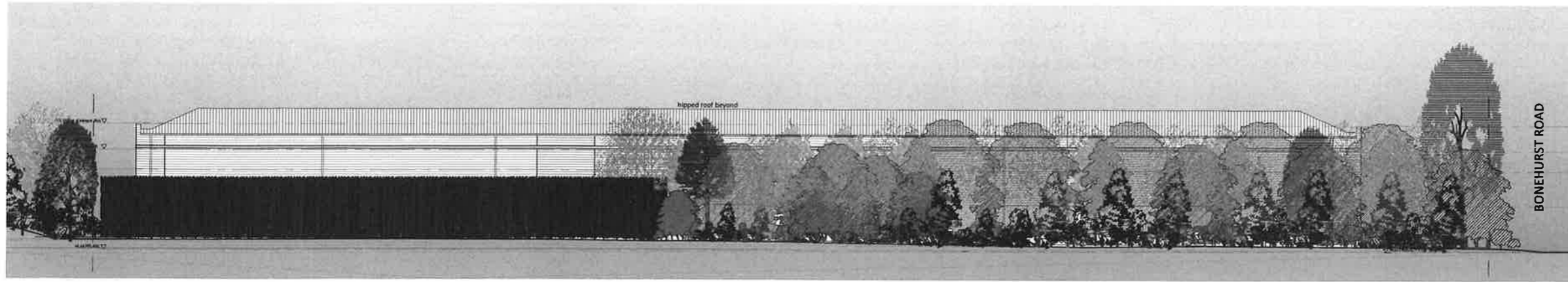
Rev: P4

P4	Sloped roof shown	23.06.2022	WR	WRA
P3	Building height lowered by 0.5m	16.05.2022	WR	WRA
P2	Rear parapet dropped and outboard gutter added	04.04.2022	WR	WRA
P1	Issued for Planning Approval	10.12.2021	WR	WRA
Rev	Notes	dd.mm.yyyy		

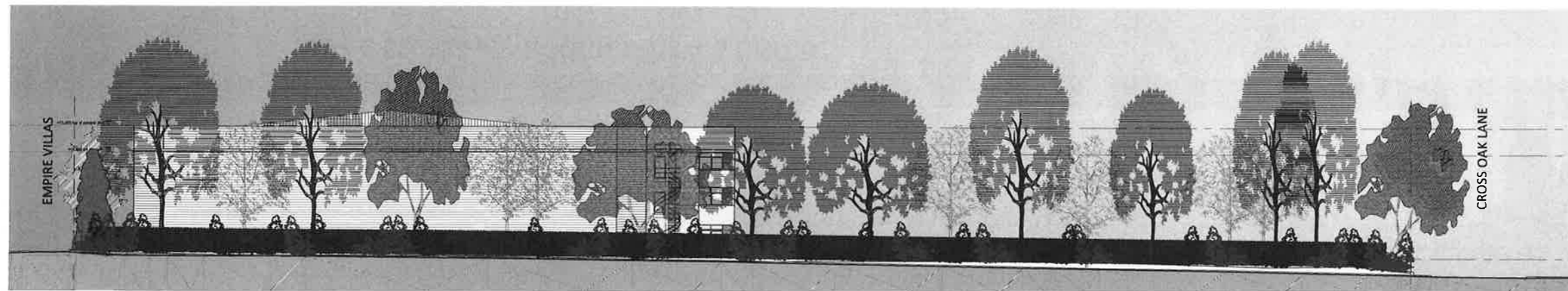


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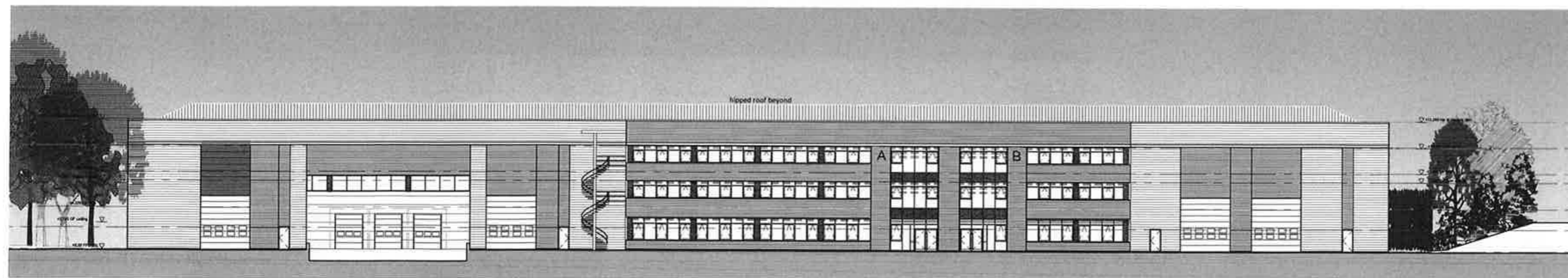
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1. SITE BOUNDARY ELEVATION - NORTH BOUNDARY (EMPIRE VILLAS)



2. SITE BOUNDARY ELEVATION - WEST BOUNDARY (BONEHURST ROAD)



3. FRONT ELEVATION UNITS A & B

P4	Slip at roof shown	23.05.2022	WR	WRA
P3	Building height lowered by 0.5m	16.05.2022	WR	WRA
P2	Rear parapet cropped and outboard gutter added	04.04.2022	WR	WRA
P1	Issued for Planning Approval	10.12.2021	WR	WRA
Rev	Notes	dd.mm.yyyy	By	Auth



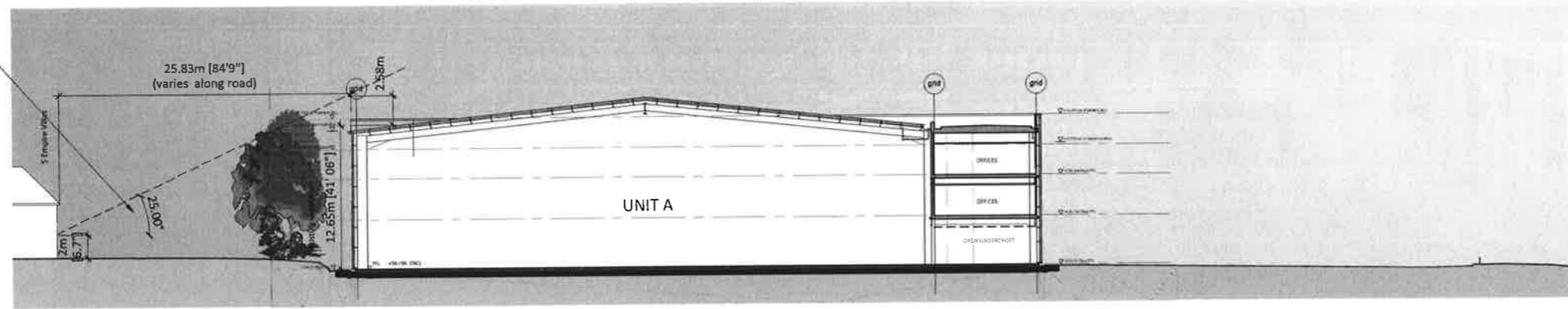
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Cross Oak Lane
Redhill RH1 5EX
 Site Boundary & Full Front Elevations

Drawing No.: 2105-PL-0021 Rev: P4
 scale: 1:500 @ A3 / 1:250 @ A1 10 December 2021

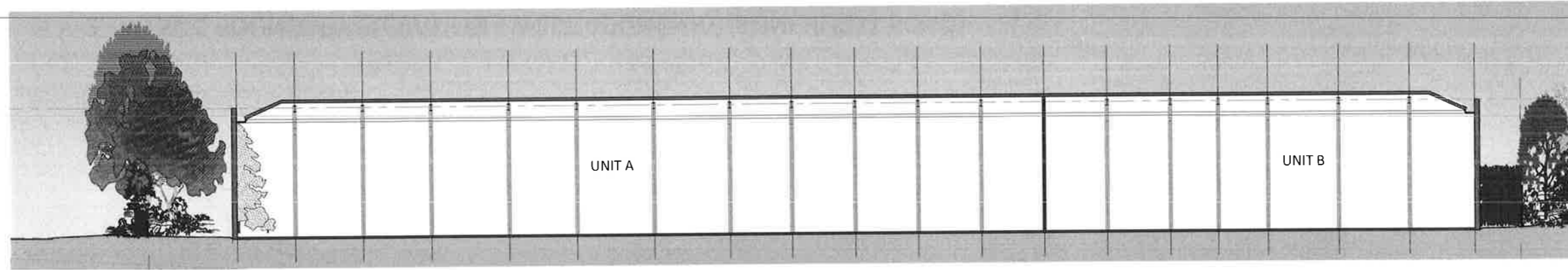
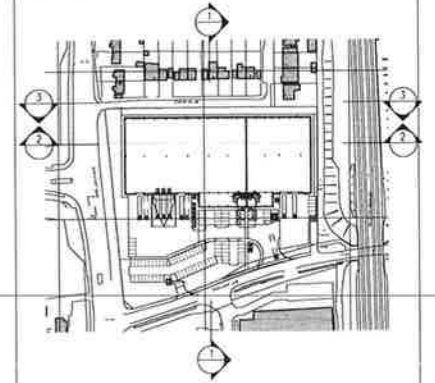
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Sight line as set out in Reigate and Banstead pre-application letter - 25 degrees at 2 metres above ground level where there are windows



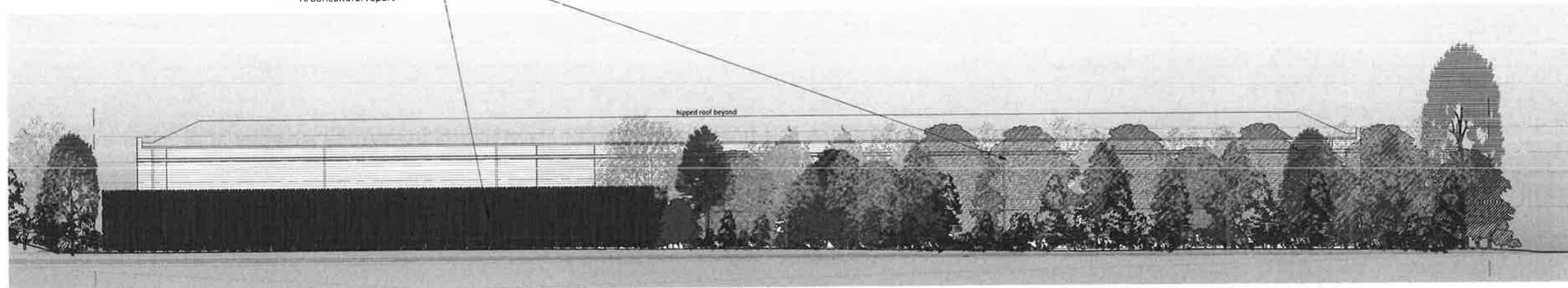
1. CROSS SECTION THROUGH UNIT A & 5 EMPIRE VILLAS

KEY PLAN:



2. LONG SECTION THROUGH UNIT A & UNIT B

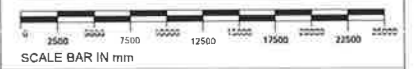
Note: varying height of existing landscaping based on information from the Arboricultural report



3. SITE BOUNDARY ELEVATION - NORTH BOUNDARY (EMPIRE VILLAS)

C	Building height lowered by 0.5m	23.06.2022	WR	WRA
A	Cladding changes to rear elevation to break scale	16.03.2022	WR	WR
Rev	Notes	dd.mm.yyyy	By	Auth

PRELIMINARY



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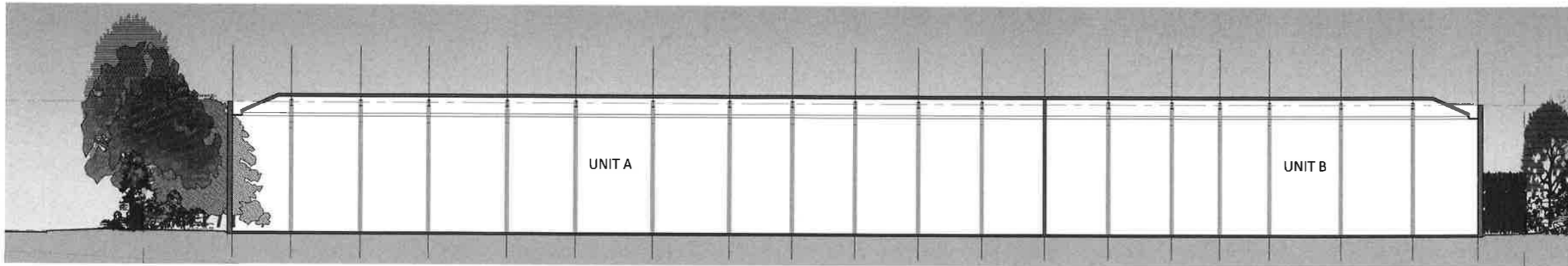
Cross Oak Lane
Redhill RH1 5EX

Site Sections & Boundary Elev.
to Empire Villas (no parapet)

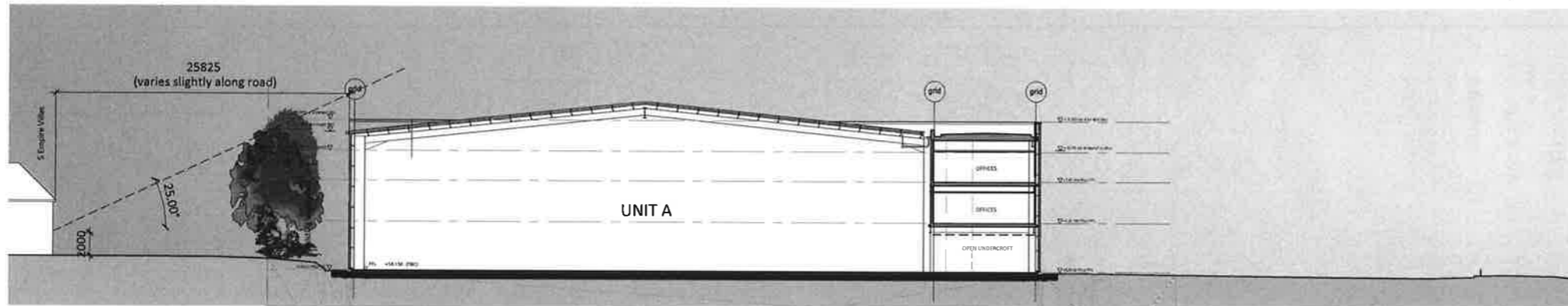
Drawing No.: 2105-SK-0023 Rev. C

scale: 1:500 @ A3 / 1:250 @ A1 07 March 2022

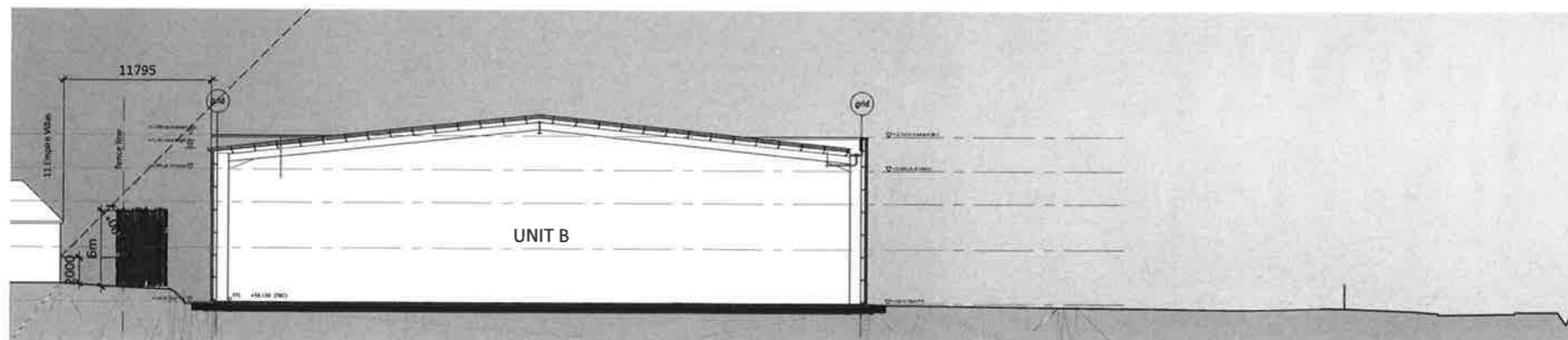
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1. LONG SECTION THROUGH UNIT A & UNIT B



2. CROSS SECTION THROUGH UNIT A & 5 EMPIRE VILLAS



3. CROSS SECTION THROUGH UNIT B & 11 EMPIRE VILLAS

61

P3	Building height lowered by 0.5m	16.05.2022	WR	WRA
P2	Rear parapet dropped and outward gutter added	04.04.2022	WR	WRA
P1	Issued for Planning Approval	10.12.2021	WR	WRA
Rev	Notes	dd.mm.yyyy	By	Auth

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Site Sections

Drawing No.: 2105-PL-0020 Rev: P3
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Agenda Item 5

Agenda Item 6

Planning Committee
6 July 2022

Agenda Item: 6
21/03215/F

 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	6 th July 2022
	REPORT OF:	HEAD OF PLANNING
	AUTHOR:	Hollie Marshall
	TELEPHONE:	01737 276010
	EMAIL:	Hollie.marshall@reigate-banstead.gov.uk
AGENDA ITEM:	6	WARD: Meadvale and St Johns

APPLICATION NUMBER:	21/03215/F	VALID:	21 December 2021
APPLICANT:	Aquinna Homes Plc	AGENT:	
LOCATION:	REDHILL AMBULANCE STATION PENDLETON ROAD REDHILL SURREY RH1 6JU		
DESCRIPTION:	Demolition of existing ambulance station and ancillary buildings, construction of 8 dwelling houses with associated access and parking. As amended on 31/01/2022 and on 30/05/2022.		
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SUMMARY

This is a full application for demolition of the existing ambulance station and ancillary buildings and the construction of 8 new houses with associated access and parking. The proposed houses include 2 x 2 bedroom houses, 2 x 3 bedroom house, 1 x 4 bedroom house and 3 x 5 bedroom houses. A total of 22 parking spaces would be proposed within the layout, this includes parking within garages.

Whilst the proposed development would result in the loss of the current Make Ready Centre(MRC)/ambulance station; a community facility and therefore the requirements of policy INF2 apply, the development is part of the wider MRC strategy, which includes the redevelopment of SECAMB's site in Banstead as an MRC and regional HQ. The Council has already granted planning permission for the new facility, and the Applicant has confirmed works are underway and at an advanced stage. As a result, it is agreed that there will be no loss of the ambulance service, and therefore the proposed closure of the ambulance station in Redhill would not result in a shortfall of local provision of this type and that improved provision is to be made in a suitable location. In this regard, there would be no conflict with DMP Policy INF2.

The pattern of development and plot sizes proposed would be commensurate with that of Cotland Acres, which wraps around the site to the east, west and north.

Agenda Item 6

Planning Committee
6 July 2022

Agenda Item: 6
21/03215/F

There would be variety in the design of the dwellings however a cohesive appearance in terms of the traditional style that would integrate well with that of Cotland Acres. The open area of land to the western side of the access road would help mitigate the visual impact of the proposal and compliment the design of the new development. This open area of land would include the retention of the protected veteran oak tree, with contributes highly to the visual amenities of the area. A tree protection condition, that must also include a management plan for the retained protected veteran oak tree, is recommended to secure its protection during and after works.

Amended plans have been secured through the application process to improve the relationship with neighbouring properties such that it is now considered acceptable in this regard.

The proposal would exceed the Council's parking standards as set out within the Development Management Plan. The County Highways Authority have raised no objection to the proposal subject to recommended conditions.

7

RECOMMENDATION(S)

Planning permission is **GRANTED** subject to conditions.

Consultations: -

Sutton and East Surrey Water Company: no comments received

Surrey Wildlife Trust: no objection subject to recommended conditions

Ramblers Association: no comments received

Highway Authority: The County Highway Authority has assessed the application on safety, capacity and policy grounds and has raised no objection subject to condition.

Contaminated Land Officer: There is some potential for contamination to be present on and or in close proximity to the applicant site. As such conditions to deal with contaminated land and an informative to provide additional guidance is recommended.

Representations:

Letters were sent to neighbouring properties on 22nd December 2021. Neighbours were re-notified on the revised plans for a 14 day period commencing 6th and 14th June 2022

6 responses have been received raising the following issues:

Issue	Response
Inadequate parking	See paragraph 6.19 – 6.20 and conditions 22 and 23
Increase in traffic and congestion	See paragraph 6.20
Hazard to highway safety	See paragraph 6.20
Inconvenience during construction	See paragraph 6.17 and condition 5
Out of character with surrounding area	See paragraph 6.5 – 6.11
Overdevelopment	See paragraph 6.10
Overbearing relationship and loss of outlook	See paragraph 6.12 – 6.15
Overshadowing	See paragraph 6.12 – 6.15
Overlooking and loss of privacy	See paragraph 6.12 - 6.16
Loss of / harm to trees	See paragraph 6.30 - 6.34 and condition 6
Noise & disturbance	See paragraph 6.18
Poor design	See paragraph 6.5 – 6.11
Drainage/sewage capacity	See paragraph 6.38 and conditions 17 and 18

Agenda Item 6

Planning Committee
6 July 2022

Agenda Item: 6
21/03215/F

Loss of private view

Not a material planning
consideration

1.0 Site and Character Appraisal

- 1.1 The application site is located on the north-western side of Pendleton Road and is currently occupied by the Redhill Ambulance Station, a large single storey building located centrally within the site with areas of parking and hardstanding around the building. The site has its own access directly from Cotland Acres which leads off Pendleton Road.
- 1.2 The area around the site is primarily in residential use although the southern side of Pendleton road is open forming part of Earlswood Common. The site has residential development to the north-west and south, in the form of two storey detached dwellings of relatively modern vintage.

2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: The applicant entered into pre-application discussions with the Council (PAM/20/00464). Advice was provided regarding layout and a reduction in the number of units (9 proposed).
- 2.2 Improvements secured during the course of the application: During the course of the application amendments have been sought to the layout, number of units, and scale to address concerns over impact on trees, neighbour amenity and character of the area.

3.0 Relevant Planning and Enforcement History

There is no relevant planning history. Planning history for land adjacent to the application site includes:

- | | | | |
|-----|------------|---|--|
| 3.1 | 88/07550/F | Pendleton Road Redhill land to the south of Earlswood mount adjacent to existing ambulance station | Approved with conditions 27 October 1988 |
| | | Demolition of existing house known as Kilbranan 60 new dwellings comprising of 2 bedroomed apartments three four and five bedroomed house | |

4.0 Proposal and Design Approach

- 4.1 This is a full application for demolition of the existing ambulance station and ancillary buildings and the construction of 8 new houses with associated access and parking. The proposed dwellings would comprise:

- Plot 1 – 4 bedroom 7 person 198sqm
- Plot 2 – 3 bedroom 5 person 109smq
- Plot 3 – 3 bedroom 5 person 124sqm
- Plot 4 – 2 bedroom 4 person 80sqm
- Plot 5 – 2 bedroom 4 person 80sqm
- Plot 6 – 5 bedroom 8 person 193sqm
- Plot 7 – 5 bedroom 10 person 192sqm
- Plot 8 – 5 bedroom 10 person 193sqm

- 4.2 A new ‘T’ shaped access road would be created into the site with three dwellings sited towards to west along the road, and then five dwellings sited around the end of the road towards the rear of the site. The dwellings would be a mix of detached and semi-detached and would be two storey in height. Plots 6, 7, and 8 would have accommodation in the roof space.
- 4.3 Plots 2, 3, 4 and 5 would have two parking spaces each, two of those would be within attached garages. Plots 1, 6, 7, and 8 would have three spaces each, one of those sited within a garage serving each of the dwellings. There would also be two visitor parking spaces within the site.
- 4.4 The dwellings would all have rear garden areas as well as small areas of landscaping to the front. The existing mature protected oak tree at the front of the site would be retained in an open area of land, outside the curtilage of any of the houses.
- 4.5 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:
Assessment;
Involvement;
Evaluation; and
Design.
- 4.6 Evidence of the applicant’s design approach is set out below:

Assessment	The character of the surrounding area is assessed as primarily of residential use, although the southern side of Pendelton Road is open forming part of Earlswood Common and the Redhill & Reigate Golf Course. The site has residential development to the north, west and south, in the form of two and three storey detached, semi and terraced form all of relatively modern vintage.
	Site features meriting retention are listed as the protected oak tree.

Agenda Item 6

Planning Committee
6 July 2022

Agenda Item: 6
21/03215/F

Involvement	No community consultation took place.
Evaluation	The statement does not include any evidence of other development options being considered.
Design	<p>The applicant's reasons for choosing the proposal from the available options were listed within the objectives for the development as follows:</p> <p>Creating positive identity, ensuring the development response to its topography. • Securing site boundaries by locating rear gardens adjacent existing boundary features and retaining structures. • Working with existing landscape and landscape assets by retaining and enhancing local features and adding new ones where possible. • Quality of life, with homes for local need, space to live and play, good access to facilities and a place for people to be proud</p> <p>The statement goes on to say:</p> <p>The Planning Layout intends to comprise the following key features: • 9 new homes over a development area of 0.32ha. • An average density of 28dph. • New area of open space adjacent to retained feature tree. • Enhance planting within the site. • Storm water drainage dealt with through S.U.D.S. design. • A mix of house types suitable for this part of the town.</p>

4.7 Further details of the development are as follows:

Site area	0.32 hectares
Existing use	Ambulance station Make Ready Facility
Proposed use	Residential
Existing parking spaces	20
Proposed parking spaces	22
Parking standard	20 minimum
Net increase in dwellings	8
Proposed site density	25 dwellings per hectare
Density of the surrounding area	30 dwellings per hectare [Cotland Acres]

5.0 Policy Context

5.1 Designation

Urban area
Tree Preservation Order RE574 - Oak
Parking standards – low accessibility

5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)
CS4 (Valued Townscapes and Historic Environment)
CS5 (Valued People/Economic Development),
CS10 (Sustainable Development),
CS11 (Sustainable Construction),
CS14 (Housing Needs)

5.3 Reigate & Banstead Development Management Plan 2019

Design, Character and Amenity (including housing)	DES1, DES4, DES5, DES8, DES9,
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Landscape & Nature Conservation	NHE2, NHE3, NHE4
Employment	EMP4
Infrastructure	INF3
Transport, Access and Parking	TAP1
Climate Change Resilience and Flooding	CCF1, CCF2

5.4 Other Material Considerations

National Planning Policy Framework
National Planning Practice Guidance
Supplementary Planning Guidance

Surrey Design
Local Distinctiveness Design Guide
Vehicle and Cycle Parking
Guidance 2018
Householder Extensions and
Alterations
Affordable Housing

Other	Human Rights Act 1998 Community Infrastructure Levy Regulations 2010 Conservation of Habitats and Species Regulations 2010
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6.0 **Assessment**

6.1 The application site is situated within the urban area where there is a presumption in favour of sustainable development and where the principle of such residential development is acceptable in land use terms.

Agenda Item 6

Planning Committee
6 July 2022

Agenda Item: 6
21/03215/F

6.2 The main issues to consider are:

- Principle of development
- Design appraisal
- Neighbour amenity
- Highway matters
- Amenity for future occupants
- Housing mix
- Biodiversity
- Impact on trees
- Sustainable construction
- Drainage
- Affordable Housing
- Community Infrastructure Levy
- Infrastructure contributions

Principle of development

6.3 The use of the site as an ambulance station Make Ready Centre (MRC) is considered to comprise a community use for the purposes of planning policy. Development Management Plan Policy INF2 relates to Community facilities and states as follows:

Loss or change of use of existing community facilities will be resisted unless it can be demonstrated that the proposed use would not have an adverse impact on the vitality, viability, balance of services and/or evening economy of the surrounding community; and

- a. Reasonable attempts have been made, without success, for at least six months to let or sell the premises for its existing community use or for another community facility that meets the needs of the community (see Annex 3 for details on what will be required to demonstrate this); or*
- b. The loss of the community facility would not result in a shortfall of local provision of this type, or equivalent or improved provision in terms of quantity and quality, or some wider community benefits, will be made in a suitable location.*

6.4 Whilst the proposed development would result in the loss of the current MRC/ambulance station, as described above, the development is part of the wider MRC strategy, which includes the redevelopment of SECAMB's site in Banstead as an MRC and regional HQ. The Council has already granted planning permission for the new facility, and the Applicant has confirmed works are underway and at an advanced stage. As a result, it is agreed that there will be no loss of the ambulance service, and therefore the proposed closure of the ambulance station in Redhill would not result in a shortfall of local provision of this type and that improved provision is to be made in a suitable location. In this regard, there would be no conflict with DMP Policy INF2.

Design appraisal

- 6.5 DMP Policy DES1 relates to the Design of New Development and requires new development to be of a high quality design that makes a positive contribution to the character and appearance of its surroundings. New development should promote and reinforce local distinctiveness and should respect the character of the surrounding area. The policy states that new development will be expected to use high quality materials, landscaping and building detailing and have due regard to the layout, density, plot sizes, building siting, scale, massing, height, and roofscapes of the surrounding area, the relationship to neighbouring buildings, and important views into and out of the site.
- 6.6 The proposal would see the demolition of the existing ambulance station and the erection of 8 dwellings. A new access road would be created from Cotland Acres into the site.
- 6.7 As you enter the site, three detached dwellings would be sited along the access road. During the course of the application amendments have been sought to reduce the scale of these dwellings to avoid a cramped appearance. The front facing dormers have been removed from plots 7 and 8 and reductions to the scale of these dwellings made so they now sit more spaciouly within the proposed plots. The dwellings would have detached garages between them, set back from the front elevations, creating visual separation between these houses.
- 6.8 At the rear of the site, four dwellings are proposed along the northern boundary, and one house towards the south western side boundary. The four at the rear of the site would comprise a pair of semi-detached houses. These would have a hipped roof, there would be a small section of flat roof, however this is relatively modest and not considered to result in a bulky appearance in this instance. There would also be a pair of linked detached houses, both would have attached garages to their western side. There is a change in levels, raising towards the north and east. The dwellings would follow the contours of the land, with plot 2 sited lower than 3, 4 and 5.
- 6.9 Plot 1 would be sited towards the south western side boundary of the site. This dwelling would face towards the access road, with the north western elevation being the front and the rear garden facing south eastwards. To the rear of plot 1, and to the western side of the access road, there would be an open area of land where the protected oak tree would be retained. This would provide a pleasant, spacious entrance to the development, which sits in close proximity to Earlswood Common. The oak tree is a very significant protected tree, and the layout has been amended to ensure the tree can be retained in the long term without future pressures to cut it back.
- 6.10 The pattern of development and plot sizes proposed would be commensurate with that of Cotland Acres, which wraps around the site to the east, west and north. There would be variety in the design of the dwellings however a

Agenda Item 6

Planning Committee
6 July 2022

Agenda Item: 6
21/03215/F

cohesive appearance in terms of the traditional style that would integrate well with that of Cotland Acres. The open area of land to the western side of the access road would help mitigate the visual impact of the proposal and compliment the design of the new development.

- 6.11 Overall, it is considered the proposed development would be acceptable in terms of its design and impact upon the character of the wider area and complies with policy DES1 of the Development Management Plan.

Neighbour amenity

- 6.12 The site is bounded by neighbours in Cotland Acres to the east, west and north. To the east of the site, the nearest neighbour is 18 Cotland Acres. This dwelling occupies a higher land level than the application site. During the course of the application amendments have been sought to the nearest plots to No. 18 (plots 4 and 5). The roof of these dwellings has been changed from a gable to a hipped roof. This results in less bulk at roof level for these dwellings, reducing the visual impact. There would be a separation distance of approximately 10m between these dwellings (at the closest point), however due to the change in levels and juxtaposition between the dwellings, the proposal is not considered to result in a harmful impact upon the amenities of this dwelling.
- 6.13 To the north of the site lies 16, 14 and 12 Cotland Acres, the nearest being No 16, orientated at 90 degrees to the application site. The rear elevations of plots 2, 3 and 4 would look towards the side elevation of this dwelling and the rear garden. The gardens of plots 2, 3 and 4 would be between 10.8m and 12.2m in length. In view of the level of separation and juxtaposition between the dwellings, the proposal is not considered to result in a harmful impact upon the amenities of this dwelling or those neighbouring at 14 and 12 Cotland Acres.
- 6.14 To the west of the site the neighbouring dwellings include 6 Cotland Acres. This dwelling would face towards the flank elevation of plot 2. During the course of the application amendments were sought to reduce the scale of this dwelling to avoid an overbearing or dominating impact. The level of the dwelling has been lowered and the roof of the main dwelling has been amended from a side facing gable to a front facing one. The roof the garage has been hipped. The amended plans are considered to have addressed initial concerns and the proposal is not considered to result in harmful an overbearing or dominating impact. Plans submitted show the proposal would pass the 25 degree assessment and therefore the proposal is not considered to result in a harmful impact in terms of loss of light.
- 6.15 To the west of plot 1 is 2 Cotland Acres. There would be a separation distance of approximately 6.5m between the dwellings and the new dwelling at Plot 1 would extend approximately 3m beyond the rear elevation of No. 2. Given the level of separation and relatively modest depth beyond the rear elevation of No. 2, this element of the proposal is not considered to give rise to a harmful impact upon neighbour amenity.

- 6.16 A condition is recommended to ensure that first floor side facing windows would be obscure glazed to avoid issues of overlooking and loss of privacy.
- 6.17 Objection has been raised from neighbouring properties regarding inconvenience during construction and noise and disturbance. Some inconvenience may occur during the construction of the proposal, however this is part and parcel of development and would not form a sustainable reason for refusal. Statutory nuisance legislation does however exist to control any significant harm that may occur and a construction method statement would be secured by planning condition were the application to be approved.
- 6.18 The proposed development may result in some additional noise and disturbance; however, the development would be in residential use and this would not be significant enough to warrant refusal of the application. Loss of a private view is not a material planning consideration.

Highway matters

- 6.19 The site is located in an area which is assessed as having a low accessibility rating. In such areas, the Council's adopted parking standards require the provision of 2 spaces for each 2 and 3 bedroom houses and 2.5 spaces for 4+ bedroom houses. The application's site layout would accord with the minimum parking standards and includes a total of 22 parking spaces. Annex 4 of the Council's DMP states garages and car ports counted towards parking provision must have minimum internal dimensions of 3.25 metres wide by 6 metres long. The garages proposed meet the minimum internal spaces standards and therefore count towards the parking provision, a condition is recommended to secure the retention of the garages for their designated purpose. The garages would create a row of three cars were future occupants to have three vehicles. This would mean residents having to manage their parking with car movements to allow cars in or out. Tandem parking is a typical layout for residential parking and the position of the garage to the rear of these two parking spaces is not untypical. Two visitor parking spaces are proposed, this would also accord with the minimum parking standards for a development of this size. Additional spaces parking spaces within the development would be at the loss of landscaping areas whereby additional areas of planting support the creation of habitat for wildlife. Given the proposal exceeds the minimum number of parking spaces, the proposal is considered acceptable in this regard.
- 6.20 The County Highway Authority has assessed the application on safety, capacity and policy grounds and has raised no objection subject to conditions. The CHA noted the proposed development includes adequate space for vehicles to be parked, exceeding minimum parking standards. Furthermore the site includes an adequate layout for refuse vehicles to enter and leave the site in forward gear.

Amenity for future occupants

Agenda Item 6

Planning Committee
6 July 2022

Agenda Item: 6
21/03215/F

- 6.21 The NPPF provides that planning decisions should provide a high standard of amenity for future users. DMP Policy DES5 relates to the delivery of high quality homes and requires, inter alia, that as a minimum, all new residential development (including conversions) must meet the relevant nationally described space standard for each individual units. In addition, the policy also requires all new development to be arranged to ensure primary habitable rooms have an acceptable outlook and where possible receive direct sunlight.
- 6.22 All the proposed units would exceed the minimum space standards. Habitable rooms would be served by front or rear facing windows providing light and outlook to these spaces. The dwellings would occupy reasonable plots with areas of outdoor amenity space and the proposal is therefore considered to comply with the requirements of policy DES5.

Housing mix

- 6.23 Policy DES4 requires on sites of up to 20 homes, at least 20% of market housing should be provided as smaller (one and two bedroom) homes. In this case two of the eight dwellings would be two bedroom houses which equates to 25%. On this basis, the proposal complies with the requirement.

Biodiversity

- 6.24 Trees, hedges and woodland areas make a particularly valuable contribution to the character and visual amenity of the borough, both in the townscape and the landscape. They can also be valuable for biodiversity, providing important habitats for local wildlife and as part of wildlife corridors. The National Planning Policy Framework (NPPF) makes it clear (para 170) that "Planning policies and decisions should contribute to and enhance the natural and local environment by; minimising impacts on and providing net gains for biodiversity, including by establishing coherent ecological networks that are more resilient to current and future pressures". Paragraph 174 requires the promotion of "the conservation, restoration and enhancement of priority habitats, ecological networks and the protection and recovery of priority species; and identify and pursue opportunities for securing measurable net gains for biodiversity".
- 6.25 The application was submitted with an Ecological Impact Assessment ref: AQU23390 Rev. B. Surrey Wildlife Trust were consulted upon the information and raise no objection to the proposal.
- 6.26 SWT note based on the survey, the presence of a bat roost in the building was assessed as being likely absent. A soprano pipistrelle roost was recorded in the veteran tree on the proposed development site, however, the Ecological Impact Assessment sets out that this tree will be retained and protected during all phases of the project. SWT strongly advise that the Applicant ensures the adequate protection of the tree, in line with the recommendations provided by ACD Environmental in the Ecological Impact Assessment.

- 6.27 SWT recommend conditions to secure a construction environmental management plan be submitted and an appropriately detailed landscape and ecological management. A further condition is recommended in regard to lighting.
- 6.28 The report also includes enhancement measures which include:
- Each new unit will support an integrated woodcrete swift *Apus apus* nesting box such as Schwegler Lightweight Sift box Type 1A
 - In addition to installing bird boxes, each new unit will be fitted with an integrated bat box.
 - Soft landscaping plans will use wildlife friendly planting throughout the scheme which wherever possible will prioritise native species.
 - Invertebrate boxes will be installed in sunny areas with pollinator friendly planting.
- 6.29 A further condition would be added to ensure the development is completed in accordance with these enhancement measures.

Impact on trees

- 6.30 There is a veteran oak tree nearby the front of the site which is protected by way of tree preservation order ref: RE574. During the course of the application concern was expressed over the impact upon this tree. This is a very significant protected tree, even so, there are issues with it. On the proposed layout the T1 tree is shown with part of its north side canopy overhanging more than half of the rear gardens of plots 1 & 2. This was not sustainable and will lead to understandable and reasonable requests to heavily reduce or even remove the tree – requests that may prove difficult to resist based on the drawing. Further to that though, even if the canopy is not so extensive, the tree will still sit on the south side of these plots and cast a lot of shade and inhibit sunlight and daylight to these houses for much of the day.
- 6.31 Another very important point on the location of these gardens is the condition of the tree. The tree has not been inspected by the LPA, but the tree survey details in this submission list many of the common features of a tree like this – historic large stem cavity, heartwood decay, fungal brackets. This is a tree that needs space to be properly retained without introducing new sensitive areas that it may fall down on. Veteran trees with hollow stems are well known for their durability but, they do also collapse and fall over. Not only that, it is also likely that before such an event happened that the new occupants of the plots next to the tree will be concerned about its condition and again make reasonable requests to remove it because it is regarded as ‘un-safe’. This is a serious point as the proposed layout will be introducing and keying up such a risk and this must be avoided instead.
- 6.32 Regarding the other areas of the site, it is pragmatic to remove the line of diseased ash trees along the southeast border of the site and replacing these with more suitable species will be a positive change. However, the proposed

Agenda Item 6

Planning Committee
6 July 2022

Agenda Item: 6
21/03215/F

layout plans do not show sufficient space to plant a line of compensatory trees to grow to maturity without likely conflict with the rear gardens and rear of the plots at this point. Any ash trees retained in this line as their crown condition does not have, or has less advanced, ash dieback may not be retained as such for long.

- 6.33 These concerns were communicated to the Applicant and an amended site layout plan was submitted. The new layout shows one dwelling where the previous two were proposed, and this dwelling has been sited further northwards, away from the tree, as well as the proposed rear garden boundary being moved northwards as well.
- 6.34 The Tree Officer has reviewed the amended site layout plan and has recommended a tree protection condition, this includes the requirement for the the Arboricultural Method Statement (AMS) to also include a management plan for the retained protected veteran oak tree. This management plan should be maintained by the future duty holder of the site and tree. The management plan must include details of a suitable inspection regime for the tree, any current and potential future works programs and be in accordance with recognized industry standards on veteran tree management

Sustainable construction

- 6.35 DMP Policy CCF1 relates to climate change mitigation and requires new development to meet the national water efficiency standard of 110litres/person/day and to achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations. However, this has been overtaken by the 2022 Building Rgeulations whereby Part L now requires a 30% improvement on emission rates compared to the previous standard, in effect superseding this aspect of the policy.
- 6.36 A condition is also recommended to ensure that each dwelling is fitted with access to fast broadband services in accordance with policy INF3 of the DMP.
- 6.37 Policy DES7 of the DMP requires that on sites of 5 or more homes at least 20% of homes should meet the Building Regulations requirements for 'accessible and adaptable dwellings'. The applicant has not referred to this requirement. Without any evidence to the contrary it is considered that such a requirement would be viable for the applicant and therefore a condition is recommended to secure adequate accessible housing in accordance with policy DES7.

Drainage matters

- 6.38 The site is in Flood Zone 1 and is not in an area identified as being at any significant risk of surface water flooding. A condition is recommended to secure details of the proposed drainage for the site and to make use of SuDS

so the development will not create an increased risk of flooding from surface water to the development site and the surrounding area.

Affordable Housing

- 6.39 Core Strategy Policy CS15 and the Council's Affordable Housing SPD require financial contributions towards affordable housing to be provided on housing developments of 1-9 units. However, in November 2014, the Government introduced policy changes through a Written Ministerial Statement and changes to the national Planning Practice Guidance which restrict the use of planning obligations to secure affordable housing contributions from developments of 10 units or less. These changes were given legal effect following the Court of Appeal judgement in May 2016.
- 6.40 In view of this, and subsequent local appeal decisions which have afforded greater weight to the Written Ministerial Statement than the Council's adopted policy, the Council is not presently requiring financial contributions from applications such as this resulting in a net gain of 10 units or less. The absence of an agreed undertaking does not therefore warrant a reason for refusal in this case.

Community Infrastructure Levy (CIL)

- 6.41 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, road, public transport and community facilities which are needed to support new development. This development would be CIL liable and the exact amount would be determined and collected after the grant of planning permission.

Infrastructure Contributions

- 6.42 In terms of other contributions and planning obligations, The Community Infrastructure Levy (CIL) Regulations were introduced in April 2010 which state that it is unlawful to take a planning obligation into account unless its requirements are (i) relevant to planning; (ii) necessary to make the proposed development acceptable in planning terms; and (iii) directly related to the proposed development. As such only contributions, works or other obligations that are directly required as a consequence of development can be requested and such requests must be fully justified with evidence. In this case, no such contributions or requirements have been requested or identified. Accordingly, any request for an infrastructure contribution would be contrary to CIL Regulation 122.

CONDITIONS

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Agenda Item 6

Planning Committee
6 July 2022

Agenda Item: 6
21/03215/F

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan Type	Reference	Version	Date Received
Street Scene	P1803.SS.01	F	09.06.2022
Section Plan	P1803.SEC.01	E	09.06.2022
Combined Plan	P1803.D1.04	G	09.06.2022
Elevation Plan	P1803.D1.03	C	09.06.2022
Site Layout Plan	P1803.02	F	09.06.2022
Location Plan	P1803.01	G	09.06.2022
Elevation Plan	P1803.H.02	B	30.05.2022
Proposed Plans	P1803.GAR.03		30.05.2022
Elevation Plan	P1803.H.03	B	30.05.2022
Proposed Plans	P1803.H.01	B	30.05.2022
Other Plan	P1803.GAR.02	B	30.05.2022
Elevation Plan	P1803.G.02	B	30.05.2022
Elevation Plan	P1803.F.03	B	30.05.2022
Proposed Plans	P1803.F.01	B	30.05.2022
Proposed Plans	P1803.GAR.01	B	30.05.2022
Proposed Plans	P1803.G.01	B	30.05.2022
Elevation Plan	P1803.F.02	B	30.05.2022
Elevation Plan	P1803.G.03	B	30.05.2022
Elevation Plan	P1803.C.02	B	30.05.2022
Elevation Plan	1803.A.02	B	30.05.2022
Proposed Plans	1803.A.01	B	30.05.2022
Elevation Plan	P1803.D.02	C	30.05.2022
Site Layout Plan	P1803.07	D	30.05.2022
Site Layout Plan	P1803.06	D	30.05.2022
Proposed Plans	P1803.C.01	B	30.05.2022
Proposed Plans	P1803.D.01	F	30.05.2022
Site Layout Plan	P1803.05	D	30.05.2022
Site Layout Plan	P1803.03	D	30.05.2022
Other Plan	P1803/DR/01		16.12.2021
Survey Plan	21/128/01		21.12.2021
Site Layout Plan	7500-EXT-001		21.12.2021

2

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

3. No development shall take place until the developer obtains the Local Planning Authority's written approval of details of both existing and proposed ground levels and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

Reason: To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Reigate and Banstead Development Management Plan DES1.

4. No development shall commence until a Construction Transport Management Plan, to include details of:
- (a) parking for vehicles of site personnel, operatives and visitors
 - (b) loading and unloading of plant and materials
 - (c) storage of plant and materials
 - (d) programme of works (including measures for traffic management)
 - (e) provision of boundary hoarding behind any visibility zones
 - (f) HGV deliveries and hours of operation
 - (g) vehicle routing
 - (h) measures to prevent the deposit of materials on the highway
 - (i) before and after construction condition surveys of the highway within the vicinity of the site and a commitment to fund the repair of any damage caused
 - (k) on-site turning for construction vehicles

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy DES8 Construction Management of the Reigate and Banstead Local Plan Development Management Plan September 2019.

5. No development shall commence until a Construction Management Statement, to include details of:
- a) Prediction of potential impacts with regard to water, waste, noise and vibration, dust, emissions and odours, wildlife. Where potential impacts are identified, mitigation measures should be identified to address these impacts.
 - b) Information about the measures that will be used to protect privacy and the amenity of surrounding sensitive uses; including provision of appropriate boundary protection.
 - c) Means of communication and liaison with neighbouring residents and businesses.
 - d) Hours of work.

Has been submitted to and improved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The condition above is required in order that the development is managed in a safe and considerate manner to help mitigate potential impact on the amenity and safety of neighbours and to accord with Reigate and Banstead Development Management Plan 2019 policy DES8.

6. No development shall commence, including any partial demolition or ground works preparation until a detailed, scaled and finalized Tree Protection Plan (TPP) and the related Arboricultural Method Statement (AMS) is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground

Agenda Item 6

Planning Committee
6 July 2022

Agenda Item: 6
21/03215/F

protection and any construction activity that may take place within the Root Protection Areas (RPAs) of trees, shown to scale on the TPP. This must include details of all service routes, materials and methodology for any excavation and construction within the RPA of retained trees and a schedule of arboricultural supervision and reporting. All works must be carried out in strict accordance with these details when approved.

The Arboricultural Method Statement (AMS) must also include a management plan for the retained protected veteran oak tree. This management plan should be maintained by the future duty holder of the site and tree. The management plan must include details of a suitable inspection regime for the tree, any current and potential future works programs and be in accordance with recognized industry standards on veteran tree management.

Reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3 and DES1 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard BS 5837:2012 'Trees in relation to design, demolition and construction – Recommendations'

7. No development shall commence on site until a scheme for the soft and hard landscaping (including hard surfacing and any street furniture), including details of existing landscape features to be retained or pruned, has been submitted and approved in writing by the local planning authority. The landscaping scheme shall include details of hard landscaping, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to first occupation of the approved development or in accordance with a programme agreed in writing with the local planning authority

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837. Trees in relation to construction.

Any trees shrubs or plants planted or any existing plants/hedging retained in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason: To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and Meath Green Conservation Area, and to comply with Reigate and Banstead Borough Development Management Plan 2019 policies NHE3 and DES1, British Standards including BS8545:2014 and British Standard 5837:2012.

8. The development shall be carried out in accordance with the impact avoidance, mitigation measures and enhancements set out within sections 6 and 7 of the Ecological Impact Assessment ref: AQU23390 rev. B dated 17.06.2022.

Reason: To ensure that any potential impact to protected species is adequately mitigated and to provide enhancements to the biodiversity of the site in accordance with the provisions of the National Planning Policy Framework and Reigate and Banstead Development Management Plan 2019 policy NHE2.

9. No development shall commence on site until an appropriately detailed Construction Environmental Management Plan (CEMP) has been submitted to and approved by the Local Planning Authority. The CEMP should include, but not be limited to:
- a) Map showing the location of all of the ecological features
 - b) Risk assessment of the potentially damaging construction activities
 - c) Practical measures to avoid and reduce impacts during construction
 - d) Location and timing of works to avoid harm to biodiversity features
 - e) Responsible persons and lines of communication
 - f) Use of protected fences, exclusion barriers and warning signs

Reason: To ensure that any potential impact to protected species is adequately mitigated in accordance with the provisions of the National Planning Policy Framework and policy NHE2 of the Development Management Plan 2019.

10. No development shall commence on site until an appropriately detailed Landscape and Ecological Management Plan (LEMP) has been submitted to and approved by the Local Planning Authority. The LEMP should include, but not be limited to:
- a) Description and evaluation of features to be managed
 - b) Ecological trends and constraints on site that might influence management
 - c) Aims and objectives of management
 - d) Appropriate management options for achieving aims and objectives
 - e) Prescriptions for management actions, together with a plan of management compartments
 - f) Preparation of a work schedule (including an annual work plan capable of being rolled forward over a five-year period
 - g) Details of the body or organisation responsible for implementation of the plan
 - h) Ongoing monitoring and remedial measures
 - i) Legal and funding mechanisms by which the long-term implementation of the plan will be secured by the applicant with the management body(ies) responsible for its delivery.
 - j) Monitoring strategy, including details of how contingencies and/or remedial action will be identified, agreed and implemented so that the development still

Agenda Item 6

Planning Committee
6 July 2022

Agenda Item: 6
21/03215/F

delivers the fully functioning biodiversity objectives of the originally approved scheme

Reason: To ensure that any potential impact to protected species is adequately mitigated in accordance with the provisions of the National Planning Policy Framework and policy NHE2 of the Development Management Plan 2019.

11. The developer must either submit evidence that the building was built post 2000 or provide an intrusive pre-demolition and refurbishment asbestos survey in accordance with HSG264 supported by an appropriate mitigation scheme to control risks to future occupiers. The scheme must be written by a suitably qualified person and shall be submitted to the LPA and must be approved prior to commencement to the development. The scheme as submitted shall identify potential sources of asbestos contamination and detail removal or mitigation appropriate for the proposed end use. Detailed working methods are not required but the scheme of mitigation shall be independently verified to the satisfaction of the LPA prior to occupation. The development shall then be undertaken in accordance with the approved details.

Reason: To ensure that a satisfactory strategy is put in place for addressing contaminated land before development commences and to make the land suitable for the development without resulting in risk to construction workers, future users of the land, occupiers of nearby land and the environment with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 and the NPPF.

12. Prior to commencement of development a written comprehensive environmental desktop study report is required to identify and evaluate possible on and off site sources, pathways and receptors of contamination and enable the presentation of all plausible pollutant linkages in a preliminary conceptual site model. The study shall include relevant regulatory consultations such as with the Contaminated Land Officer and be submitted to the Local Planning Authority and is subject to the approval in writing of the Local Planning Authority and any additional requirements that it may specify. The report shall be prepared in accordance with the Environment Agency's Land Contamination: Risk Management Guidance (2020) and British Standard BS 10175.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Development Management Plan 2019 policy DES9 and the NPPF.

13. Prior to commencement of development, in follow-up to the environmental desktop study, a contaminated land site investigation proposal, detailing the extent and methodologies of sampling, analyses and proposed assessment

criteria required to enable the characterisation of the plausible pollutant linkages identified in the preliminary conceptual model, shall be submitted to the Local Planning Authority. This is subject to the written approval in writing of the Local Planning Authority, and any additional requirements that it may specify, prior to any site investigation being commenced on site. Following approval, the Local Planning Authority shall be given a minimum of two weeks written notice of the commencement of site investigation works. Please note this means a proposal is required to be submitted and approved prior to actually undertaking a Site Investigation.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard the Reigate and Banstead Development Management Plan 2019 policy DES9 and the NPPF.

14. Prior to commencement of the development, a contaminated land site investigation and risk assessment, undertaken in accordance with the site investigation proposal as approved that determines the extent and nature of contamination on site and is reported in accordance with the standards of DEFRA's and the Environment Agency's Land Contamination: Risk Management Guidance (2020) and British Standard BS 10175, shall be submitted to the Local Planning Authority and is subject to the approval in writing of the Local Planning Authority and any additional requirements that it may specify. If applicable, ground gas risk assessments should be completed inline with CIRIA C665 guidance.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard the Reigate and Banstead Development Management Plan 2019 policy DES9 and the NPPF.

15. A. Prior to commencement of the development a detailed remediation method statement should be produced that details the extent and method(s) by which the site is to be remediated, to ensure that unacceptable risks are not posed to identified receptors at the site and details of the information to be included in a validation report, has been submitted to and approved in writing by the Local Planning Authority, and any additional requirements that it may specify, prior to the remediation being commenced on site. The Local Planning Authority shall then be given a minimum of two weeks written notice of the commencement of remediation works.

B. Prior to occupation, a remediation validation report for the site shall be submitted to the Local Planning Authority in writing. The report shall detail evidence of the remediation, the effectiveness of the remediation carried out and the results of post remediation works, in accordance with the approved remediation method statement and any addenda thereto, so as to enable future interested parties, including regulators, to have a single record of the remediation undertaken at the site. Should specific ground gas mitigation measures be required to be incorporated into a development the testing and verification of such systems should have regard to CIRIA C735 guidance

Agenda Item 6

Planning Committee
6 July 2022

Agenda Item: 6
21/03215/F

document entitled 'Good practice on the testing and verification of protection systems for buildings against hazardous ground gases' and British Standard BS 8285 Code of practice for the design of protective measures for methane and carbon dioxide ground gases for new buildings

Reason: To demonstrate remedial works are appropriate and demonstrate the effectiveness of remediation works so that the proposed development will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Development Management Plan 2019 policy DES9 and the NPPF.

16. Unexpected ground contamination: Contamination not previously identified by the site investigation, but subsequently found to be present at the site shall be reported to the Local Planning Authority as soon as is practicable. If deemed necessary development shall cease on site until an addendum to the remediation method statement, detailing how the unsuspected contamination is to be dealt with, has been submitted in writing to the Local Planning Authority. The remediation method statement is subject to the written approval of the Local Planning Authority and any additional requirements that it may specify.

Note: Should no further contamination be identified then a brief comment to this effect shall be required to discharge this condition

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Development Management Plan 2019 policy DES9 and the NPPF.

17. No development shall commence until a strategy for the disposal of surface and foul water is submitted to and approved in writing by the Local Planning Authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDs, NPPF and Ministerial Statement on SuDs The works shall be carried out in accordance with the approved details.

Reason: To ensure that the site is satisfactorily drained and in order to protect water and environmental quality with regard to Policy CS10 of the Core Strategy 2014, Policy CCF2 of the Development Management Plan 2019 and the NPPF.

18. Prior to the first occupation of the development a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority. This must demonstrate that the drainage system has been constructed as per the agreed scheme, or detail any minor variations, it must provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/area, flow restriction devices and outfalls).

The drainage system shall therefore be retained and maintained in accordance with the agreed details.

Reason: To ensure the drainage system is constructed to the national Non-Statutory Technical Standards for SuDs in order to mitigate against the risk of surface water flooding with regard to policy INF1 and CCF2 of the Reigate and Banstead Development Management Plan 2019.

19. No development shall take place above slab level until written details of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

20. No development above slab level shall take place until details setting out how the applicant will ensure that at least 20%, unless otherwise agreed in writing, of the homes meet the Building Regulations requirements for 'accessible and adaptable dwellings' have been submitted to and agreed in writing by the Local Planning Authority. The development shall be implemented in accordance with the agreed details.

Reason: In order that the scheme provides accessible housing in accordance with Reigate and Banstead Development Management Plan 2019 policy DES7

21. No part of the development shall be occupied unless and until 30 metres of the proposed access road has been provided in accordance with the approved plan numbered P1803.01 Rev B.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

22. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans numbered P1803.05 Rev B and 22 011 001 Rev A for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear, all shall be permanently retained and maintained for their designated purposes.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

Agenda Item 6

Planning Committee
6 July 2022

Agenda Item: 6
21/03215/F

23. The development hereby approved shall not be first occupied unless and until the proposed garages with minimum dimensions of 3.25 metres wide by 6 metres deep with 6 metres aisles in front of garages have been laid out within the site in accordance with the approved plans, all shall be permanently retained and maintained for their designated purposes.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

24. Prior to the occupation of the development a Travel Information Pack containing information on education, employment, retail and leisure uses within 2 km walking distance and 5km cycling distance of the site and by public transport shall be submitted for the written approval of the Local Planning Authority in accordance with the sustainable development aims and objectives of the National Planning Policy Framework, and Surrey County Council's "Travel Plans Good Practice Guide". And then the approved Travel Information Pack shall be distributed upon first occupation of each unit.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

25. The development hereby approved shall not be occupied unless and until each of the proposed dwellings are provided with a fast charge socket (current minimum requirements - 7 kw Mode 3 with Type 2 connector - 230v AC 32 Amp single phase dedicated supply) in accordance with a scheme to be submitted and approved in writing by the Local Planning Authority and thereafter retained and maintained to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

26. The development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected has been submitted to and approved in writing by the Local Planning Authority. The boundary treatment shall be completed before the occupation of the development hereby permitted.

Reason: To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the Reigate and Banstead Development Management Plan 2019 policy DES1 and NHE3

27. Prior to the first occupation of the development full details (and plans where appropriate) of the waste management storage and collection points, (and pulling distances where applicable), throughout the development shall be submitted to and approved in writing by the Local Planning Authority.

All waste storage and collection points should be of an adequate size to accommodate the bins and containers required for the dwelling(s) which they are intended to serve in accordance with the Council's guidance contained within Making Space for Waste Management in New Development.

Each dwelling shall be provided with the above facilities in accordance with the approved details prior to occupation of the relevant dwellings.

Reason: To provide adequate waste facilities in the interests of the amenities of the area and to encourage recycling in accordance with the Development Management Plan 2019 policy DES1.

28. The development hereby approved shall not be first occupied unless and until an Energy and Water Efficiency Statement has been submitted to and approved in writing by the Local Planning Authority. The Statement shall detail how the development will:
- a) Ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day
 - b) Achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations

The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

29. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:
- a) A broadband connection accessed directly from the nearest exchange or cabinet
 - b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Agenda Item 6

Planning Committee
6 July 2022

Agenda Item: 6
21/03215/F

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

30. No external lighting shall be installed on the building hereby approved or within the site until an external lighting scheme, which shall include indication of the location, height, direction, angle and cowling of lights, and the strength of illumination and timings/method of illumination, accompanied by a light coverage diagram, has been submitted to and agreed in writing by the local planning authority.

The external lighting shall be implemented in accordance with the approved scheme and be retained thereafter and maintained in accordance with the manufacturer's instructions.

Reason: To protect the visual amenity of the area, neighbouring residential amenities and to ensure that any potential impact to protected species is adequately mitigated with regard to Reigate and Banstead Core Strategy 2014 Policy CS10 and policy DES9 and NHE2 of the Reigate and Banstead Development Management Plan 2019.

31. The first floor windows in the side elevations of the development hereby permitted shall be glazed with obscured glass which shall be fixed shut, apart from a top hung opening fanlight whose cill height shall not be less than 1.7 metres above internal floor level, and shall be maintained as such at all times.

Reason: To ensure that the development does not affect the amenity of the neighbouring property by overlooking with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

32. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015, (or any Order revoking and re-enacting that Order with or without modification), no first floor windows, dormer windows or rooflights other than those expressly authorised by this permission shall be constructed.

Reason: To ensure that the development does not affect the amenity of the neighbouring property by overlooking and to protect the visual amenities of the area in accordance with Reigate and Banstead Development Management Plan policy DES1.

INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.info.

2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : [Climate Change Information](#).
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, to contact the Council's Neighbourhood Services team to confirm the number and specification of recycling and refuse bins that are required to be supplied by the developer. All developer enquires on recycling and refuse bin ordering, collections and discussing waste matters is via our department email address RC@reigate-banstead.gov.uk . Please also note our website area for developers https://www.reigate-banstead.gov.uk/info/20062/recycling_and_refuse/392/fees_for_recycling_and_refuse_services/3.
4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
 - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
 - (e) There should be no burning on site;
 - (f) Only minimal security lighting should be used outside the hours stated above; and
 - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - www.ccscheme.org.uk/index.php/site-registration.

5. The applicant is advised that the essential requirements for an acceptable communication plan forming part of a Method of Construction Statement are viewed as: (i) how those likely to be affected by the site's activities are identified and how they will be informed about the project, site activities and programme; (ii) how neighbours will be notified prior to any noisy/disruptive

Agenda Item 6

Planning Committee
6 July 2022

Agenda Item: 6
21/03215/F

work or of any significant changes to site activity that may affect them; (iii) the arrangements that will be in place to ensure a reasonable telephone response during working hours; (iv) the name and contact details of the site manager who will be able to deal with complaints; and (v) how those who are interested in or affected will be routinely advised regarding the progress of the work. Registration and operation of the site to the standards set by the Considerate Constructors Scheme (<http://www.ccscheme.org.uk/>) would help fulfil these requirements.

6. The applicant is advised that the Borough Council is the street naming and numbering authority and you will need to apply for addresses. This can be done by contacting the Address and Gazetteer Officer prior to construction commencing. You will need to complete the relevant application form and upload supporting documents such as site and floor layout plans in order that official street naming and numbering can be allocated as appropriate. If no application is received the Council has the authority to allocate an address. This also applies to replacement dwellings. If you are building a scheme of more than 5 units please also supply a CAD file (back saved to 2010) of the development based on OS Grid References. Full details of how to apply for addresses can be found [http://www.reigatebanstead.gov.uk/info/20277/street naming and numbering](http://www.reigatebanstead.gov.uk/info/20277/street_naming_and_numbering)
7. The use of a suitably qualified arboricultural consultant is essential to provide acceptable submissions in respect of the arboricultural tree condition above. All works shall comply with the recommendations and guidelines contained within British Standard BS 5837:2012 'Trees in relation to design, demolition and construction – Recommendations'
8. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above landscaping condition. The planting of trees and native hedging shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate structural landscape trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Advanced Nursery Stock sizes with initial planting heights of not less than 4.5m with girth measurements at 1m above ground level in excess of 16/18cm.
9. The applicant is advised that the development should seek to achieve standards contained within the Secured by Design award scheme to ensure that it results in a safe development.
10. Environmental Health would like to draw the applicant attention to the specifics of the contaminated land conditional wording such as 'prior to commencement', 'prior to occupation' and 'provide a minimum of two weeks notice'.

The submission of information not in accordance with the specifics of the planning conditional wording can lead to delays in discharging conditions, potentially result in conditions being unable to be discharged or even

enforcement action should the required level of evidence/information be unable to be supplied. All relevant information should be formally submitted to the Local Planning Authority and not direct to Environmental Health.

11. Notwithstanding any permission granted under the Planning Acts, no signs, devices or other apparatus may be erected within the limits of the highway without the express approval of the Highway Authority. It is not the policy of the Highway Authority to approve the erection of signs or other devices of a non-statutory nature within the limits of the highway.
12. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.

The permission hereby granted shall not be construed as authority to carry out any works (including Stats connections/diversions required by the development itself or the associated highway works) on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a permit and, potentially, a Section 278 agreement must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works (including Stats connections/diversions required by the development itself or the associated highway works) on the highway will require a permit and an application will need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see

<http://www.surreycc.gov.uk/roads-and-transport/permits-and-licences/traffic-managementpermit-> scheme. The applicant is also advised that Consent may be required under Section 23 of the Land Drainage Act 1991. Please see www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice.

REASON FOR PERMISSION

The development hereby permitted has been assessed against development plan policies DES1, DES4, DES5, DES8, DES9, NHE2, NHE3, NHE4, EMP4, INF3, TAP1, CCF1, CFF2 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

Agenda Item 6

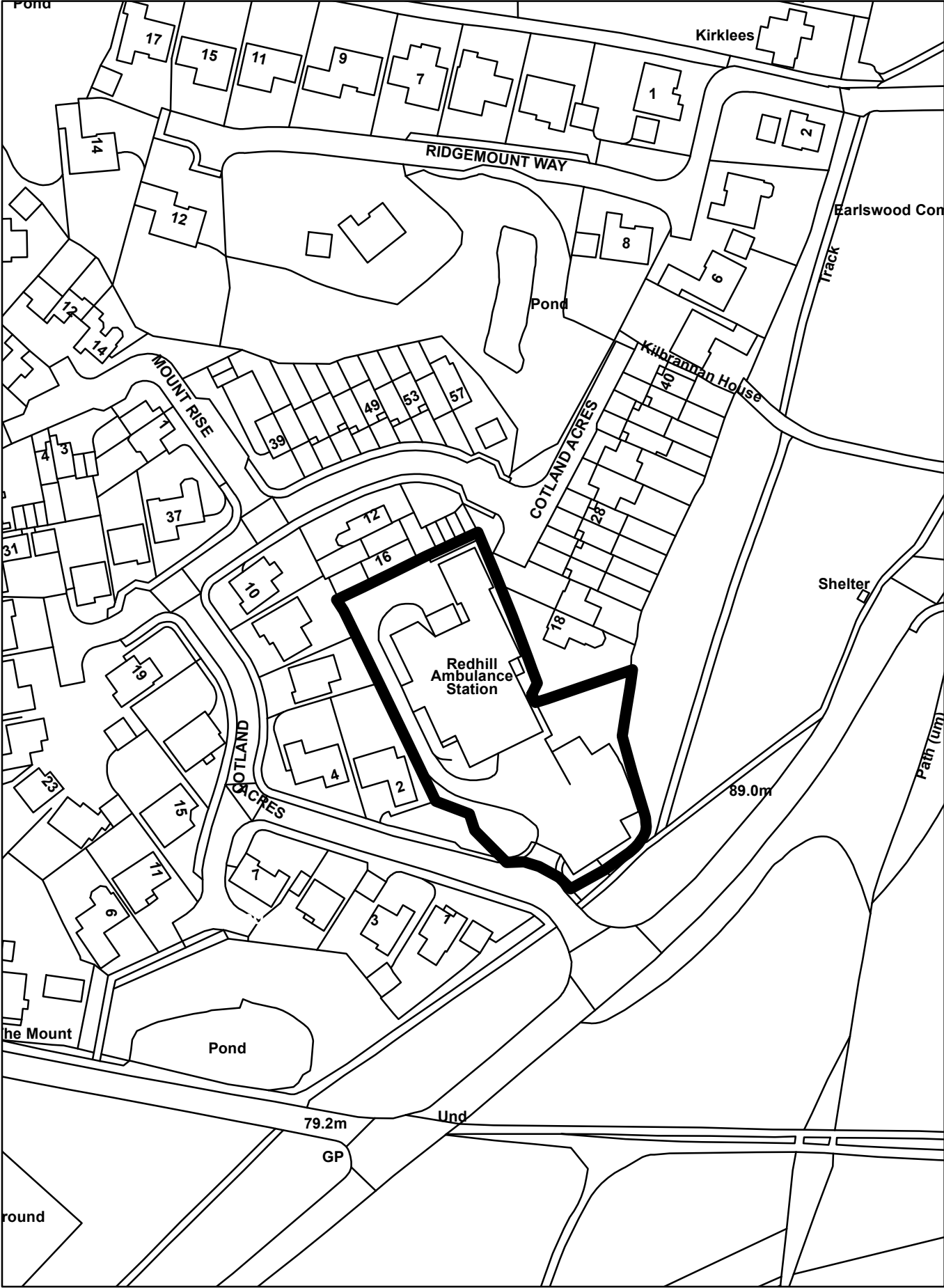
Planning Committee
6 July 2022

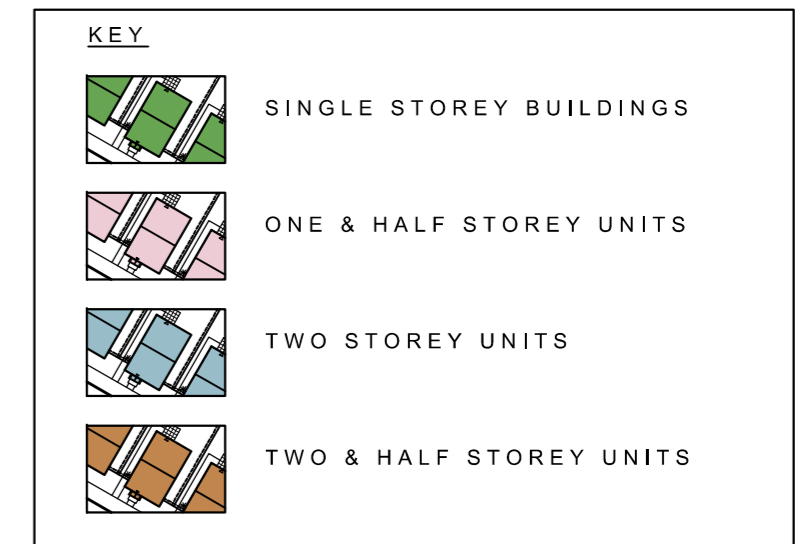
Agenda Item: 6
21/03215/F

Proactive and Positive Statements

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

21/03215/F Redhill Ambulance Station
Pendleton Road Redhill RH1 6JU





Rev.	Date	Description	Drawn	Checked
D	20-05-22	Updated in line with Planning Layout.	DGP	DGP
C	12-05-22	Updated in line with Planning Layout.	DGP	DGP
B	18-02-22	Updated in line with Planning Layout.	DGP	DGP
A	31-01-22	Updated in line with Planning Layout.	DGP	DGP

Client  

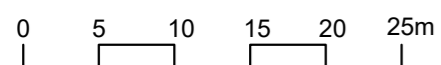
Project
**Redhill Ambulance Station, Pendleton Road,
 Redhill, Surrey RH1 6JU**

Drawing Title
Building Heights Layout

Scale **1:500 @ A2** Drawn **BAS** Checked **DGP**
 Date **July 2021** File **P1803 Layout** Drawing Status
 Drg.No. **P1803.03** Rev. **D** **PLANNING**

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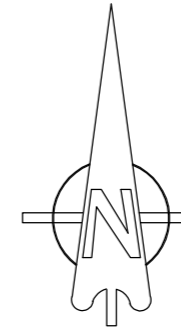
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SITE LAYOUT 1:500



SITE LAYOUT 1:500



KEY	
	Brick - Orange
	Tile hanging
	Tile - Red

Rev.	Date	Description	Drawn	Checked
F	08-06-22	Updated in line with Planning Layout.	VR	DGP
E	25-05-22	Updated in line with Planning Layout.	DGP	DGP
D	20-05-22	Updated in line with Planning Layout.	DGP	DGP
C	12-05-22	Updated in line with Planning Layout.	DGP	DGP
B	18-02-22	Updated in line with Planning Layout.	DGP	DGP
A	31-01-22	Updated in line with Planning Layout.	DGP	DGP

Client  

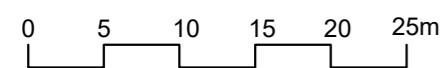
Project
**Redhill Ambulance Station, Pendleton Road,
 Redhill, Surrey RH1 6JU**

Drawing Title
Materials Layout

Scale **1:500 @ A2** Drawn **BAS** Checked **DGP**
 Date **July 2021** File **P1803 Layout** Drawing Status
 Drg.No. **P1803.02** Rev. **F** **PLANNING**

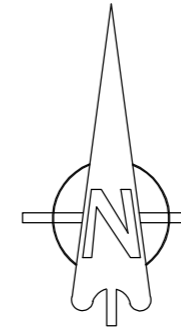
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SITE LAYOUT 1:500



REFUSE STRATEGY:

Private Refuse Storage for houses is located in rear gardens and moved by residents on collection day, to kerbside (on curtilage) or collection points.

Private Refuse Collection is from kerbside, adjacent to the dwelling boundary or from collection points.

Communal Refuse Storage is provided for apartments.

- Private refuse storage locations, generally in rear gardens. Moved on collection day by resident.
- Kerbside refuse collection points for individual houses.

Travel Distances

- - - - - Operatives - Generally 3-5m (25m max.)
- - - - - Residents - Generally <30m

Refuse Provision for Houses

- One 240 ltr. Wheeled Bin (Refuse)
- One 240 ltr. Wheeled Bin (Recycling)
- One 25 ltr. Kitchen Bin (Not Shown)

Rev.	Date	Description	Drawn	Checked
D	20-05-22	Updated in line with Planning Layout.	DGP	DGP
C	12-05-22	Updated in line with Planning Layout.	DGP	DGP
B	18-02-22	Updated in line with Planning Layout.	DGP	DGP
A	31-01-22	Updated in line with Planning Layout.	DGP	DGP

Client

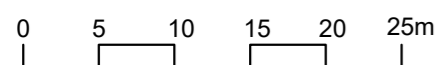
Project
**Redhill Ambulance Station, Pendleton Road,
 Redhill, Surrey RH1 6JU**

Drawing Title
Refuse Layout

Scale **1:500 @ A2** Drawn **BAS** Checked **DGP**
 Date **July 2021** File **P1803 Layout** Drawing Status
 Drg.No. **P1803.06** Rev. **D** **PLANNING**

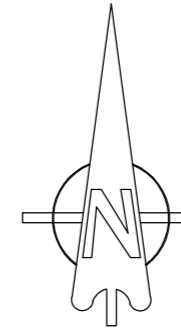
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SITE LAYOUT 1:500



KEY	
	14 ALLOCATED DRIVEWAY PARKING SPACE
	6 ALLOCATED GARAGE PARKING SPACE
	2 UNALLOCATED PARKING SPACE
22 Spaces Total	

Rev.	Date	Description	Drawn	Checked
D	20-05-22	Updated in line with Planning Layout.	DGP	DGP
C	12-05-22	Updated in line with Planning Layout.	DGP	DGP
B	18-02-22	Updated in line with Planning Layout.	DGP	DGP
A	31-01-22	Updated in line with Planning Layout.	DGP	DGP

Client  

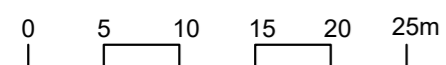
Project
**Redhill Ambulance Station, Pendleton Road,
 Redhill, Surrey RH1 6JU**

Drawing Title
Parking Layout

Scale **1:500 @ A2** Drawn **BAS** Checked **DGP**
 Date **July 2021** File **P1803 Layout** Drawing Status
 Drg.No. **P1803.05** Rev. **D** **PLANNING**

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KEY	
	1.8m CLOSE BOARD FENCE
	1.8m CLOSE BOARD FENCE ON EXISTING 2m RETAINING WALL
	GATE

Rev.	Date	Description	Drawn	Checked
D	20-05-22	Updated in line with Planning Layout.	DGP	DGP
C	12-05-22	Updated in line with Planning Layout.	DGP	DGP
B	18-02-22	Updated in line with Planning Layout.	DGP	DGP
A	31-01-22	Updated in line with Planning Layout.	DGP	DGP

Client  

Project
**Redhill Ambulance Station, Pendleton Road,
 Redhill, Surrey RH1 6JU**

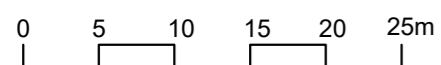
Drawing Title
Enclosures Layout

Scale **1:500 @ A2** Drawn **BAS** Checked **DGP**
 Date **July 2021** File **P1803 Layout** Drawing Status
 Drg.No. **P1803.07** Rev. **D** **PLANNING**

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SITE LAYOUT 1:500





PLOT 1
TYPE F



PLOT 2
TYPE D1

PLOT 3
TYPE D

PLOT 4
TYPE C

PLOT 5
TYPE C



PLOT 6
TYPE A

PLOT 7
TYPE G

PLOT 8
TYPE H

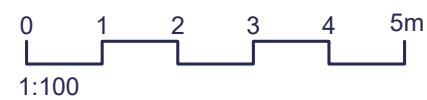
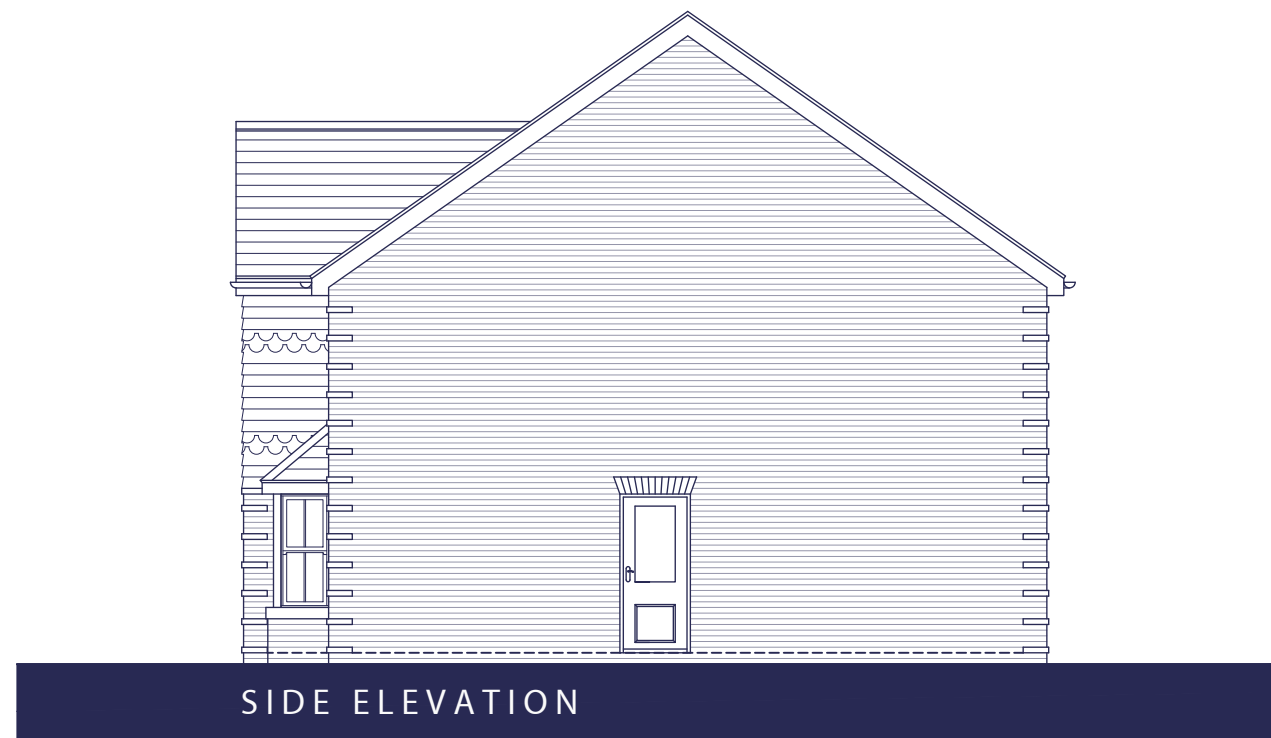
Rev.	Date	Description
F	08.06.22	Plot 2 amended
E	24.05.22	Plot 2 amended
D	23.05.22	Updated in line with planning layout
C	12.05.22	Updated in line with planning layout
B	05.05.22	Updated in line with planning layout
A	28.01.22	Updated in line with planning layout

Drawn	Checked
VR	DGP
VR	DGP
VR	DGP
VR	DGP
VR	DGP
VR	DGP

Client	AQUINNA HOMES	Scale	1:200 @ A3	Drawn	ACG	Checked	DGP
Project	Redhill Ambulance Station, Cotland Acres, Date Pendleton Road, Redhill, RH16JU.		September 2021	File	StreetScenes		
Drawing Title	StreetScenes	Drg.No.	P1803.SS.01	Rev.	F	Drawing Status	PLANNING


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100



Rev.	Date	Description
B	19.05.22	Bay windows and Tile Hanging added.
A	12.05.22	Updated in line with clients comments.

MCN	DGP
VR	DGP
Drawn	Checked

Client	 AQUINNA HOMES	Scale	1:100 @ A3	Drawn	MCN	Checked	DGP
Project	Redhill Ambulance Station, Cotland Acres, Date Pendleton Road, Redhill, RH16JU.		May 2022	File	Type F		
Drawing Title	Type F Front & Side Elevations	Drg.No.	P1803.F.02	Rev.	B	Drawing Status	PLANNING



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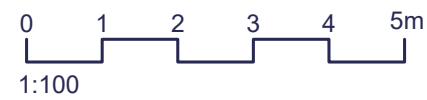
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REAR ELEVATION




SIDE ELEVATION



Rev.	Date	Description
B	19.05.22	Bay windows and Tile Hanging added.
A	12.05.22	Updated in line with clients comments.

MCN	DGP
VR	DGP
Drawn	Checked

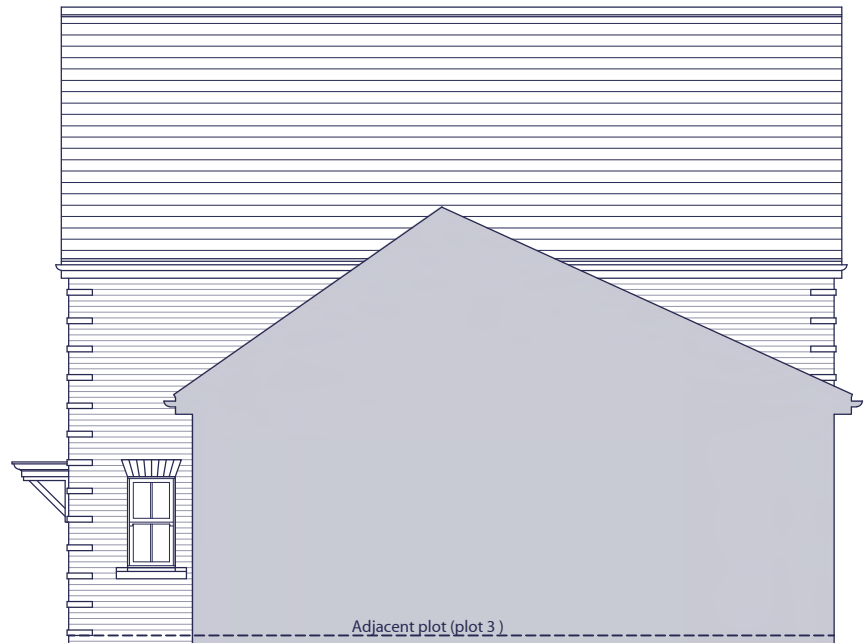
Client	 AQUINNA HOMES	Scale	1:100 @ A3	Drawn	MCN	Checked	DGP
Project	Redhill Ambulance Station, Cotland Acres, Pendleton Road, Redhill, RH16JU.	Date	May 2022	File	Type F		
Drawing Title	Type F Rear & Side Elevations	Drg.No.	P1803.F.03	Rev.	B	Drawing Status	PLANNING



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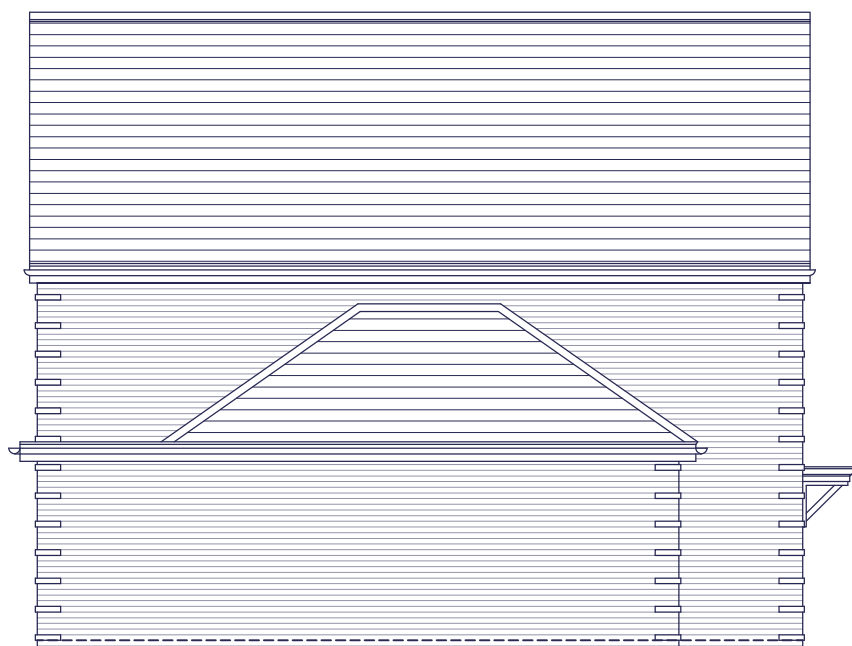
FRONT ELEVATION



SIDE ELEVATION

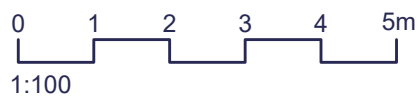


REAR ELEVATION



SIDE ELEVATION

102



Rev.	Date	Description
G	08.06.22	Garage roof amended.
F	24.05.22	Barn hip added
E	05.05.22	Roof amended to clients comments
D	27.01.22	Garage enlarged to 3.25m.
C	20.09.21	Grey roof changed to red.
B	13.09.21	Garage and home office increased. Rear door amended.
A	06.09.21	Bi-fold door added. 1st floor redesigned.

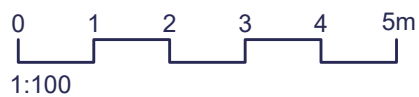
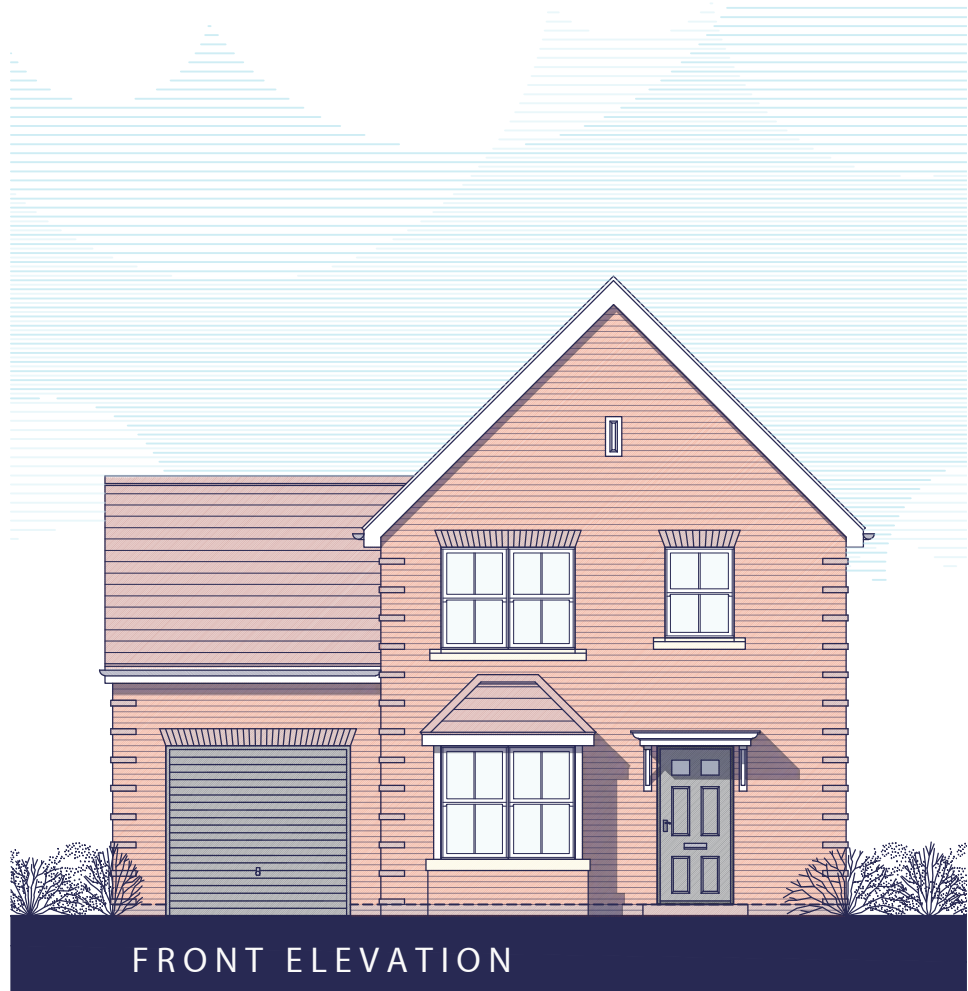
Drawn	Checked
VR	DGP
VR	DGP
VR	DGP
VR	DGP
ACG	DGP
ACG	DGP
ACG	DGP

Client	AQUINNA HOMES	Scale	1:100 @ A3	Drawn	ACG	Checked	DGP
Project	Redhill Ambulance Station, Cotland Acres, Pendleton Road, Redhill, RH16JU.	Date	August 2021	File	Type D		
Drawing Title	Type D1 Floor & Roof Plans	Drg.No.	P1803.D1.04	Rev.	G	Drawing Status	PLANNING



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103



Rev.	Date	Description
C	20.05.22	Bay added to front elevation.
B	05.05.22	Roof amended to clients comments
A	27.01.22	Garage enlarged to 3.25m.

Drawn	Checked
VR	DGP
VR	DGP
VR	DGP

Client	AQUINNA HOMES	Scale	1:100 @ A3	Drawn	ACG	Checked	DGP
Project	Redhill Ambulance Station, Cotland Acres, Pendleton Road, Redhill, RH16JU.	Date	September 2021	File	Type D		
Drawing Title	Type D Floor & Roof Plans	Drg.No.	P1803.D.02	Rev.	C	Drawing Status	PLANNING



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104



FRONT ELEVATION



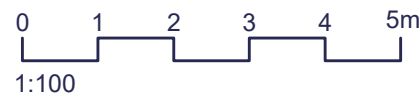
SIDE ELEVATION



REAR ELEVATION



SIDE ELEVATION



Rev.	Date	Description
B	19.05.22	Amended to clients comments
A	05.05.22	Roof amended to clients comments

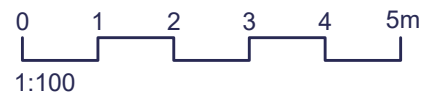
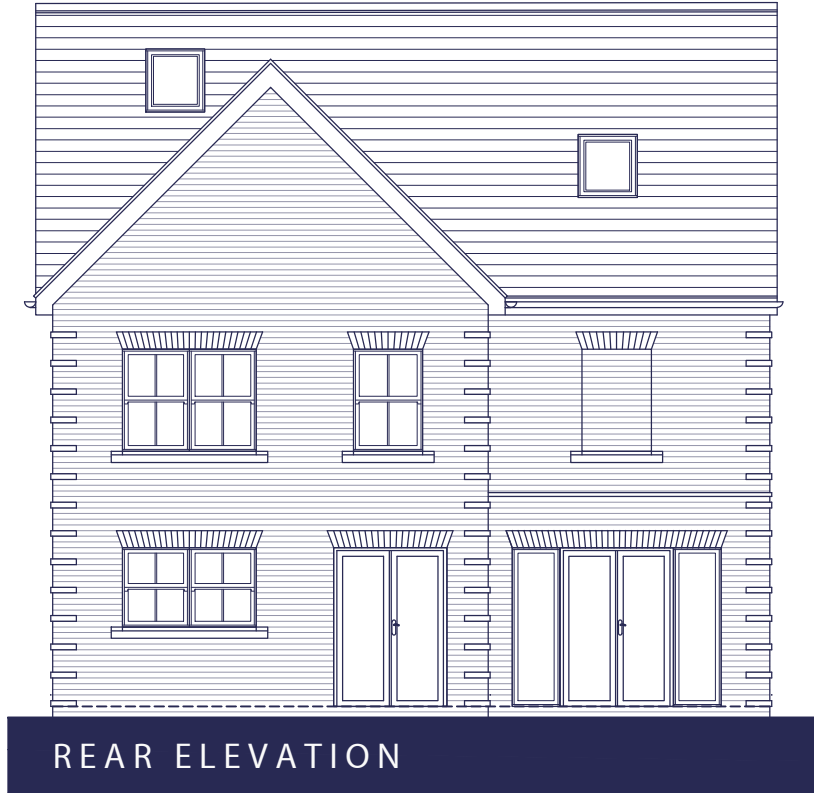
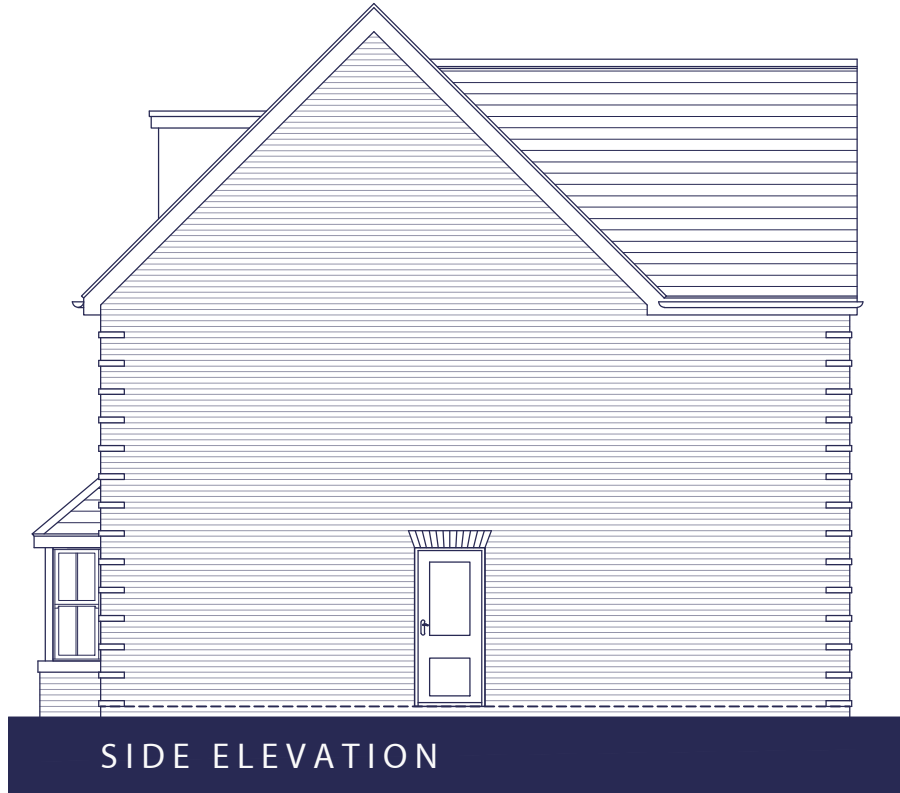
Drawn	Checked
VR	DGP
VR	DGP

Client	AQUINNA HOMES	Scale	1:100 @ A3	Drawn	ACG	Checked	DGP
Project	Redhill Ambulance Station, Cotland Acres, Date Pendleton Road, Redhill, RH16JU.	Date	August 2021	File	Type C		
Drawing Title	Type C Floor & Roof Plans	Drg.No.	P1803.C.02	Rev.	B	Drawing Status	PLANNING



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105



Rev.	Date	Description
B	20.05.22	Bays added to front elevation.
A	12.05.22	Updated in line with clients comments.

VR DGP
VR DGP
Drawn Checked

Client	AQUINNA HOMES	Scale	1:100 @ A3	Drawn	ACG	Checked	DGP
Project	Redhill Ambulance Station, Cotland Acres, Date Pendleton Road, Redhill, RH16JU.		August 2021	File	Type A		
Drawing Title	Type A Elevations	Drg.No.	1803.A.02	Rev.	B	Drawing Status	PLANNING



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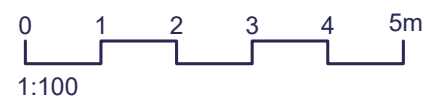
Agenda Item 6



FRONT ELEVATION



SIDE ELEVATION



Rev.	Date	Description
B	20.05.22	Updated in line with clients comments.
A	12.05.22	Updated in line with clients comments.

MCN	DGP
VR	DGP
Drawn	Checked

Client	 AQUINNA HOMES	Scale	1:100 @ A3	Drawn	MCN	Checked	DGP
Project	Redhill Ambulance Station, Cotland Acres, Pendleton Road, Redhill, RH16JU.	Date	May 2022	File	Types G & H		
Drawing Title	Type G Front & Side Elevations	Drg.No.	P1803.G.02	Rev.	B	Drawing Status	PLANNING



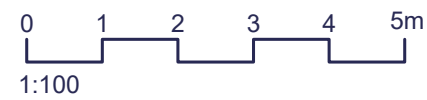
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REAR ELEVATION




SIDE ELEVATION



Rev.	Date	Description
B	20.05.22	Updated in line with clients comments.
A	12.05.22	Updated in line with clients comments.

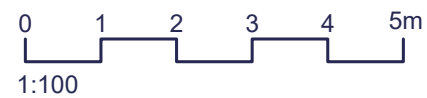
Drawn	Checked
MCN	DGP
VR	DGP

Client	 AQUINNA HOMES	Scale	1:100 @ A3	Drawn	MCN	Checked	DGP
Project	Redhill Ambulance Station, Cotland Acres, Pendleton Road, Redhill, RH16JU.	Date	May 2022	File	Types G & H		
Drawing Title	Type G Rear & Side Elevations	Drg.No.	P1803.G.03	Rev.	B	Drawing Status	PLANNING



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Rev.	Date	Description	Drawn	Checked
B	20.05.22	Updated in line with clients comments.	MCN	DGP
A	12.05.22	Updated in line with clients comments.	VR	DGP

Client	 AQUINNA HOMES	Scale	1:100 @ A3	Drawn	MCN	Checked	DGP	
Project	Redhill Ambulance Station, Cotland Acres, Date Pendleton Road, Redhill, RH16JU.		May 2022	File	Types G & H			
Drawing Title	Type H Front & Side Elevations	Drg.No.	P1803.H.02	Rev.	B	Drawing Status	PLANNING	

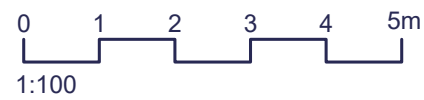
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REAR ELEVATION



SIDE ELEVATION



Rev.	Date	Description
B	20.05.22	Updated in line with clients comments.
A	12.05.22	Updated in line with clients comments.

Drawn	Checked
MCN	DGP
VR	DGP

Client	 AQUINNA HOMES	Scale	1:100 @ A3	Drawn	MCN	Checked	DGP
Project	Redhill Ambulance Station, Cotland Acres, Pendleton Road, Redhill, RH16JU.	Date	May 2022	File	Types G & H		
Drawing Title	Type H Rear & Side Elevations	Drg.No.	P1803.H.03	Rev.	B	Drawing Status	PLANNING



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Agenda Item 7

Agenda Item: 7

22/00557/F

Planning Committee
6th July 2022

 <p>Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate</p>	TO:	PLANNING COMMITTEE
	DATE:	6 th July 2022
	REPORT OF:	HEAD OF PLACES & PLANNING
	AUTHOR:	Matthew Sheahan
	TELEPHONE:	01737 276514
	EMAIL:	Matthew.sheahan@reigate-banstead.gov.uk
AGENDA ITEM:	7	WARD: Horley Central and South

APPLICATION NUMBER:	22/00181/F	VALID:	08/04/2022
APPLICANT:	Oakwood School	AGENT:	S & C Slatter
LOCATION:	OAKWOOD SPORTS CENTRE, BALCOMBE ROAD, HORLEY, SURREY, RH6 9AE		
DESCRIPTION:	<p>Creation of a Community 3G Football Turf Pitch (FTP) (11020 sq. metres) with associated features including: 3G football turf pitch (7460 sq. metres); 4.5m high ball stop fencing with entrance gates to form an enclosure around FTP perimeter; 1.2m high and 2.0m high pitch barriers with entrance gates internally within fenced FTP enclosure; 2.6m high maintenance equipment storage container (15 sq. metres) within fenced FTP enclosure; 15.0m high LED floodlights (6no.) around FTP perimeter; 4.0m high LED amenity light (1no.) along pedestrian access; Hard-standing areas for pedestrian access and circulation, portable goals storage, as well as vehicular access (807 sq. metres); 0.75m high grass flood defence crest around FTP perimeter (formed with recycled soils from the FTP construction (2753 sq. metres). (No change of use.) As amended on 21/04/2022 and on 13/05/2022.</p>		
<p>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</p>			

SUMMARY

This is a full application for the development of a new Community 3G Football Turf Pitch and associated works at Oakwood School in Horley. The pitch would facilitate both training and competitive football for both current and visiting school pupils, as well as be for the use of local teams, including women and girls teams. The pitch would be located to the west side of the existing school playing field and would cover an area of 7460 sq.m. The total area covered by the proposed development would be 11020m². The pitch would be enclosed by fencing a maximum of 4.5m in height. The fencing would be coloured black. A total of 6 floodlights would be installed around the pitch, installed on masts and would total 15m in height.

Agenda Item 7

Planning Committee
6th July 2022

Agenda Item: 7
22/00557/F

The site is located within an area of Urban Open Space as defined within the Reigate and Banstead Development Management Plan 2019 (DMP). Policy OSR1 of the DMP states that proposals which directly complement and enhance the value and use of the Urban Open Space for recreation, biodiversity and/or nature conservation will be looked upon favourably provided that the predominant open character of the space is maintained. The proposed 3G football pitch would facilitate the improvement of existing recreational provision for both Oakwood School and the wider community, including local football teams, who would be able to use the facility in the evenings and at weekends. Much of the existing playing field to the west would be kept open, with views across to the countryside beyond remaining. The proposed pitch would meet an identified need for such facilities in the borough and would cater for a wider a wider range of people. Sport England, a statutory consultee on such applications, have raised no objection and support the proposal.

The design of the facility would inevitably by its nature be functional and utilitarian in appearance, particularly the use of mesh fencing and the installation of floodlights. However the fencing would be painted in dark colours which would not stand out significantly against the green backdrop of the existing field, which would remain open. The use of fencing and integrated lockable gates is required for the security of the facility. The wider boundaries of the existing playing field are lined by mature trees and other vegetation which would assist in obscuring the facility to a degree and lessen visual impact on wider views of the field.

The facility would be located 64m from the shared rear boundary with properties located to the north side of Silverlea Gardens, located to the south of the playing field. Whilst it is accepted that the presence of the new pitch would result in change, the proposed lighting specification has been reviewed by the Councils' external consultants who consider that the proposed lighting would not have an adverse impact on the amenity of adjoining properties. Whilst there have been concerns raised with respect to noise impacts in view of the proposed hours of use (08:00-22:00 Monday to Sunday) the hours of use can be reviewed and controlled by a condition requiring the submission of a management plan for the facility prior to commencement of development.

The scheme would have a negligible impact with regard to trees and ecology matters. Biodiversity enhancements can be achieved for the wider school site, details of which could be secured by condition. The drainage scheme for the development has been reviewed by the Lead Local Flood Authority who have raised no objection subject to conditions requiring the submission of a finalised surface water drainage scheme for approval prior to commencement of development.

RECOMMENDATION(S)

Planning permission is **GRANTED** subject to conditions.

Consultations:

Sport England: No objection raised. This is discussed later in this report.

Surrey Wildlife Trust: Have reviewed the submitted Preliminary Ecology Appraisal and have raised no objections with regard ecological impacts. Advice is given that best practice be observed with regard to sensitive lighting. Measures should be put in place to allow wildlife to move across the site and create wildlife connectivity across the landscape. Biodiversity enhancement measures should be put in place in accordance with those set out within the submitted appraisal.

Regulatory Support Services: No objection raised with regards to impact of proposed lighting on neighbouring amenity. Concern has been raised with regard to noise impact on neighbouring residents to the south, and a condition restricting the hours of use has been recommended.

Horley Town Council: No objection raised to the proposed scheme however would wish to see rainwater discharging into the schools existing sewer system as a preference than into the drainage ditch that runs around the boundary of the playing field. This would allay residents' fears of the possible increase in flooding incidents for which this area has a record of events.

Surrey FA: Comment in support of the application on the basis that it would provide opportunity for the school to become a community hub for the local area which in turn will support grassroots football clubs. The new facility will achieve outcomes that will benefit the local footballing community, providing a fantastic facility for affiliated, recreational and underrepresented groups. It is noted that the project is a priority in Reigate and Banstead and is highlighted in the Local Football Facility Plan (LFFP).

Representations:

Letters were sent to neighbouring properties on the 11th February 2022. A total of 205 response have been received, 53 raising objection and 152 in support. Objection has been raised on the following issues:

Issue	Paragraph
Overdevelopment	Paragraph 6.7-6.8
Poor design	Paragraph 6.7-6.8
Overlooking and loss of privacy	Paragraph 6.9-6.13
Overshadowing	Paragraph 6.9-6.13
Health fears	Paragraph 6.21
Noise and disturbance	Paragraph 6.9-6.13
Loss of/harm to trees	Paragraph 6.14
Inadequate parking	Paragraph 6.15-6.18
Inconvenience during construction	Paragraph 6.15-6.18
Increase in traffic and congestion	Paragraph 6.15-6.18
Hazard to highway safety	Paragraph 6.15-6.18
Harm to wildlife habitat	Paragraph 6.19-6.21
Flooding	Paragraph 6.22-6.25

Agenda Item 7

Planning Committee
6th July 2022

Agenda Item: 7
22/00557/F

Drainage/ Sewerage capacity	Paragraph 6.22-6.25
Property Devalue	Paragraph 6.26
Crime Fears	Paragraph 6.26
Loss of private view	Paragraph 6.26
Harm to Conservation Area	Paragraph 6.26
Harm to Green Belt/countryside	Paragraph 6.7-6.8

A significant number of letters have been written in support of the application on the grounds of the community/ regeneration benefit it would bring.

Claire Coutinho MP for East Surrey has also written to express support for the application on the basis that the scheme would be of huge benefit to the wider community and will be a valuable asset for grassroots sports clubs.

1.0 Site and Character Appraisal

- 1.1 The site of the proposed 3G football pitch is within the campus of Oakwood School. The school is located within Horley to the east side of Balcombe Road. The area is predominantly residential in character, though the site is close to the edge of the Station Road Local Centre, which features a mix of land uses, including residential, retail and other commercial/ professional services, and light industrial. The main entrance to the school is accessed via Balcombe Road however there is a further access found on Smallfield Road to the north.
- 1.2 The proposed 3G football pitch is to be located to the southern part of the school site where currently the playing fields are located. A number of football and other pitches are located across the fields. To the south beyond the southern boundary of the site is located Silverlea Gardens, a residential street of detached and semi-detached houses.
- 1.3 The site is relatively flat throughout, with levels ranging between 56.45m and 56.60m. There is observed to be a marginal fall from west to east. The site is located entirely within Flood Zone 2, and the north-east section on the playing fields is prone to surface water flooding. There is an existing ditch network within the playing fields of Oakwood School. The closest of these is along the western edge of the proposed pitch. A watercourse is located along the eastern boundary of Oakwood School, which flows from south to north. The existing ditch network at the site outfalls to this adjacent watercourse. A number of trees of varying condition can be found immediately to the west of the site of the proposed pitch.

2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: Formal pre-application advice was not sought from the Local Planning Authority prior to the submission of the application.

- 2.2 Improvements secured during the course of the application: Improvements have not been sought during the course of the application as the principle of the proposed development is considered to be acceptable. Additional information related to trees, ecology and drainage has been received in response to comments raised by consultees on these matters.
- 2.3 Further improvements could be secured: Further improvements could be sought by way of conditions. A condition restricting the hours of use would be included.

3.0 Relevant Planning and Enforcement History

The planning history for the site is extensive however the most recent applications are as follows:

22/01357/CON	Non-material amendment to planning permission ref: RE21/02101/CON dated 16 December 2021 to allow alterations to the parking layout and cycle storage, the addition of raised kerbs, planting areas and a revised location for the electric vehicle charging	Pending Consideration
22/00095/CON	Details of a surface water drainage scheme; and method statement for carpark resurfacing, bicycle store and footpath submitted pursuant to Conditions 8 and 10 of planning permission RE21/02101/CON dated 16 December 2021	No Objection 04/02/22
21/02101/CON	Two-storey extension to school to provide additional classrooms and reorganisation of existing hard standing areas to provide parking and play space, including two additional parking spaces.	No Objection 26/08/2021

Agenda Item 7

Planning Committee
6th July 2022

Agenda Item: 7
22/00557/F

4.0 Proposal and Design Approach

4.1 This is a full application for the Creation of a Community 3G Football Turf Pitch (FTP) (11020 sq. metres) and associated features. These features are as follows: 3G football turf pitch (7460 sq. metres); 4.5m high ball stop fencing with entrance gates to form an enclosure around FTP perimeter; 1.2m high and 2.0m high pitch barriers with entrance gates internally within fenced FTP enclosure; 2.6m high maintenance equipment storage container (15 sq. metres) within fenced FTP enclosure; 15.0m high LED floodlights (6no.) around FTP perimeter; 4.0m high LED amenity light (1no.) along pedestrian access; Hard-standing areas for pedestrian access and circulation, portable goals storage, as well as vehicular access (807 sq. metres); 0.75m high grass flood defence crest around FTP perimeter (formed with recycled soils from the FTP construction (2753 sq. metres).

4.2 The pitch would cover an area of 7460 sq.m. Hard-standing areas around the facility for pedestrian access and circulation, as well as portable goals storage and vehicular access would account for 807sq.m., whilst the flood defence around the pitch would total 2753m². The total area covered by the proposed development would be 11020m².

4.3 The playing surface would be marked so as to provide different size pitches to cater for different specific needs and types of football being played. The pitch arrangement will be as follows:

- > 1no. 11v11 Youth U17/18 and Open Ages football pitch (100 x 64m).
- > 2no. 9v9 Youth U11 / U12 football pitches (63.8 x 45.9m each).
- > 2no. 7v7 Mini Soccer U9 / U10 pitches (55 x 37m each).
- > 4no. 5v5 Mini Soccer U7 / U8 pitches (37 x 27m each).
- > 4no. Training pitches (48 x 30m each).

Each of these different sized pitches would be marked in a different colour.

4.4 A total of 6 floodlights would be installed around the pitch, installed on masts and would total 15m in height. The lights would utilise modern LED technology with integral louvres to reduce light spill and back light as much as practically possible. Regarding materials the lights would be RAL7016 Anthracite Grey.

4.5 The circumference of the pitch would be enclosed by fencing of various types and heights. The maximum height of the fencing would be 4.5m. Various storage areas for portable goals and other equipment would be found around the pitch, as well as housing home and away team dugouts, waste bins, football boot cleaning stations. Pitch barriers would vary in height from between 1.2m-2m. The fencing would be of a weld mesh design comprising see-through steel mesh (polyester powder coated RAL9005 Jet Black) and this type is commonly installed around artificial sports pitches. The fencing would be coloured RAL9005 Jet Black. A maintenance equipment storage container would be installed on site to the west side of the pitch. This would be 2.529m high x 6.06m long x 2.44m wide finished in polyester powder coated RAL6005 Moss

Green. A new access path would lead from the pitch to join up with the main school site. This would be a new porous asphalt surface.

4.6 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:
Assessment;
Involvement;
Evaluation; and
Design.

4.7 Evidence of the applicant’s design approach is set out below:

Assessment	The site is identified as being located within located at Oakwood School, Balcombe Road, Horley. The proposed FTP is located within the wider site of Oakwood School, with school buildings to the West and North, and playing fields to the East and South. Oakwood School is bound by Langshott School to the North, dwellings to the North East, agricultural fields to the East, dwellings to the South, and dwellings to the West and North West with the B2036 Balcombe Road beyond. The site is currently grass playing fields associated with Oakwood School. The site is shown to be located in Flood Zone 2 and at a medium probability of fluvial flooding. The site is shown to be at a low risk of flooding from surface water and artificial sources such as reservoirs, and a medium risk of flooding from groundwater.
Involvement	The applicants sought the views of Surrey County Council and Sport England prior to submission of the application.
Evaluation	The statement does not include any evidence of other development options being considered.
Design	The scheme has been designed to respond to the pattern and character of development in the surrounding area in terms of quantum of development, layout, scale and, as far as is reasonably possible, protect or enhance the local landscape and the setting of the settlement/s. The scheme has been designed to be compatible with surrounding land uses and would be of a design that is functional and typical of facilities of this kind.

4.8 Further details of the development are as follows:

Agenda Item 7

Planning Committee
6th July 2022

Agenda Item: 7
22/00557/F

Site Area	1.102Ha
Existing Use	School (use class F1a) and Sports Centre (use class F2c)
Proposed Use	Community 3G Football Turf Pitch
Parking Accessibility Level	Accessibility level – High
Parking Standard	1 car space per 2 playing Participants
Current Parking Provision	150 standard car parking spaces, 3 accessible car parking spaces and 10 minibus spaces.
Flood Zone	2 (surface water flooding to the north-east corner).

5.0 Policy Context

5.1 Designation

Urban Area
Urban Open Space
Flood Zone 2

5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)
CS10 (Sustainable Development),

5.3 Reigate & Banstead Development Management Plan 2019

Design	DES1
Protecting and enhancing biodiversity	NHE2
Tree and landscaping	NHE3
Transport, access and parking	TAP1
Climate Change resilience and flooding	CCF1
Infrastructure	INF1
Community facilities	INF2
Outdoor Sport and recreation	OSR3

5.4 Other Material Considerations

National Planning Policy Framework
2021

National Planning Practice Guidance

National Design Guide

Supplementary Planning Guidance

Surrey Design

Local Distinctiveness Design Guide

A Parking Strategy for Surrey

Parking Standards for Development

Other

Human Rights Act 1998

Community Infrastructure Levy

Regulations 2010

6.0 **Assessment**

6.1 The application seeks planning permission for the creation of a Community 3G Football Turf Pitch (FTP) and associated works.

- Principle of the development
- Design and character assessment
- Neighbouring Amenity
- Trees and landscaping
- Transport matters
- Ecology
- Flooding
- Environmental Health

Principle of the development

6.2 The application site is within the urban area, where there is a presumption in favour of sustainable development and where the principle of development is accepted. Paragraph 92 of the National Planning Policy Framework 2021 (NPPF) requires planning policies and decision to enable and support healthy lifestyles, especially where this would address identified local health and well-being needs. This can be achieved through the provision of, amongst other things, sports facilities. Paragraph 93 states that, in order to provide social, recreational and cultural facilities and services the community needs, planning policies and decisions should: (a) plan positively for the provision and use of shared spaces, community facilities (such as local shops, meeting places, sports venues, open space, cultural buildings, public houses and places of worship) and other local services to enhance the sustainability of communities and residential environments. With regard to provision for open space and recreation, existing open space, sports and recreational buildings and land, including playing fields, should not be built on unless: the development is for alternative sports and recreational provision, the benefits of which clearly outweigh the loss of the current or former use (paragraph 99(c)).

Agenda Item 7

Planning Committee
6th July 2022

Agenda Item: 7
22/00557/F

- 6.3 The site is located within an area designated as Urban Open Space within the Councils' Development Management Plan 2019 (DMP). Policy OSR1 of the DMP states that proposals which directly complement and enhance the value and use of the Urban Open Space for recreation, biodiversity and/or nature conservation will be looked upon favourably provided that the predominant open character of the space is maintained. The proposed 3G football pitch would facilitate the improvement of recreational provision for both Oakwood School and the wider community, including local football teams, who would be able to use the facility in the evenings and at weekends. Whilst the area of the pitch would be large it would be relatively low scale and located to the western part of the existing playing field adjacent to the existing developed school site, with the remaining field to the south, west and north-east remaining open, with views across the open fields beyond the west boundary (located in the rural surrounds of Horley) remaining unobscured. On this basis it is considered that the requirements of Policy OSR1 are met.
- 6.4 Policy OSR3 of the DMP requires new development for new or upgraded provision for outdoor sports and recreation, including buildings, structures, synthetic pitches and play equipment to be of a scale that is appropriate to the location, be designed and sited to minimise visual obstruction, light pollution and noise and to ensure that the amenity of neighbouring properties would not be adversely affected and; not have an adverse effect on the features of nature conservation, geology and biodiversity value or landscape value character of the site. The proposed development of a new 3G football pitch would need to be found acceptable with regard to the above considerations, which will be discussed in more detail in the following sections of this report.
- 6.5 Sport England (SE), as a statutory consultee, has been consulted on the proposal. SE note that the location of the pitch would result in the loss of a playing field currently with an 11x11 equivalent sized pitch. There is also a training grid to the north of the football pitch that would also be affected by the proposal, and the existing running track can be remarked to the east of the proposed development. All other existing pitches can be retained. SE, as part of their assessment, has consulted national governing bodies for sport and comments have been received from the Football Foundation (FF) on behalf of the FA, and Rugby Football Union (RFU). The FF comments that, whilst noting the loss of one existing pitch, the benefits afforded by the proposed 3G FTP provides a significant enhancement of the provision. The remaining existing pitches would continue to be used. It is also noted that the site is listed in the Local Facilities Plan for the provision of a 3G pitch at this site. The development would cater for a range of opportunities, particularly the development of girls and women's football, with a number of girls and women's teams anticipated to make use of the facility across a range of ages. As well as hosting competitive football by clubs, the proposal would include recreational opportunities, including walking football sessions. The pitch is being procured via the FF Framework and is technically compliant with their requirements. SE go on to note that the emergent Reigate and Banstead Playing Pitch Study (PPS) identifies shortfalls in certain types of football provision and a lack of full size 3G football pitches within the borough. Consultation carried out with

football clubs shows that there are particular issues with clubs being able to access mid-week floodlit training slots due to a lack of available 3G provision. More teams could be run if such facilities were available. On this basis SE is satisfied that the facility would meet an identified need within the borough and has raised no objection subject to conditions requiring the submission of a community use agreement for approval prior to the commencement of development.

- 6.6 In light of the above considerations, the principal of providing a new 3G pitch would be acceptable and would meet the aims of the NPPF 2021 in this regard.

Design and character assessment

- 6.7 The design of the proposed pitch and associated works would be quite utilitarian in appearance which would be somewhat contrasting when compared to the open playing field, however the type of fencing materials would be quite typical for a facility of this kind. The pitch would be quite close to the existing school site which is similarly functional and much of the surrounding playing fields would remain. It is noted that the school has an existing, smaller Astroturf pitches which utilise similar fencing materials which have a similar impact to the proposal. Whilst the facility would be of quite a considerable size in terms of floor area it would not be disproportionate to the size of the field. The boundaries of the wider site are lined by mature trees and other vegetation that would reduce views to a degree from the wider area. The proposed fencing would be dark coloured either Jet black or Moss Green that would have a more subtle impact on the character of the field.
- 6.8 In light of the above it is considered that the design of the dwellings and the impact upon the character of the area would be acceptable and would comply with Policies DES1 and DES2 of the Development Management Plan 2019 and Character and Local Distinctiveness Design Guide.

Impact on neighbouring amenity

- 6.9 The nearest neighbouring residential properties are located to the south of the site along Silverlea Gardens. As stated earlier in the report these are a mix of detached and semi-detached dwellings, which are predominantly two storey in scale. The rear gardens of dwellings to the north side of Silverlea Gardens share a rear boundary with the school playing field, which is lined by trees and other vegetation. The distance between the proposed pitch and this boundary would approx. be 64m. whilst the distance to the rear of properties would be approx.. 93m. These properties contain rear facing windows at ground and first floor level serving habitable rooms, as well as gardens containing outbuildings, seating areas etc.
- 6.10 The main impacts of the proposed use are likely to arise from the use of the floodlights and noise generated while the facility is in use, particularly during the evening outside of school hours. The Council' has sought the advice of external consultants with regard to these matters. The submitted light assessment has been reviewed and no objection has been raised with regard

Agenda Item 7

Planning Committee
6th July 2022

Agenda Item: 7
22/00557/F

to the proposed lighting and impact on surrounding amenity. The lighting design has considered the appropriate environmental zone as set out in Guidance Note 1 of The Reduction of Obtrusive Light 2021 issued by the Institute of Lighting Professionals and is therefore deemed acceptable.

6.11 With regard to the impact of noise the following comments have been made:

‘The proposed hours of operation until 22:00 seven days a week are a concern. Currently outdoor sporting activities at the site do not extend to these hours. Therefore this would represent a significant intensification in activity at the site with no respite to local residents. The site monitoring detailed in the noise impact assessment shows the ambient noise levels decline significantly through the evening period. The noise assessment gives the noise levels from sporting activity measured at a site in Bristol in 2014. These are then compared with the site monitoring result to provide an assessment of impact of sport noise as an hourly averaged (LEQ) value. The values modelled are marginally above the measured background levels and therefore it is concluded that impacts are acceptable. However, experience of investigating complaints of noise from 3G pitches has shown that it is the number of individual maximum noise events (Lmax) – typically shouting, ball impacts and whistles- that cause complaint. This is principally due to the sporadic and unpredictable nature of this noise, as well as other factors such as foul language. This makes this noise acoustically distinctive compared to broadband sources such as road traffic more typical of the locality. The noise impact assessment measured Lmax noise levels as high as 86dB from sports activity, which would equate to noise significantly above the ambient noise levels measured in the evening period at the nearest residential receptor.

The principal difficulty with noise from team sports pitches is that it is difficult to mitigate this effectively. A boundary treatment such as acoustically absorbent fencing could be deployed but the site boundaries are extensive, and this is likely to be very costly. The simplest and most effective control is to limit the times the facility can be used. This would ensure a fair balance between the use of the facility by the wider community and the amenity of the local residents. This is for the LPA to decide but I would advise that use of the facility until 22:00 should be permitted only on one or two nights a week.’

6.12 The applicants propose to use the facility from 08:00 to 22:00 Monday-Sunday. As stated in paragraph 6.9 at the closest point the pitch would be approx.. 64m from the rear garden boundaries of properties to the north side of Silverlea Gardens. The level and type of potential noise disturbance as identified above taking place up until 10pm 7 Days a week is likely to result in a level of harm to the amenity and enjoyment of these neighbouring properties that would be unacceptable. In order that the facility will operate more reasonable hours of use that would not be unacceptably harmful to neighbour amenity, a condition requiring the submission of a management plan to agree proposed hours, amongst other matters, would form any granting of planning permission.

- 6.13 Subject to the aforementioned condition it is considered that the proposed development would not result in sufficient harm to the amenity of neighbouring properties to warrant refusal and would therefore comply with Policy DES1 of the DMP in this regard.

Trees and Landscaping

- 6.14 The site does not currently feature a large number of trees, however there are trees and hedgerows running along the western boundary of the site. There are no protected trees and there is some variance in terms of their quality. The Council's Tree Officers have been consulted on the application, who have reviewed the submitted arboricultural information as a desktop assessment. A request for additional information and justification for some of the proposed works to a number of the trees was made. Additional information was submitted in order to address these concerns/ questions, which have been deemed satisfactory. A condition requiring the submission of full tree protection details prior to commencement of development would be included in the event of planning permission being granted. Subject to compliance with the aforementioned conditions the proposal would be acceptable with regard to impact on trees and would comply with Policies DES1 and NHE3 of the DMP 2019.

Transport Matters

- 6.15 Pedestrian and vehicular access to the site is gained via Balcombe Road to the west and Smallfield Road to the north. These will continue to be utilised and it is not the intention to create any new access to the site. The existing car park at Oakwood School provides onsite car parking with approximately 150no. standard car parking spaces, 3no. accessible car parking spaces; 8no. coach parking spaces, and 10no. mini bus parking spaces. All parking areas allow adequate vehicle circulation and turning to enable vehicles to enter and exit in a forward gear. During the school day the facility would be used by pupils as well as visiting school teams, therefore the parking demand is likely to be negligible, and would be more applicable to use in the evening when in use by members of the community or local sports teams. The Council's non-residential parking standards, as contained within Annex 4 of the DMP, require field sports clubs to provide 1 parking space per 2 playing participants.
- 6.16 The planning statement submitted in support of the application sets out the following anticipated parking demands specific to the different ways that the facility would be used. The main periods of use will follow the football league seasons (typically during autumn and winter throughout September to April). Sports activities would also take place during the spring and summer catering for football training, development activities, summer clubs. The following sets out the maximum traffic demand.

WEEKDAY EVENING TRIPS (MID-WEEK EVENING TRAINING)

Agenda Item 7

Planning Committee
6th July 2022

Agenda Item: 7
22/00557/F

If players and coaches / managers all travel to site by car alone or are driven by parents (100% single occupancy travel), then maximum car parking demands are as follows:

- > Presuming the pitch may be used in halves for coaching / training / grassroots development (two teams simultaneously using the FTP);
- > Presuming each session lasts for one hour;
- > Presuming each team will include 20no. players including 2no. coaches – 22no. persons in total per team;
- > Then the maximum persons to use the pitch at any time will be 44no.;
- > Multiply by two for session change-over;
- > Then the maximum participants on site at any time will be 88no.;
- > This will result in a maximum accumulation of 88no. parked vehicles any one time within a one-hour period.

WEEKEND TRIPS (MORNING MATCHES)

If players and coaches / managers all travel to site by car alone or are driven by parents (100% single occupancy travel), then maximum car parking demands are as follows:

- > Presuming the pitch may be used in halves for mini-soccer matches (four teams simultaneously using the FTP);
- > Presuming each match lasts for forty minutes (2no. twenty-minute halves);
- > Presuming each team will include 10no. players and 2no. coaches (or similar) – 12no. persons in total per team;
- > Then the maximum participants on site at any time will be 96no.;
- > This will result in a maximum accumulation of 96no. parked vehicles any one time within a one-hour period; presuming match times are staggered adequately, say thirty to sixty minutes between matches; to prevent excessive parking accumulation.

There would be a cross over period of users arriving for and departing after each session, however it is expected that users would arrive no earlier than thirty minutes prior to the start of each session and then to depart within fifteen minutes from each session ending. Based on the maximum anticipated parking demand the level of existing parking provision at the site would be able to accommodate this.

- 6.17 It is expected that visitors/ users of the facility would be encouraged to make use of sustainable methods of transport e.g. public transport, cycling, car sharing/ car drop-off and collect system. Oakwood School would implement an Active Travel Plan, a final version of which could be secured by condition for approval of the local planning authority, to set out how this would be achieved.
- 6.18 In light of the above considerations the scheme is considered to comply with Policy TAP1 of the DMP 2019 with regard to highway safety, capacity and policy grounds and would therefore be acceptable.

Ecology

- 6.19 A Preliminary Ecology Appraisal has been submitted in support of the application, which has been reviewed by Surrey Wildlife Trust (SWT). The site is not subject to any specific ecology designations (SSSI, Site of Nature Conservation Importance or Special Area of Conservation), however given the presence of a number of trees and hedges in close proximity to the pitch, there is potential that any species present may be impacted by the development, particularly as a consequence of noise and the proposed lighting. The appraisal identifies that the existing trees and hedgerows provide potential nesting environments for birds, as well as for foraging and commuting bats. The existing ditch provides aquatic habitat for Great Crested Newts in all areas that were filled with water, though the survey identifies there being a low probability of Great Crested Newt presence.
- 6.20 In reviewing the appraisal SWT have recommended that, prior to commencement of development, a Sensitive Lighting Management Plan be submitted for approval prior to commencement of development. Informatives advising the applicants of a legal requirement to apply for a Great Crested Newt licence should any presence be identified and the need to cease works immediately, and of need to keep landscape connectivity during the course of development would be included should the application be approved.
- 6.21 Subject to compliance with these conditions and informatives the development would comply with Policy NHE2 of the Development Management Plan 2019.

Flooding and Drainage

- 6.22 The site of the proposed pitch would be located entirely within Flood Zone 2 (FZ2). The north-east section of the playing field is within Flood Zone 3 and is prone to surface water flooding, however the pitch would be outside of this area. The facility would be classed as 'Water Compatible Development' as defined within the Planning Practice Guidance for Flood Risk (PPG) and Annex 3 of the NPPF 2021. As such any proposed development in FZ2 would need to following the standing advice as contained within the PPG. Development proposals located within FZ2 would be expected to carry out a sequential test in accordance with the NPPF and PPG. Development proposals must avoid areas at risk of flooding where possible and prioritise development in areas with the lowest risk of flooding. Development should not be permitted if there are reasonably available sites appropriate for the proposed development in areas with a lower risk of flooding. The strategic flood risk assessment will provide the basis for applying this test. The sequential approach should be used in areas known to be at risk now or in the future from any form of flooding.
- 6.23 Whilst the proposed development of a 3G pitch at Oakwood School would partly be for the use of the wider community, it would also be for the use of pupils at Oakwood School and constitute an improvement to existing facilities. Such a site specific scheme could not reasonably be provided elsewhere. The development is classed as water compatible, and the site is already utilised for sport/ recreational activity, therefore there would not be a change to a more

Agenda Item 7

Planning Committee
6th July 2022

Agenda Item: 7
22/00557/F

vulnerable use. A site-specific Flood Risk Assessment (FRA) has been submitted to demonstrate the development would not result in increased risk of flooding. The Level 1 Strategic Flood Risk Assessment (SFRA) for the three authorities of Reigate & Banstead Borough Council, Mole Valley District Council and Tandridge District Council, and The Areas at Risk of Groundwater Flooding map have been reviewed and it is concluded that, following ground investigation, there would be a medium risk of groundwater flooding at the site.

- 6.24 In terms of management of surface water, the site is currently an open field with no existing drainage. It is a requirement to dispose of surface water collected by a development in accordance with the following, listed in order of priority: 1. Infiltration systems where ground condition permit 2. To watercourses 3. To sewers. Surface water will outfall to the ditch located to the West of the site. The proposed drainage strategy will provide attenuation within the permeable subbase of the pitch. The construction depth of the proposed pitch is shown to be sufficient to accommodate all storage required for a 1 in 30-year flood event, with no above ground flooding. Above ground flooding is triggered for the 1 in 100 year and 1 in 100-year plus 40% climate change events. It is proposed that the facility will be surrounded by 500mm high barriers. The barriers will provide sufficient storage for the 1 in 100-year plus 40% climate change event to accommodate the 272.9m³ volume that exceeds the sub-base capacity in this scenario. The submitted FRA and Surrey County Council SUDs proforma have been reviewed by the County Council who have raised no objection subject to conditions requiring the submission of a finalised surface water drainage scheme for approval prior to commencement of development.
- 6.25 Subject to compliance with these conditions the application is considered acceptable with regard to flooding and drainage matters and would comply with the NPPF 2021 and Policy CCF2 of the DMP 2019.

Other Matters

- 6.26 Objections have been received on the grounds that the development would result in harm to both the Green Belt and a Conservation Area. The site does not fall within either of these designations therefore these matters have not been considered. Further objection has been raised with regard to crime fears. It is not the view that the development would result in an increase in crime. Any such matters, including anti-social behaviour, would be a matter for the police. An operation management plan would be required to be submitted prior to commencement of the use requiring details to be submitted which would include measures demonstrating how users of the facility will be prevented from causing nuisance for people in the area. It has also been commented that the development would impact property values. This is not a material planning consideration that can be taken in to account.

CONDITIONS

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan type	Reference	Version	Received
Location Plan	S21-242 / DWG / 0001	00	28.01.2022
Location Plan	S21-242 / DWG / 0002	00	28.01.2022
Floor Plan	S21-242 / DWG / 0003	P2	28.01.2022
Site Layout Plan	S21-242 / DWG / 0004	00	28.01.2022
Proposed Plans	S21-242 / DWG / 0005	00	28.01.2022
Proposed Plans	S21-242 / DWG / 0006	00	28.01.2022
Proposed Plans	S21-242 / DWG / 0007	00	28.01.2022
Proposed Plans	S21-242 / DWG / 0008	00	28.01.2022
Elevation Plan	S21-242 / DWG / 0009	00	28.01.2022
Elevation Plan	S21-242 / DWG / 0010	00	28.01.2022
Other Plan	S21-242 / DWG / 0011	00	28.01.2022
Other Plan	2001	00	28.01.2022

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

3. The development hereby permitted shall not commence until details of the design of a surface water drainage scheme have been submitted to and approved in writing by the planning authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDS, NPPF and Ministerial Statement on SuDS. The required drainage details shall include:

- a) Evidence that the proposed final solution will effectively manage the 1 in 30 & 1 in 100 (+40% allowance for climate change) storm events, during all stages of the development. The final solution should follow the principles set out in the approved drainage strategy. Associated discharge rates and storage volumes shall be provided using a maximum discharge rate of **3.1 l/s**.
- b) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels, and long and cross sections of each element including details of any flow restrictions and maintenance/risk reducing features (silt traps, inspection chambers etc.).

Agenda Item 7

Planning Committee
6th July 2022

Agenda Item: 7
22/00557/F

- c) Evidence that the downstream receiving watercourse within the site is in a suitable condition to receive and convey flows.
- d) A plan showing exceedance flows (i.e. during rainfall greater than design events or during blockage) and how property on and off site will be protected from increased flood risk. Including details of the management of the 1 in 100 year + CC event within the pitch area.
- e) Details of drainage management responsibilities and maintenance regimes for the drainage system.
- f) Details of how the drainage system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the drainage system is operational.

Reason: To ensure the design meets the national Non-Statutory Technical Standards for SuDS and the final drainage design does not increase flood risk on or off site.

4. Prior to the first occupation of the development, a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority. This must demonstrate that the surface water drainage system has been constructed as per the agreed scheme (or detail any minor variations), provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/areas, flow restriction devices and outfalls), and confirm any defects have been rectified.

Reason: To ensure the Drainage System is constructed to the National Non-Statutory Technical Standards for SuDS.

5. The use hereby permitted shall not commence until an operational management plan has been submitted and approved in writing by the local planning authority. This should include but not be limited to, hours of operation, management responsibilities during all operating hours, periodic inspection and maintenance, measures to control noise from site and appropriate review mechanisms. The use hereby permitted shall thereafter be operated in accordance with the approved details.

Reason: Reason: To ensure that the development does not affect the amenity of the neighbouring properties with regard noise and disturbance with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

6. No development shall commence, including demolition and/or groundworks preparation until a finalized, scaled Tree Protection Plan (TPP) and a related Arboricultural Method Statement (AMS) is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, any required ground protection and details of any construction activity that may take place within the Root Protection Areas (RPA) of trees shown to scale on the TPP. This shall include the installation of service routings as applicable. The AMS shall also include a schedule for a pre-commencement meeting and supervisory site visits for implementation and monitoring with an agreed reporting process to the LPA.

All works shall be carried out in strict accordance with these details when approved.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3 and DES1 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard BS 5837:2012 'Trees in relation to design, demolition and construction – Recommendations'

7. Prior to commencement of development a Sensitive Lighting Management Plan shall be submitted to and approved in writing by the Local Planning Authority.

Reason: To ensure that any potential impact to light sensitive species and dark landscapes is adequately mitigated in accordance with the provisions of the National Planning Policy Framework and policy NHE2 of the Development Management Plan 2019.

8. Use of the development shall not commence until a community use agreement prepared in consultation with Sport England has been submitted to and approved in writing by the Local Planning Authority, and a copy of the completed approved agreement has been provided to the Local Planning Authority. The agreement shall apply to the Artificial Grass Pitch (AGP) and include details of pricing policy, hours of use, access by non-educational establishment users, management responsibilities and a mechanism for review. The development shall not be used otherwise than in strict compliance with the approved agreement.

Reason: To secure well managed safe community access to the sports facility/facilities, to ensure sufficient benefit to the development of sport and to accord with Development Management Plan Policy INF2.

9. No development above ground level shall commence until a scheme to provide positive biodiversity benefits, informed by the submitted ecology report 'Preliminary Ecological Appraisal ECO02175 Oakwood School, Horley A ' dated December 2021, has been submitted to and approved in writing by the local planning authority (LPA). The biodiversity enhancement measures approved shall be carried out and maintained in strict accordance with these details or as otherwise agreed in writing by the LPA, and before occupation of this development.

Reason: To provide enhancements to the biodiversity of the site in accordance with the provisions of the National Planning Policy Framework and Reigate and Banstead Development Management Plan 2019 policy NHE2.

10. The proposed use shall not commence until a Travel Plan has submitted taking account of the occupants of the site for the written approval of the Local Planning Authority in accordance with the sustainable development

Agenda Item 7

Planning Committee
6th July 2022

Agenda Item: 7
22/00557/F

aims and objectives of the National Planning Policy Framework, and Surrey County Council's "Travel Plans Good Practice Guide". The approved Travel Plan shall then be implemented upon occupation of the site and for each and every subsequent occupation of the development, and thereafter maintain and develop the Travel Plan to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.info.
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : Climate Change Information.
3. You are advised that the Council will expect the following measures to be included in the above CMS condition to control noise, pollution and parking:
 - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
 - (e) There should be no burning on site;
 - (f) Only minimal security lighting should be used outside the hours stated above; and
 - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - www.ccscheme.org.uk/index.php/site-registration.

4. The developer is advised that as part of the detailed design of the highway works required by the above conditions, the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
5. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
6. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
7. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to: <http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.
8. Notwithstanding any permission granted under the Planning Acts, no signs, devices or other apparatus may be erected within the limits of the highway without the express approval of the Highway Authority. It is not the policy of the Highway Authority to approve the erection of signs or other devices of a non-statutory nature within the limits of the highway.
9. The permission hereby granted shall not be construed as authority to obstruct the public highway by the erection of scaffolding, hoarding or any other device or apparatus for which a licence must be sought from the Highway Authority Local Highways Service.
10. The use of a suitably qualified arboricultural consultant is essential to provide acceptable submissions in respect of the arboricultural condition above. All works shall comply with the recommendations within British Standard BS 5837:2012 'Trees in relation to design, demolition and construction – Recommendations'
11. If proposed site works affect an Ordinary Watercourse, Surrey County Council as the Lead Local Flood Authority should be contacted to obtain prior written Consent. More details are available on our website.
12. Guidance on preparing Community Use Agreements is available from Sport England. <http://www.sportengland.org/planningapplications/> For artificial grass

Agenda Item 7

Planning Committee
6th July 2022

Agenda Item: 7
22/00557/F

pitches it is recommended that you seek guidance from the Football Association/England Hockey/Rugby Football Union on pitch construction when determining the community use hours the artificial pitch can accommodate.

13. The applicant should be made aware of the requirement for them to apply for a great crested newt mitigation licence from Natural England where development activities may cause an offence. The licence can only be applied for once planning permission has been granted.
14. Measures should be undertaken to enhance the site boundaries for European hedgehog including:
 - Ensuring the species can move across the landscape by creating gaps into all closeboarded fencing
 - Creating habitat connectivity across the landscape
 - Creating a wild corner with minimal habitat management
 - Incorporating hedgehog homes into development.

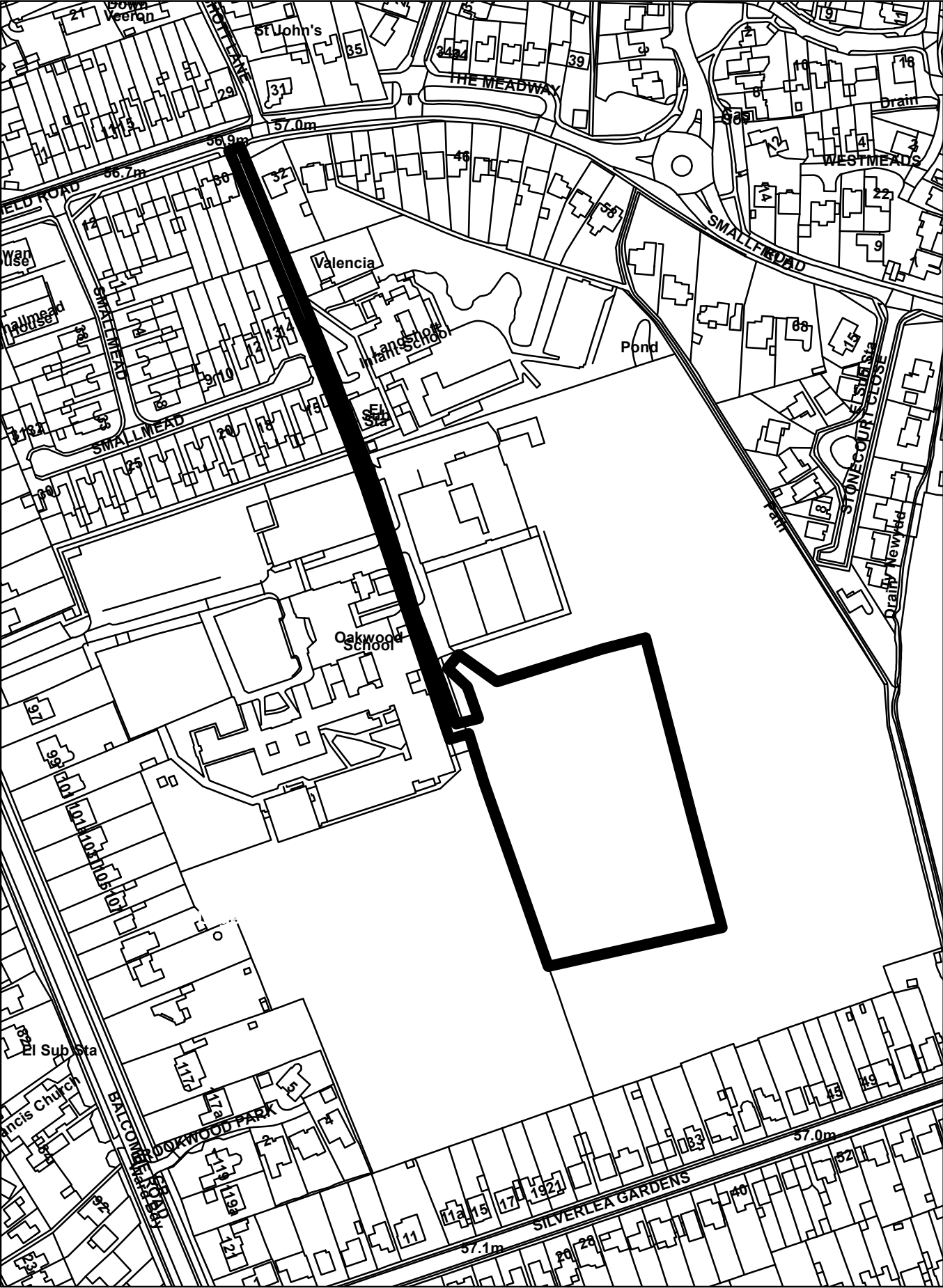
REASON FOR PERMISSION

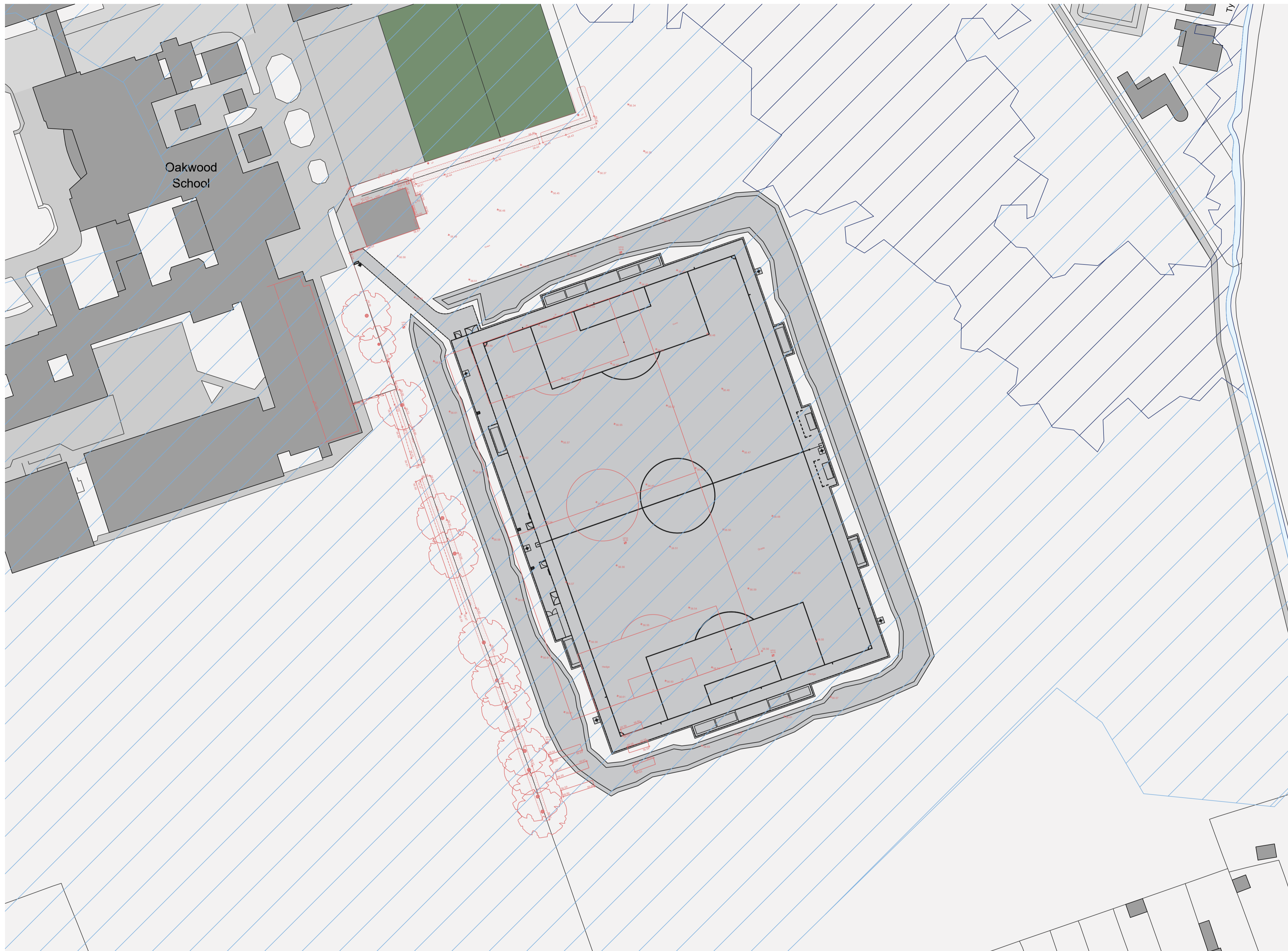
The development hereby permitted has been assessed against the NPPF 2021 and Development Management Plan policies DES1, NHE2, NHE3, TAP1, CCF1, INF1, INF2, OSR3, and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

Proactive and Positive Statements

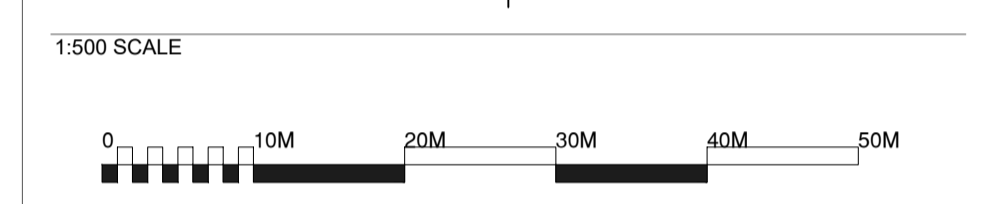
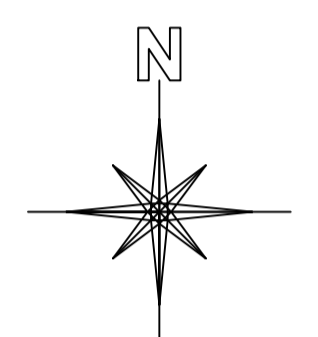
The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

22/00181/F Oakwood Sports Centre
Balcombe Road Horley RH6 9AE





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 INFORMATION RELATING TO USE, MAINTENANCE, AND DEMOLITION TO BE DETAILED IN DUE COURSE WITHIN A BUILDING MANUAL AND HEALTH AND SAFETY FILE.



REVISION	DATE	DRAWN	DESCRIPTION
.00	26 10 2021	TB	ORIGINAL ISSUE



STEPHEN'S YARD ENBORNE NEWBURY
 BERKSHIRE RG20 0HA
 01635 34521
 info@sandcslatter.com
 www.sandcslatter.com

CLIENT
OAKWOOD SCHOOL

PROJECT
COMMUNITY 3G FOOTBALL TURF PITCH (FTP)

LOCATION
**OAKWOOD SCHOOL
 BALCOMBE ROAD
 HORLEY
 SURREY
 RH6 9AE**

DRAWING TITLE
PROPOSED FTP LAYOUT

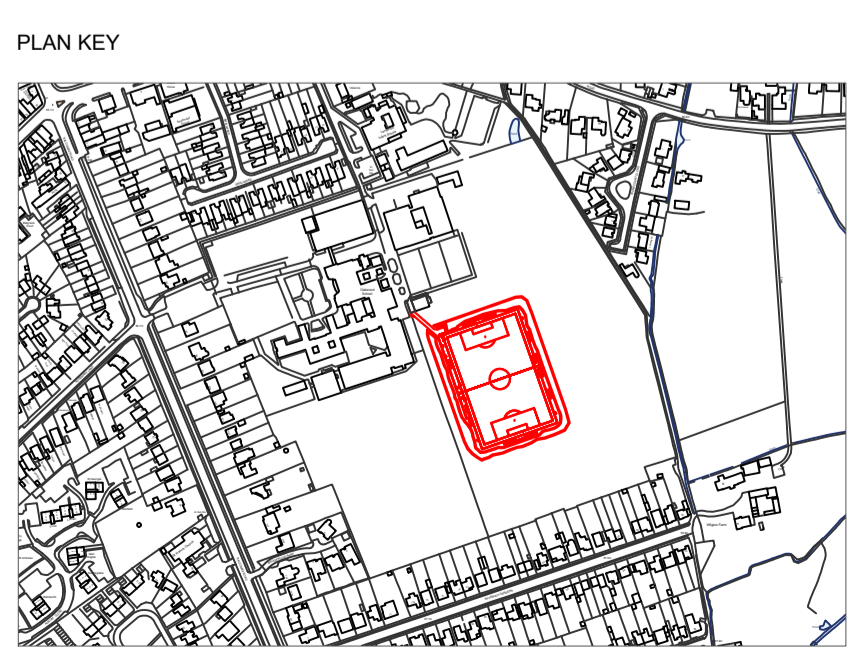
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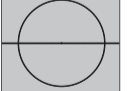



PROJECT NO.
S21-242

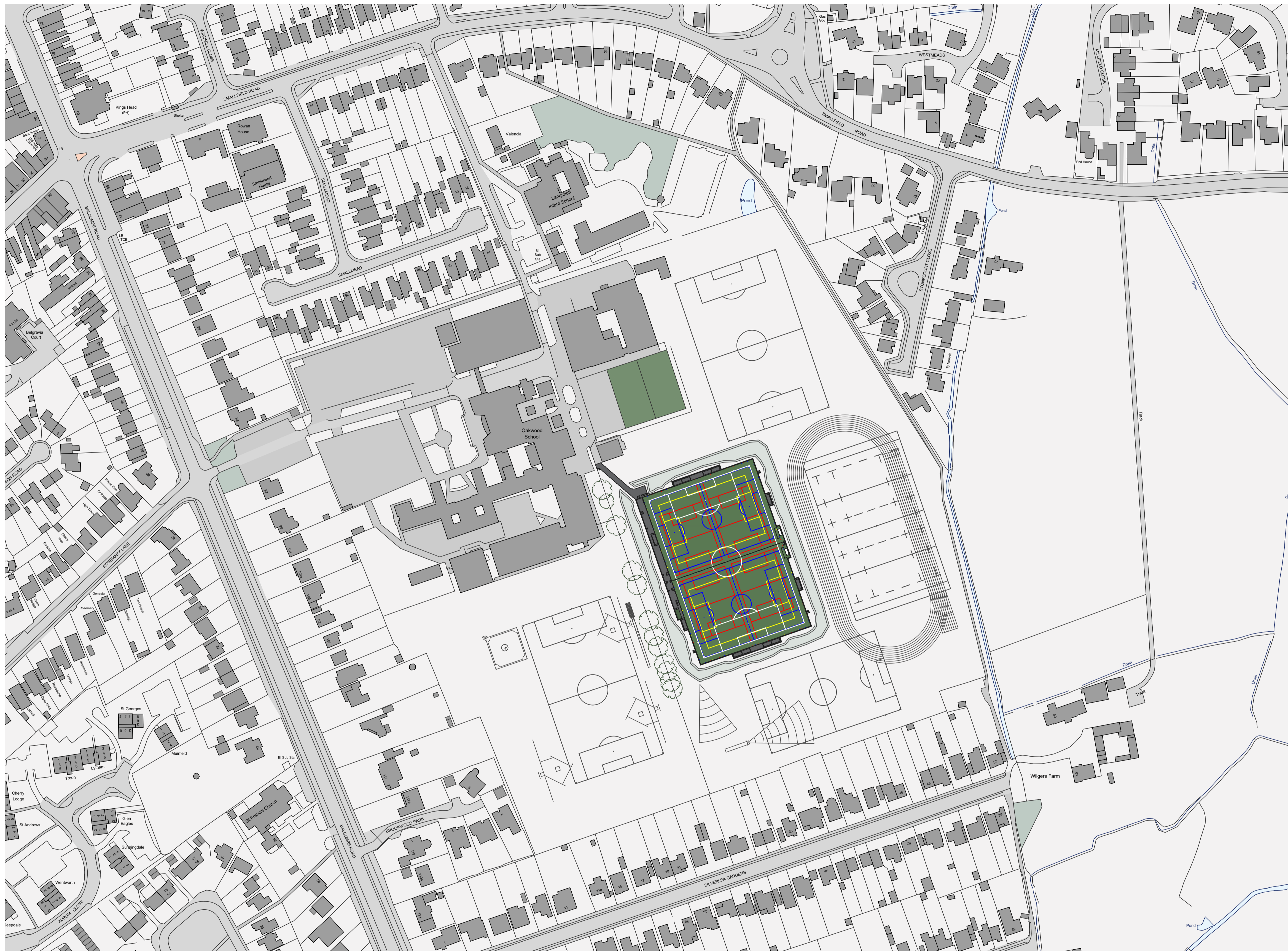
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S21-242 / DWG / 0004

REVISION NO.
.00

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- LEGEND**
-  COMMUNITY 3G FOOTBALL TURF PITCH (FTP)
 -  TOPOGRAPHICAL / LAND SURVEY
 -  EXTENT OF FLOOD ZONE 2 WITHIN SCHOOL PLAYING FIELD
 -  EXTENT OF FLOOD ZONE 3 WITHIN SCHOOL PLAYING FIELD



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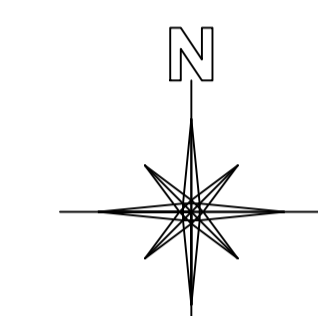
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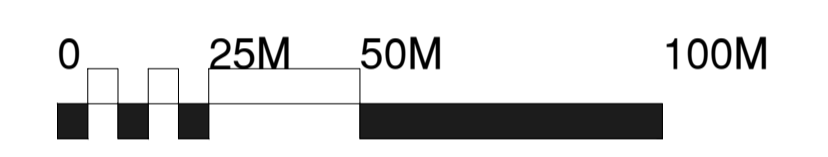
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1:1250 SCALE



REVISION	DATE	DRAWN	DESCRIPTION
.00	26 10 2021	TB	ORIGINAL ISSUE



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BERKSHIRE RG20 0HA
01635 34521
info@sandcslatter.com
www.sandcslatter.com

CLIENT
OAKWOOD SCHOOL

PROJECT
COMMUNITY 3G FOOTBALL TURF PITCH (FTP)

LOCATION
**OAKWOOD SCHOOL
BALCOMBE ROAD
HORLEY
SURREY
RH6 9AE**

DRAWING TITLE
SITE PLAN

STATUS
PLANNING

PROJECT NO.
S21-242

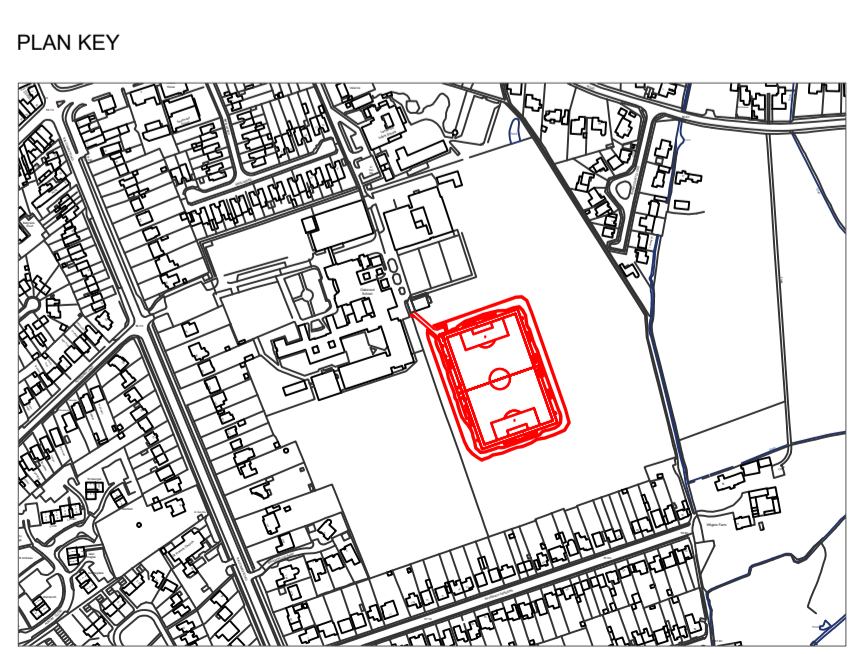
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S21-242 / DWG / 0002

REVISION NO.
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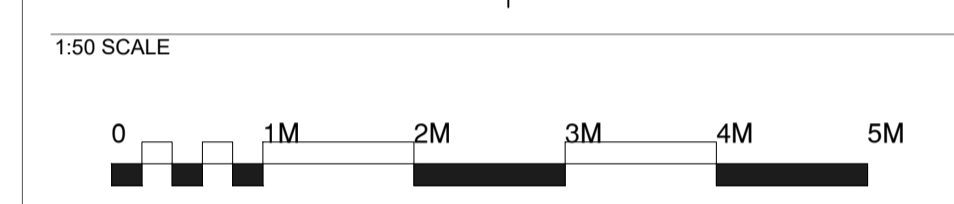
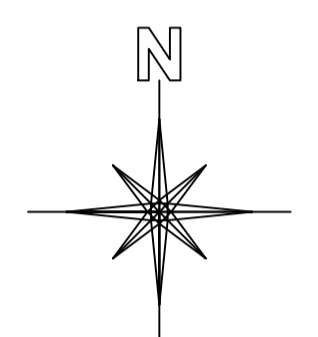
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Agenda Item 7

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REVISION	DATE	DRAWN	DESCRIPTION
.00	26 10 2021	TB	ORIGINAL ISSUE



STEPHEN'S YARD ENBORNE NEWBURY
 BERKSHIRE RG20 0HA
 01635 34521
 info@sandsslatter.com
 www.sandsslatter.com

CLIENT
OAKWOOD SCHOOL

PROJECT
COMMUNITY 3G FOOTBALL TURF PITCH (FTP)

LOCATION
**OAKWOOD SCHOOL
 BALCOMBE ROAD
 HORLEY
 SURREY
 RH6 9AE**

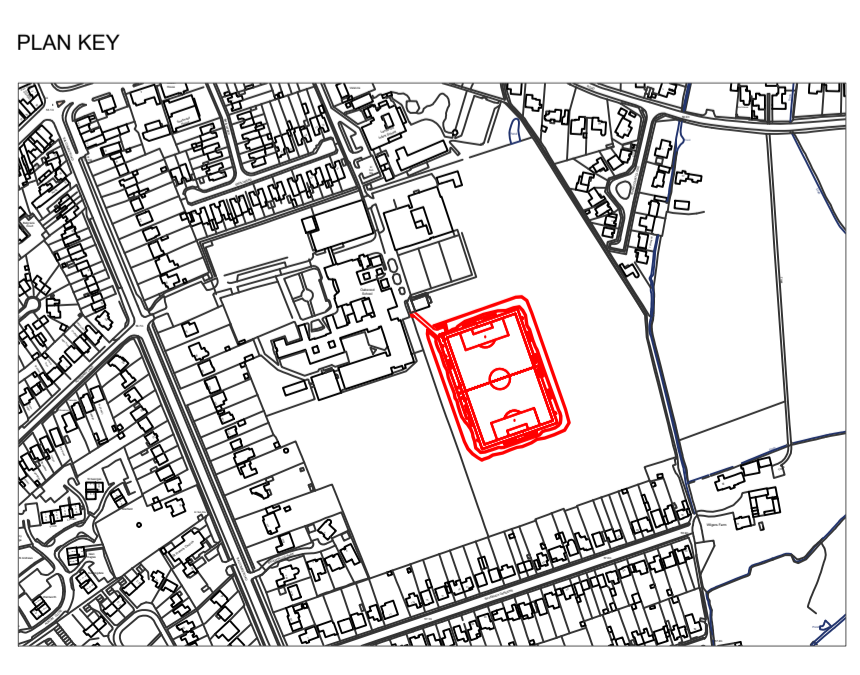
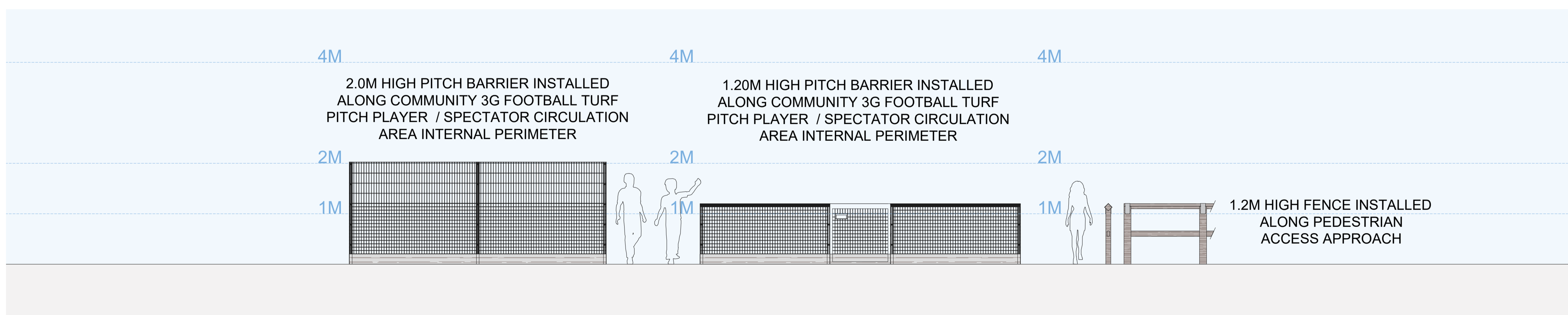
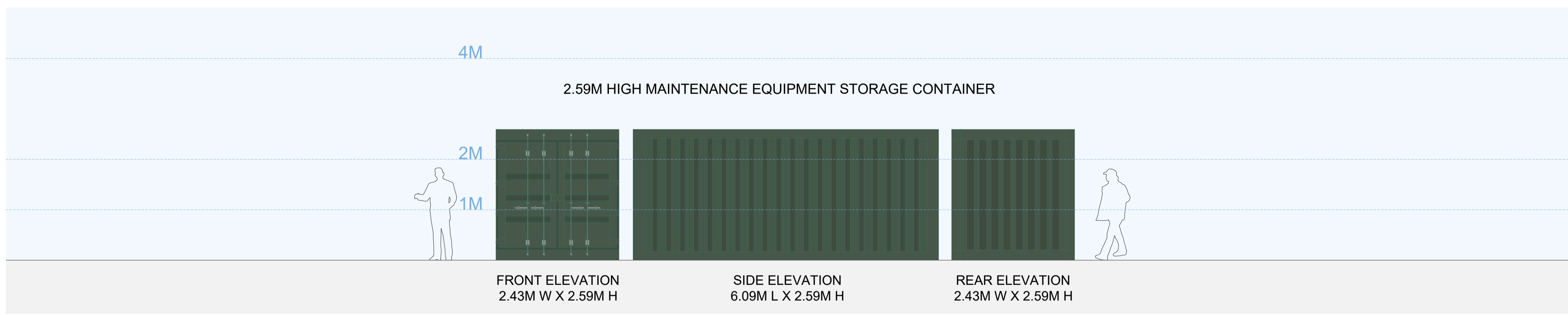
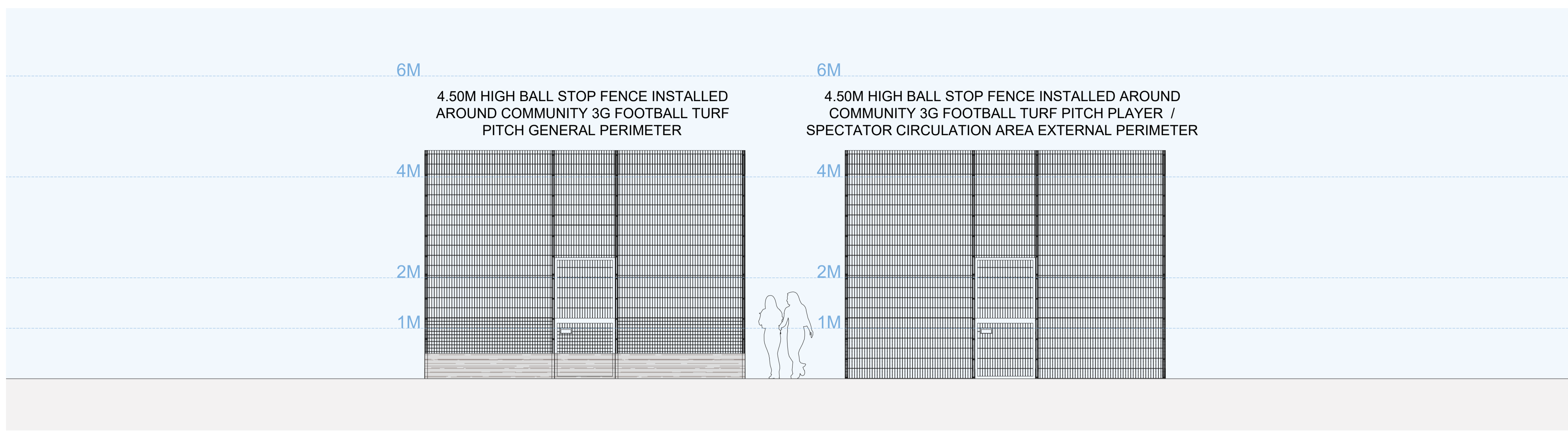
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**PROPOSED FTP
 ELEVATIONS**

STATUS
PLANNING

PROJECT NO.
S21-242

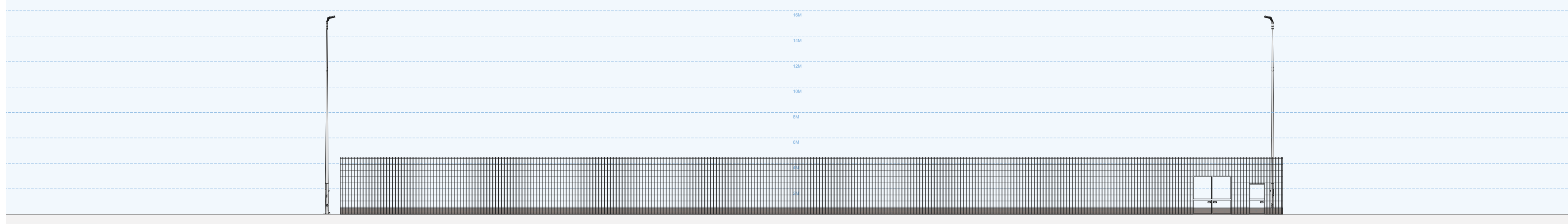
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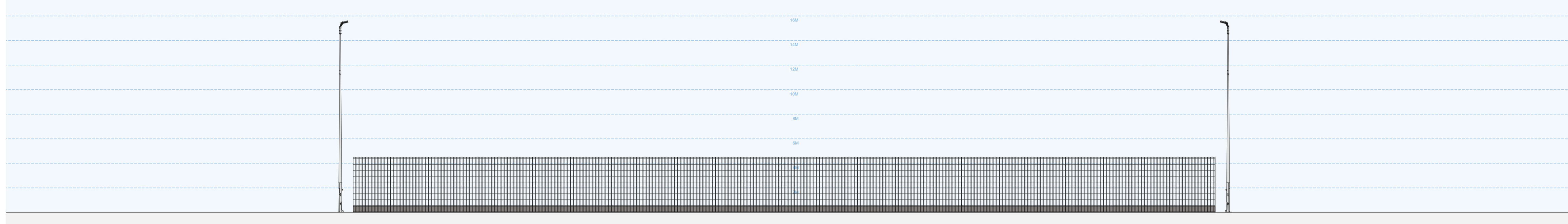


- LEGEND
- 4.50M HIGH BALL STOP FENCE WITH RIGID STEEL PANELS CONTAINING 200X50MM MESH (AND 66X50MM REBOUND MESH PLUS 500MM HIGH INFILL CONTAINMENT BARRIERS WHERE REQUIRED) COLOURED RAL9005 JET BLACK
 - 2.59M HIGH MAINTENANCE EQUIPMENT STORAGE CONTAINER COLOURED RAL6005 MOSS GREEN
 - 2.00M HIGH PITCH BARRIER WITH RIGID STEEL PANELS CONTAINING 200X50MM MESH AND 66X50MM REBOUND MESH PLUS 200MM HIGH INFILL CONTAINMENT BARRIERS COLOURED RAL9005 JET BLACK
 - 1.20M HIGH PITCH BARRIER WITH RIGID STEEL PANELS CONTAINING 66X50MM REBOUND SECTION MESH WITH HANDRAILS AND 200MM HIGH INFILL CONTAINMENT BARRIER COLOURED RAL9005 JET BLACK
 - 1.20M HIGH ACCESS FENCING WITH TIMBER POST AND RAIL BARRIER WITH COLOURED NATURAL WOOD FINISH - INSTALLED ALONG ACCESS APPROACH

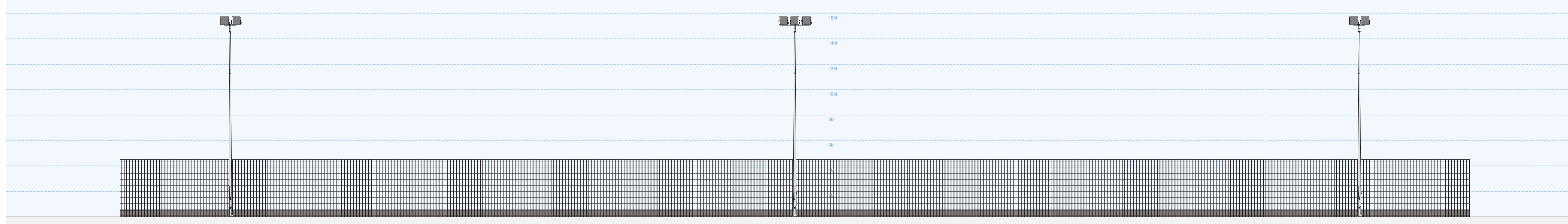
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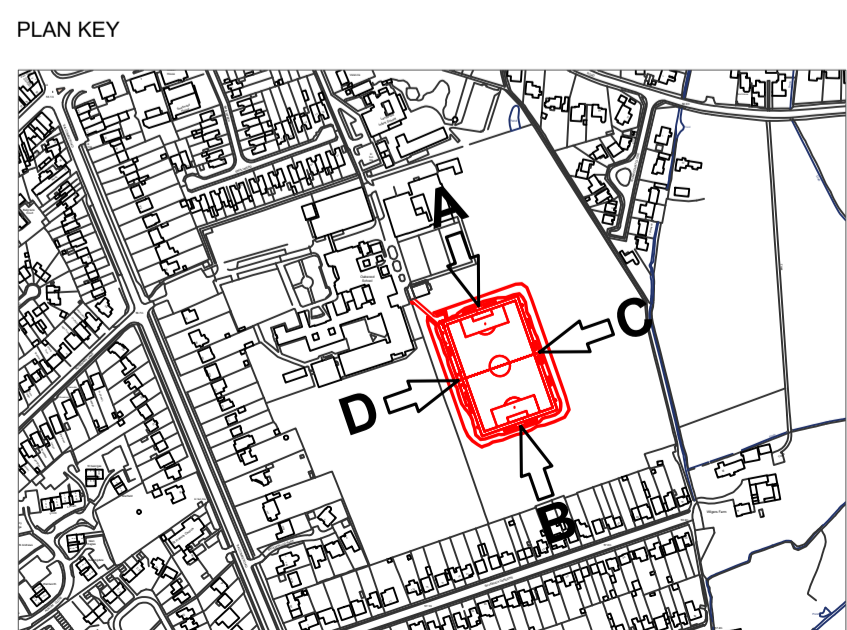
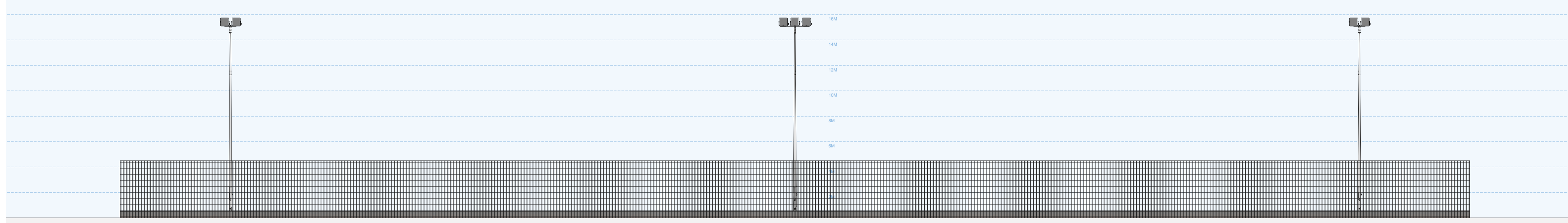
SOUTHERN FTP ENCLOSURE (VIEWPOINT B)



EASTERN FTP ENCLOSURE (VIEWPOINT C)



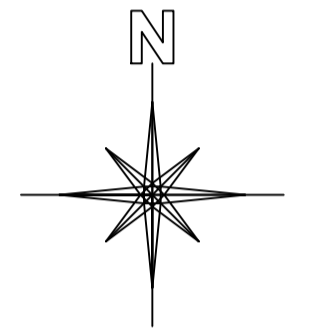
WESTERN FTP ENCLOSURE (VIEWPOINT D)



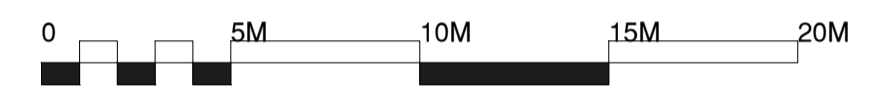
LEGEND

- 15.00M HIGH FLOODLIGHTS WITH STEEL MASTS FINISHED GALVANISED (Z275) SELF-COLOURED, MOUNTED WITH LED LUMINAIRES WITHIN A DIE-CAST HOUSING FINISHED RAL7016 ANTHRACITE GREY
- 4.00M HIGH AMENITY LIGHT LAMP POST FINISHED GALVANISED (Z275) SELF-COLOURED, MOUNTED WITH LED LUMINAIRE WITHIN A DIE-CAST HOUSING FINISHED RAL7016 ANTHRACITE GREY
- 4.50M HIGH BALL STOP FENCE WITH RIGID STEEL PANELS CONTAINING 200X50MM MESH (AND 66X50MM REBOUND MESH PLUS 500MM HIGH INFILL CONTAINMENT BARRIERS WHERE REQUIRED) COLOURED RAL9005 JET BLACK
- 2.59M HIGH MAINTENANCE EQUIPMENT STORAGE CONTAINER COLOURED RAL6005 MOSS GREEN
- 2.00M HIGH PITCH BARRIER WITH RIGID STEEL PANELS CONTAINING 200X50MM MESH AND 66X50MM REBOUND MESH PLUS 200MM HIGH INFILL CONTAINMENT BARRIERS COLOURED RAL9005 JET BLACK
- 1.20M HIGH PITCH BARRIER WITH RIGID STEEL PANELS CONTAINING 66X50MM REBOUND SECTION MESH WITH HANDRAILS AND 200MM HIGH INFILL CONTAINMENT BARRIER COLOURED RAL9005 JET BLACK
- 1.20M HIGH ACCESS FENCING WITH TIMBER POST AND RAIL BARRIER WITH COLOURED NATURAL WOOD FINISH - INSTALLED ALONG PEDESTRIAN ACCESS ROUTE

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1:200 SCALE



REVISION	DATE	DRAWN	DESCRIPTION
.00	26 10 2021	TB	ORIGINAL ISSUE



STEPHEN'S YARD ENBORNE NEWBURY
 BERKSHIRE RG20 0HA
 01635 34521
 info@sandcslatter.com
 www.sandcslatter.com

CLIENT
OAKWOOD SCHOOL

PROJECT
COMMUNITY 3G FOOTBALL TURF PITCH (FTP)

LOCATION
**OAKWOOD SCHOOL
 BALCOMBE ROAD
 HORLEY
 SURREY
 RH6 9AE**

DRAWING TITLE
PROPOSED FTP ELEVATIONS

STATUS
PLANNING

PROJECT NO. S21-242	REVISION NO. .00
DRAWING NO. S21-242 / DWG / 0010	CHECKED
SCALE 1:200	DATE A1
DRAWN TB	CHECKED

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Agenda Item 8

Planning Committee
6th July 2022

Agenda Item: 8
21/02108/F

 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE	
	DATE:	6 th July 2022	
	REPORT OF:	HEAD OF PLANNING	
	AUTHOR:	Hollie Marshall	
	TELEPHONE:	01737 276010	
	EMAIL:	Hollie.marshall@reigate-banstead.gov.uk	
AGENDA ITEM:	8	WARD:	Lower Kingswood Tadworth And Walton

APPLICATION NUMBER:	21/02108/F	VALID:	5 th August 2021
APPLICANT:	Devine Homes PLC	AGENT:	
LOCATION:	64 & REAR OF 62 SHELVERS WAY TADWORTH SURREY KT20 5QF		
DESCRIPTION:	Demolition of 64 Shelveys Way and the erection of 3 x 4 bedroom dwellings and 1 x 3 bedroom dwelling. As amended on 08/02/2022 and on 23/05/2022.		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

SUMMARY

This is a full application for the demolition of 64 Shelveys Way and the erection of a replacement dwelling as well as three new dwellings on land to the rear of 64 and 62 Shelveys Way. The replacement dwelling would be a 3 bedroom, detached bungalow with accommodation in the roof served by a rear facing dormer. To the rear of the site, 3 x 4 bedroom detached houses are proposed. All the dwellings would have an individual design, with a traditional style.

During the course of the application amendments were sought to reduce the number of dwellings and avoid a cramped, overdevelopment of the site. The proposal was reduced from six dwellings at the rear of the site to three. The plot sizes for the three proposed dwellings, as well as the new frontage dwelling, would be smaller than those of the dwellings fronting Shelveys Way, particularly those for the detached dwellings. However, they would nonetheless be proportionate to the size of the dwellings and so the layout is considered acceptable. The dwellings would have spacious gaps between them and to side boundaries, similar to the pattern of development in Shelveys Way. Therefore, the development would broadly reflect the pattern of development in the area and is not considered to result in an unacceptable increase in density or loss of the spacious character.

The proposal would meet the Council's parking standards as set out within the Development Management Plan. The County Highways Authority have raised no objection to the proposal subject to recommended conditions.

Agenda Item 8

Planning Committee
6th July 2022

Agenda Item: 8
21/02108/F

Within the rear part of the site and beyond the site boundaries, trees are protected by way of a group tree preservation order. Following amendments to the proposal and a reduction in the number of dwellings, the Tree Officer is satisfied the plots will not experience excessive shading during the day and two conditions are recommended to ensure tree protection during the works.

The proposal, whilst representing a change, is not considered to result in a harmful impact upon neighbour amenity.

RECOMMENDATION(S)

Planning permission is **GRANTED** subject to conditions.

Consultations:

Highway Authority: The County Highway Authority has assessed the application on safety, capacity and policy grounds and has raised no objection subject to condition.

The developer is providing car parking fully in accordance with Reigate and Banstead Parking Standards which states that the developer should be providing 10 car parking spaces for the dwellings, visitor parking is not required. But the developer is proposing 13 spaces. One of the four dwellings has three parking spaces, two dwellings have one garage and a two parking spaces. The fourth dwelling has a double garage and two parking spaces.

The site layout includes space for refuse vehicles to enter and leave the site in forward gear. This would allow refuse vehicles to enter the site to collect refuse. However it will mean that parking restrictions need to be provided at the access so that refuse vehicles can enter the site without obstruction by parked cars.

In order to avoid this situation, the refuse collection point could be located to a point within 25 metres of the highway. This would allow refuse collection from Shelveys Way. However, this would mean residents of the development would potentially be walking further than 30 metres to reach the refuse collection point of the development.

Housing: no comments received

Sutton and East Surrey Water Company: no comments received

Tadworth and Walton Residents Association: Objects on the grounds of harm to/loss of trees, future pressure to prune trees, cramped, inadequate parking, amenity for future occupants, noise and disturbance, overdevelopment, loss of biodiversity, lack of infrastructure, flooding, drainage, hazard to highway safety, bat survey required.

Surrey County Council SUDS: As the application sites outside our Statutory Duty requirements, as the LLFA we would have no comments.

Drainage Consultant: 'There is no specific FRA or drainage strategy submitted so I have undertaken a data capture.

The site is within Flood Zone 1 and so does not need a FRA to the NPPF. It should also pass the sequential and exception tests.

There is a noted surface water flow route across the site, but this is not present in the "medium" risk scenario. Therefore, it is likely to be low risk and just needs to be considered as part of any proposed drainage strategy.

The site is located on topsoil over loam over chalk so there may be opportunity for infiltration devices on site. Any infiltration design should be supported by BRE Digest 365 soakage testing.

Agenda Item 8

Planning Committee
6th July 2022

Agenda Item: 8
21/02108/F

I can find no reason to refuse the scheme on flood risk or drainage, noting that a drainage design should be submitted prior to commencement as a standard condition on approval.'

Surrey Wildlife Trust:

The Local Planning Authority (LPA) has a duty to conserve biodiversity in line with the planning and legislative context. Relevant legislation and planning policies are detailed in Appendix 1. We have reviewed the relevant application documents submitted on the planning portal, and other relevant publicly available information, and assessed these against published best practice guidance to determine whether submitted information was sufficient in order for the LPA to assess the planning application. Following this, we assessed the proposals against relevant legislation and planning policy and recommended appropriate course of action to ensure the LPA is fulfilling its duty to conserve biodiversity.

We have reviewed the following reports:

- Preliminary Ecological Appraisal and Preliminary Inspection for Bats (aLyne Ecology, 2022)*
- Bat Survey Report (aLyne Ecology, 2022)*
- Arboricultural Report (David Archer Associates, 2022)*

Mole Gap to Reigate Escarpment SAC

In the Preliminary Ecological Appraisal and Preliminary Inspection for Bats report it states that Natural England and Reigate and Banstead Borough Council should be consulted to confirm whether an Assessment of Likely Significant Effects is required in relation to Mole Gap to Reigate Escarpment SAC. We would advise that the project is assessed against the requirement for a Screening Assessment.

Protected species - bats

The legal protection afforded to bats is presented in Appendix 1.

The applicant should be made aware of the requirement for them to apply for a bat mitigation licence from Natural England where development activities may cause an offence. The licence can only be applied for once planning permission has been granted.

The above referenced Bat Survey Report appears appropriate in scope and methodology and has identified the likely absence of active bat roosts within B1 on the development site. We therefore advise that bats do not appear to present a constraint to the proposed demolition of B1.

Protected species – badger

Three badger setts were recorded by aLyne Ecology on the northern site boundary (Preliminary Ecological Appraisal and Preliminary Inspection for Bats report). In Table 14 it states that the development could cause disturbance to an active badger sett, therefore further survey is required. In Section 7.7 it states that badgers are likely to forage on the site.

In conflict with Table 14, in Section 8.2 it states that “However, as the current scheme shows gardens at the rear of the site, it is unlikely that the badger setts will require closure. If the scheme changes, and disturbance of the setts is likely, further survey for badgers will be required”. This appears to imply that no further survey of the badger setts is required. We would advise that this is clarified.

The exact location of the badger setts is unknown due to confidentiality. Therefore we must assume that it covers the whole northern boundary of the proposed development site, as a precaution. The Tree Protection Plan in the Arboricultural Report shows the removal of a Laurel, a Magnolia and H5 within 15m of the northern boundary. In addition, taken together the Wider Context Plan (Vision Architects, 2021) and the Accommodation Schedule (Vision Architects, 2021) shows the location of houses within approximately 10-15m of the northern boundary of the proposed development.

On our review, we would advise the LPA that the construction of three houses, and associated tree and hedge clearance within 15m of three badger setts has the potential to disturb badgers or obstruct how badgers use the sett (s) which would be breach of the legislation afforded to badger. Without further detail on the dimensions and structure of the badger setts, we cannot advise on whether the project has the potential to damage the badger setts.

Natural England advise that to understand the level of mitigation needed, the development proposal must show how likely it is that badgers will be affected by any development work. We would advise the LPA that we have not reviewed sufficient evidence to conclude that badger will not be affected by the proposed development work.

Prior to determination, we would advise that the Applicant provides the LPA with further information on the potential impact of the proposed development upon badger, in line with the Protection of Badgers Act 1992. We would advise that this includes information on the type of badger setts present, the level of activity present, the potential impacts of the proposed development, the distance of the setts in relation to the operational and construction phases of the project and a mitigation strategy.

Construction Environmental Management Plan (CEMP)

Given the presence of ecological receptors on site, there is a risk of causing ecological harm resulting from construction activities. Should the LPA be minded to grant permission for the proposal the applicant should be required to implement the development only in accordance with an appropriately detailed CEMP. This document will need to be submitted to and approved by the LPA in writing, prior to the commencement of the development. The CEMP should include, but not be limited to:

- a) Map showing the location of all of the ecological features*
- b) Risk assessment of the potentially damaging construction activities*
- c) Practical measures to avoid and reduce impacts during construction*
- d) Location and timing of works to avoid harm to biodiversity features*
- e) Responsible persons and lines of communication*
- f) Use of protected fences, exclusion barriers and warning signs*
- g) Invasive Plant Species Management and Removal Plan.*

Sensitive Lighting

Agenda Item 8

Planning Committee
6th July 2022

Agenda Item: 8
21/02108/F

Nocturnal species including bats are known to be present at the development site. These species are sensitive to any increase in artificial lighting of their roosting and foraging places and commuting routes.

Paragraph 185 of the NPPF (2021) states that planning policies and decisions should “limit the impact of light pollution from artificial light on ... dark landscapes and nature conservation.”

The applicant should ensure that the proposed development will result in no net increase in external artificial lighting at primary bat foraging and commuting routes across the development site, in order to comply with above referenced legislation and the recommendations in BCT & ILP (2018) Guidance Note 08/18. Bats and artificial lighting in the UK. Bats and the Built Environment. Bat Conservation Trust, London & Institution of Lighting Professionals, Rugby”.

We advise that compliance with this best practice guidance is secured through a Sensitive Lighting Management Plan submitted to the LPA for approval in writing prior to commencement of development.

Landscape and Ecological Management Plan (LEMP)

Should the LPA be minded to grant planning permission for this proposed development, we recommend that the LPA requires the development to be implemented in accordance with an appropriately detailed landscape and ecological management plan (LEMP).

This document should be submitted to and approved in writing by the LPA prior to the commencement of development. The LEMP should be based on the proposed impact avoidance, mitigation and enhancement measures specified in the above referenced report and should include, but not be limited to following:

- a) Description and evaluation of features to be managed*
- b) Ecological trends and constraints on site that might influence management*
- c) Aims and objectives of management*
- d) Appropriate management options for achieving aims and objectives*
- e) Prescriptions for management actions, together with a plan of management compartments*
- f) Preparation of a work schedule (including an annual work plan capable of being rolled forward over a five-year period*
- g) Details of the body or organisation responsible for implementation of the plan*
- h) Ongoing monitoring and remedial measures*
- i) Legal and funding mechanisms by which the long-term implementation of the plan will be secured by the applicant with the management body(ies) responsible for its delivery.*
- j) Monitoring strategy, including details of how contingencies and/or remedial action will be identified, agreed and implemented so that the development still delivers the fully functioning biodiversity objectives of the originally approved scheme*
- k) Ecological Enhancement Plan*

This LEMP should be written in line with Section 7.2 of the Preliminary Ecological Appraisal and Preliminary Inspection for Bats report which states “Native trees should be retained, where possible and any trees lost as a result of the proposed development, should be replaced with equivalent numbers of native species”.

Invasive Plant Species Management and Removal Plan

Section 7.3 of the Preliminary Ecological Appraisal and Preliminary Inspection for Bats report sets out the presence of non-native invasive plant species on the proposed development site.

Should the LPA be minded to grant planning permission of the proposed development, we recommend the LPA requires the applicant to submit an invasive species management plan, prepared by a suitably qualified individual that details how the control of invasive species will be managed on site, including roles and responsibilities. This should be submitted as part of the CEMP.

Representations:

Letters were sent to neighbouring properties on 6th August 2021. Neighbours were re-notified on the revised plans for a 14 day period commencing 8th February 2022.

86 responses have been received raising the following issues:

Issue	Response
Inadequate parking	See paragraph 6.21 – 6.24
Increase in traffic and congestion	See paragraph 6.21 – 6.25
Hazard to highway safety	See paragraph 6.21 – 6.25
Inconvenience during construction	See paragraph 6.15 and condition 5
Out of character with surrounding area	See paragraph 6.3 – 6.9
Overdevelopment	See paragraph 6.3 – 6.9
Overbearing relationship and loss of outlook	See paragraph 6.10 – 6.14
Health fears	See paragraph 6.15
Poor design	See paragraph 6.3 – 6.9
Overlooking and loss of privacy	See paragraph 6.10 – 6.14
Loss of / harm to trees	See paragraph 6.26 – 6.28 and conditions 10 and 11
Replacement of telegraph pole and street lamp	See paragraph 6.39
Flooding	See paragraph 6.31 and conditions 13 and 14
Crime fears	See paragraph 6.17
Harm to wildlife habitat	See paragraph 6.29 – 6.30
Harm to green belt / countryside	See paragraph 6.40
Drainage/sewage capacity	See paragraph 6.31 and conditions 13 and 14

Agenda Item 8

Planning Committee
6th July 2022

Agenda Item: 8
21/02108/F

Harm to Conservation Area	See paragraph 6.40
Pollution	See paragraph 6.17
Noise & disturbance	See paragraph 6.16
Increased demand to utilities and services with no infrastructure proposed	See paragraph 6.37 and 6.38
No need for the development	Each scheme must be assessed on its own planning merits
Property devaluation	This is not a material planning consideration
Alternative scheme preferred	Submitted scheme must be assessed on its own planning merits
Loss of private view	Not a material planning consideration
Benefit to housing need	See paragraph 6.1
Community/regeneration benefit	See paragraph 6.1
Economic growth/jobs	See paragraph 6.1
Visual amenity benefits	See paragraph 6.3 – 6.9

1.0 Site and Character Appraisal

- 1.1 The application site comprises 64 Shelveys Way and the rear part of the garden of 62 Shelveys Way. No. 64 includes a detached bungalow, set in a generous plot on the northern side of Shelveys Way. The rear garden of the site, and that of No. 62 contains mature trees protected by way of Tree Preservation Order ref: BAN114. The site is relatively flat.
- 1.2 The surrounding area is characterised by residential dwellings varying in style and scale. The dwellings in this part of Shelveys Way are set in generous plots and have a spacious character. To the rear of the site is a small parcel of land and beyond this are dwellings fronting Fleetwood Close. These are terrace houses set in modest sized plots. On the southern side of Shelveys Way are semi-detached houses that lie within elongated plots. Towards the eastern entrance to Shelveys Way there is an example of more recent residential development at Stanton Grove.

2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: The applicant did not approach the Council for pre-application advice therefore the opportunity to secure improvements did not arise

2.2 Improvements secured during the course of the application: During the course of the application amendments have been sought to address concerns over the proposal resulting in a cramped development. The proposal has since been reduced from a proposal for 7 dwellings, to 4 dwellings.

2.3 Further improvements to be secured through the use of conditions regarding the use of materials, landscaping, tree protection, ecology, highways and drainage would be attached to a grant of planning permission.

3.0 Relevant Planning and Enforcement History

There is no recent planning history relevant to the proposal at 62 or 64 Shelvers Way.

4.0 Proposal and Design Approach

4.1 This is a full application for the demolition of 64 Shelvers Way and the erection of a replacement dwelling as well as three new dwellings on land to the rear of 64 and 62 Shelvers Way. The replacement dwelling would be a three bedroom, detached bungalow with accommodation in the roof served by a rear facing dormer. To the rear of the site, three x four bedroom detached houses are proposed. All the dwellings would have an individual design, with a traditional style.

4.2 A new access road would be created into the site from Shelvers Way. Along the access road would be space for landscaping to either side and two visitor parking spaces. To the rear of the replacement dwelling would be a car barn and 2 parking spaces to the front and to the eastern end of the access road would be a double garage and another 2 parking spaces to the front. Plots 2 and 3 would have 1 parking space to the front of each dwelling and the replacement dwelling would have car parking space for 3 vehicles to the front.

4.3 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:
Assessment;
Involvement;
Evaluation; and
Design.

4.4 Evidence of the applicant's design approach is set out below:

Assessment	The character of the surrounding area is assessed as predominantly residential and comprises a mix of detached and semi-detached houses. Land to the north of the site comprises residential development . Land to the
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Agenda Item 8

Planning Committee
6th July 2022

Agenda Item: 8
21/02108/F

	east is also primarily residential with some Urban Open Space Land around Fleetwood Close, with Waterhouse Lane Local Centre 0.6mi away across the A217. To the south comprises residential development fronting on to Shelvers Way. There has been a number of new dwellings built on land at 1-41 Shelvers Way.
	Site features meriting retention are listed as mature trees where possible.
Involvement	No community consultation took place.
Evaluation	The statement does not include any evidence of other development options being considered.
Design	The applicant's reasons for choosing the proposal from the available options were informed by a character appraisal of the surrounding area, undertaken to understand the character and features of properties within the local area.

4.5 Further details of the development are as follows:

Site area	0.31 hectares
Proposed parking spaces	14
Parking standard	9.5 (minimum)
Net increase in dwellings	3
Existing site density	5 dwellings per hectare
Proposed site density	16 dwellings per hectare
Density of the surrounding area	7.5 dwellings per hectare 58 – 74a Shelvers Way 25 dwellings per hectare – Stanton Grove 25 dwellings per hectare 115 – 118 Fleetwood Close

5.0 Policy Context

5.1 Designation

Urban area
Tree Preservation Order BAN114
Surface Water Flood Model 1 in 1000 years
Parking standards – low accessibility

5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)
CS2 (Valued Landscapes and Natural Environment),
CS4 (Valued Townscapes and Historic Environment)
CS10 (Sustainable Development),
CS11 (Sustainable Construction),
CS14 (Housing Needs)
CS15 (Affordable Housing)

5.3 Reigate & Banstead Development Management Plan 2019

Design, Character and Amenity (including housing)	DES1, DES2, DES4, DES5, DES7, DES8,
Landscape & Nature Conservation Infrastructure Transport, Access and Parking Climate Change Resilience and Flooding	NHE2, NHE3, INF3 TAP1, CCF1, CCF2

5.4 Other Material Considerations

National Planning Policy Framework	
National Planning Practice Guidance	
Supplementary Planning Guidance	Surrey Design Local Distinctiveness Design Guide Vehicle and Cycle Parking Guidance 2018 Householder Extensions and Alterations Affordable Housing
Other	Human Rights Act 1998 Community Infrastructure Levy Regulations 2010 Conservation of Habitats and Species Regulations 2010

6.0 **Assessment**

6.1 The application site is situated within the urban area where there is a presumption in favour of sustainable development and where the principle of such residential development is acceptable in land use terms.

6.2 The main issues to consider are:

- Design appraisal
- Neighbour amenity

Agenda Item 8

Planning Committee
6th July 2022

Agenda Item: 8
21/02108/F

- Housing mix
- Amenity for future occupants
- Highway matters
- Impact on trees
- Ecology
- Drainage and flooding
- Sustainable construction
- Affordable Housing
- Community Infrastructure Levy
- Infrastructure contributions
- Other matters

Design appraisal

- 6.3 DMP Policy DES1 relates to the Design of New Development and requires new development to be of a high quality design that makes a positive contribution to the character and appearance of its surroundings. New development should promote and reinforce local distinctiveness and should respect the character of the surrounding area. The policy states that new development will be expected to use high quality materials, landscaping and building detailing and have due regard to the layout, density, plot sizes, building siting, scale, massing, height, and roofscapes of the surrounding area, the relationship to neighbouring buildings, and important views into and out of the site.
- 6.4 DMP Policy DES2 which relates to development of residential garden land seeks to ensure that backland developments are of high-quality. The policy requires, amongst other things, that garden land developments should be designed to respect the scale, form and external materials of existing buildings in the locality and a height, mass and bulk to be in keeping with the existing street scene. DES2 requires that developments do not create an undue disruption in the street scene and that developments should ensure that a good standard of amenity is retained for existing and future occupants. The policy also encourages the retention of mature trees and hedges.
- 6.5 The application proposes the demolition of No. 64, and the construction of a replacement dwelling and new access road into the rear of the site in its place. The frontage dwelling would be a detached bungalow. This dwelling would follow the building line of Shelveys Way and would have a gap of 3m to the side boundary with No. 66 Shelveys Way and a gap of 1m to the access road. The separation distances are such that the dwelling would not appear cramped, conforming to the spacious appearance of this part of Shelveys Way. The bungalow would have a fully hipped roof with front face gable that would include a bay window. There would be accommodation in the roof served by side elevation roof lights and a rear facing dormer window, the design of which would accord with the Council's design guidance. To the front of the dwelling would be a driveway utilising the existing access from Shelveys Way, and space for 3 cars to park. There are areas of soft

landscaping proposed adjacent to the front of the house and the front boundary.

- 6.6 Turning to the rear of the site, three detached, two storey houses are proposed. The design of the dwellings would be traditional with fully pitched, hipped roofs and front gable features. The dwellings would have an individual design however a cohesive appearance in terms of the style. The three dwellings would be similar in scale and appearance although with a variance in some features such as the window design, porch design and layout; this variety is considered a positive design approach and akin to the variety of designs seen in the streetscene.
- 6.7 The rear gardens for these dwellings would have depths of between approximately 12.5m – 14.5m. The plot sizes for the proposed dwellings would be smaller than may of those of the dwellings fronting Shelveys Way, particularly those for the detached dwellings. However, they would nonetheless be proportionate to the size of the dwellings and commensurate with plots on the southern side of Shelveys Way and at Fleetwood Close and so the layout is considered acceptable. The dwellings would have gaps of between 2.5 – 3m between them. Plot 2 would be set away from the side boundary with No. 66 by 4.3m and plot 4 would be set away from the side boundary with No. 62 by 5.5m. The spacing between the detached dwellings would be similar with the pattern of development in Shelveys Way. Therefore, the development would broadly reflect the pattern of development in the area and not result in an unacceptable increase in density or loss of the spacious character.
- 6.8 Within the site, to the front of the dwellings and along the access road, there is space for soft landscaping, providing areas for planting to soften the appearance of the development. This would accord with the landscaped frontages in the streetscene.
- 6.9 Overall, it is considered that the proposal would respect the character of the existing area and would accord with policies DES1 and DES2.

Neighbour amenity

- 6.10 The proposed development has been considered with regards to its impact on the amenity of neighbouring properties. The proposed dwellings would be well separated from the donor property fronting Shelveys Way by approximately 30m (from rear elevation of No. 62 the front elevation of Plot 4). This is sufficient to ensure that no material adverse harm would occur to the property by way of overlooking, loss of light or overbearing impact.
- 6.11 The new access road would be sited between the replacement dwelling and No 62. The design of the proposed access road, its width and the opportunities for landscaping and suitable boundary treatment on either side, it is considered that the proposal would not seriously affect the living conditions of neighbouring residents.

Agenda Item 8

Planning Committee
6th July 2022

Agenda Item: 8
21/02108/F

- 6.12 To the east and west of the site are 66 and 62 Shelveys Way. Plot 2 would be set away from the side boundary with No. 66 by 4.3m and plot 4 would be set away from the side boundary with No. 62 by 5.5m. The level of separation is such that it is considered the proposal would not result in a harmful impact in terms of overbearing and domination and these gaps would allow for landscaping along these boundaries to soften the visual impact of the development. First floor side facing windows (serving bathrooms) would be conditioned to be obscure glazed to avoid issues of overlooking and loss of privacy.
- 6.13 Further beyond to the east are 115 – 118 Fleetwood Close. These dwellings would have a separation distance from their rear boundaries to the site boundary of approximately 24m, and gaps of between 34 – 39m between the flank elevation of plot 4 and the garage and the rear elevations of these dwellings. In view of the level of separation, the proposal is not considered to result in a harmful impact in terms of overbearing, domination or harm to outlook.
- 6.14 To the rear of the site, beyond an open parcel of land, are dwellings 73 – 80 Fleetwood Close. The proposed dwellings would have a separation distance of approximately 36m to the rear elevations of these dwellings, with mature trees to be retained between the two sites. Given the level of separation, the proposal is not considered to result in harmful impact upon the amenities of these dwellings.
- 6.15 Objection has been raised from neighbouring properties regarding inconvenience during construction, noise and disturbance, pollution, crime and health fears. Some inconvenience may occur during the construction of the proposal; however, this is part and parcel of development and would not form a sustainable reason for refusal. Statutory nuisance legislation does however exist to control any significant harm that may occur, and a construction method statement would be secured by planning condition were the application to be approved.
- 6.16 The proposed development may result in some additional noise and disturbance; however, the development would be in residential use and this would not be significant enough to warrant refusal of the application.
- 6.17 The proposal would result in the redevelopment of rear gardens, new boundary treatment is proposed, and the development is not considered to cause crime issues. No significant health or pollution issues are considered to arise as a result of the planning application. Given the scale of the proposed development and residential nature, the proposal is not considered to result in a harmful impact in regard to light pollution or nuisance from headlights. The separation distances to neighbouring dwellings is satisfactory so as to avoid a harmful impact in terms of outlook or an oppressive appearance.

Housing mix

- 6.18 DMP Policy DES4 relates to Housing Mix and states that all new residential developments should provide homes of an appropriate type, size and tenure to meet the needs of the local community. The proposed housing mix must on sites of up to 20 homes, have at least 20% of market housing provided as smaller (one and two bedroom) homes. In this case, the scheme would provide 3 x 4 bedroom dwellings and 1 x 3 bedroom dwelling. The replacement dwelling at the front of the site has a floor plan with 1 bedroom at ground floor and 2 bedrooms the roof space. The layout of the ground floor could be versatile in this instance, with the front room used as a second reception room to the kitchen/lounge/diner at the rear of the dwelling and would accord with the terms of the policy by providing a two bedroom home equating to 25% of this development.

Amenity for future occupants

- 6.19 The NPPF provides that planning decisions should provide a high standard of amenity for future users. DMP Policy DES5 relates to the delivery of high quality homes and requires, inter alia, that as a minimum, all new residential development (including conversions) must meet the relevant nationally described space standard for each individual units except where the Council accepts that an exception to this should be made in order to provide an innovative type of affordable housing that does not meet these standards. In addition, the policy also requires all new development to be arranged to ensure primary habitable rooms have an acceptable outlook and where possible receive direct sunlight.
- 6.20 All units would exceed the minimum internal spaces standards. Each dwelling would be orientated to face south eastwards and main habitable rooms would be afforded adequate levels of daylight and sunlight. The resultant plot sizes are considered to create an adequate level of amenity for future occupants with acceptable private outdoor amenity space for each dwelling and the proposal is therefore considered to comply with the requirements of policy DES5.

Highway matters

- 6.21 The site is located in an area which is assessed as having a low accessibility rating. In such areas, the Council's adopted parking standards require the provision of 2 spaces for each 2/3 bedroom house, 2.5 spaces for each 4 bedroom house. The application proposes a total of 14 parking spaces. This number includes the provision of 2 visitor parking spaces. The DMP requires a minimum of 9.5 parking spaces for a development of this size and therefore the proposal meets and exceeds the minimum requirement.
- 6.22 The proposed development has been considered by the county highway authority who having assessed the application on safety, capacity and policy grounds, and made the following comments:

The developer is providing car parking fully in accordance with Reigate and Banstead Parking Standards which states that the developer should be

Agenda Item 8

Planning Committee
6th July 2022

Agenda Item: 8
21/02108/F

providing 10 car parking spaces for the dwellings, visitor parking is not required. But the developer is proposing 13 spaces. One of the four dwellings has three parking spaces, two dwellings have one garage and a two parking spaces. The fourth dwelling has a double garage and two parking spaces.

The site layout includes space for refuse vehicles to enter and leave the site in forward gear. This would allow refuse vehicles to enter the site to collect refuse. However it will mean that parking restrictions need to be provided at the access so that refuse vehicles can enter the site without obstruction by parked cars.

In order to avoid this situation the refuse collection point could be located to a point within 25 metres of the highway. This would allow refuse collection from Shelveys Way. However this would mean residents of the development would potentially be walking further than 30 metres to reach the refuse collection point of the development. (It is important to note that the above suggestion would in no way meet with the Borough Councils own refuse collection design criteria with the submitted layout being more appropriate)

- 6.23 The application proposes 3 spaces to the front of the bungalow. 1 parking space would be provided to the front of both plots 2 and 3. A car barn for 2 vehicles to park and 2 further parking spaces would be provided to the front of plot 2. This would create 3 parking spaces each for plots 2 and 3. A double garage and 2 parking spaces would be provided to the front of plot 4. The double garage would be 6.1m in width. The DMP notes 'garages will only be counted as car parking spaces if they are a minimum of 3.5m by 6m. The average width of the top 20 cars in UK in 2018 was 1.96m. The minimum standard of a 3.5m wide garage would allow for 0.75m either side for door opening and circulation. 2 cars (2m x 2) + 0.75m to each side wall (0.75m x 2) + a minimum of 1m circulation between the cars would require a minimum of 6.5m in width for a garage to be considered to provide 2 parking spaces. In this instance the garage is therefore counted as 1 additional space. This would create 3 parking spaces for plot 4. The proposal also includes 2 visitor parking spaces. This brings the total number of spaces to 14.
- 6.24 The County Highways Authority have recommended a condition to secure parking restriction measures. Conditions are also recommended to secure vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking and turning areas shall be retained and maintained for their designated purposes. A condition is also recommended to require the development not to be occupied until a 1.8 metres wide footway has been provided on the east side of the access road north of the termination point of the western footway of the access road and the eastern footway shall extend to the parking spaces numbered 10 to 13 in accordance with a revised scheme to be submitted to and approved in writing with the Local Planning Authority
- 6.25 Finally conditions are recommended to secure a construction transport management plan and electric vehicle fast charge sockets

Impact on trees

- 6.26 The site contains and is bordered by mature trees protected by way of Tree Preservation Order ref: BAN114. The application has been supported by tree constraints plan, tree survey plan, arboricultural report. Following the submission of amended plans to show a reduction in the number of dwellings, the Tree Officer has commented the updated arboricultural report includes a shade assessment the off-site trees will have on each plot, which concludes the plots will not experience excessive shading during the day. The report acknowledges plot 2 will be shaded because of the buildings and not the trees, but the garden will be un shaded during the afternoon.
- 6.27 The distance between the off site trees and the side elevation of plot 2 has increased compared to the previous scheme but will require remedial works to be carried out to increase the distance between the canopy and building.
- 6.28 Therefore, based on the existing layout he raises no objection subject to recommended tree protection conditions being attached to the decision notice.

Ecology

- 6.29 The site is not subject to any designation to indicate a particular importance for nature conservation interests, but it does contain many trees and shrubs, some of which would be lost as a result of the development, as well as the demolition of No. 64. Policy NHE2 of the DMP expects in such locations without a particular designation that development proposals be designed, wherever possible, to achieve a net gain in biodiversity. A Preliminary Ecological Appraisal and preliminary inspection for bats, and a bat survey report has been submitted with the application. A dusk emergence survey was carried out on 17th May 2022.
- 6.30 At the time of the writing of the committee report, further information has been requested from the Applicant in regard to surveys of the site and screening assessments. Should the issues raised by Surrey Wildlife Trust be satisfactorily addressed prior to the Planning Committee meeting on 6th July 2022, this shall be updated within the addendum and conditions added to ensure the development is in accordance with local and national policy in this regard

Drainage and flooding

- 6.31 The site is in Flood Zone 1, however there have been known surface ater flood events in the locality. A condition is recommended to secure details of the proposed drainage for the site and to make use of SuDS so the development will not create an increased risk of flooding from surface water to the development site and the surrounding area and to ensure that the site is satisfactorily drained with regard to Development Management Plan policy CCF2 and National Planning Policy Framework 2019. Sewage capacity would be dealt with under Building Regulations.

Agenda Item 8

Planning Committee
6th July 2022

Agenda Item: 8
21/02108/F

Sustainable construction

- 6.32 DMP Policy CCF1 relates to climate change mitigation and requires new development to meet the national water efficiency standard of 110litres/person/day and to achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations. However the 2022 changes to Part L of the Building Regulations have taken effect from 15th June and provide a 30% improvement on emission rates so effectively superseding this policy requirement.
- 6.33 A condition is also recommended to ensure that each dwelling is fitted with access to fast broadband services in accordance with policy INF3 of the DMP.
- 6.34 Policy DES7 of the DMP requires that on sites of 5 or more homes at least 20% of homes should meet the Building Regulations requirements for 'accessible and adaptable dwellings'. The applicant has not referred to this requirement. Without any evidence to the contrary it is considered that such a requirement would be viable for the applicant and therefore a condition is recommended to secure adequate accessible housing in accordance with policy DES7.

Affordable Housing

- 6.35 Core Strategy Policy CS15 require financial contributions towards affordable housing to be provided on housing developments of 1-9 units. However, in November 2014, the Government introduced policy changes through a Written Ministerial Statement and changes to the national Planning Practice Guidance which restrict the use of planning obligations to secure affordable housing contributions from developments of 10 units or less. These changes were given legal effect following the Court of Appeal judgement in May 2016.
- 6.36 In view of this, and subsequent local appeal decisions which have afforded greater weight to the Written Ministerial Statement than the Council's adopted policy, the Council is not presently requiring financial contributions from applications such as this resulting in a net gain of 10 units or less. The absence of an agreed undertaking does not therefore warrant a reason for refusal in this case.

Community Infrastructure Levy (CIL)

- 6.37 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, road, public transport and community facilities which are needed to support new development. This development would be CIL liable and, although the exact amount would be determined and collected after the grant of planning permission, an informal calculation shows a CIL liability of around £93,000.

Infrastructure Contributions

- 6.38 In terms of other contributions and planning obligations, The Community Infrastructure Levy (CIL) Regulations were introduced in April 2010 which state that it is unlawful to take a planning obligation into account unless its requirements are (i) relevant to planning; (ii) necessary to make the proposed development acceptable in planning terms; and (iii) directly related to the proposed development. As such only contributions, works or other obligations that are directly required as a consequence of development can be requested and such requests must be fully justified with evidence. In this case, no such contributions or requirements have been requested or identified. Accordingly, any request for an infrastructure contribution would be contrary to CIL Regulation 122.

Other matters

- 6.39 Objection was raised on the basis of the relocation of streetlighting and a telegraph pole. This would be a matter for the Highways Authority at Surrey County Council and British Telecom.
- 6.40 Objection was raised on the grounds of harm to Green Belt/countryside and a Conservation Area. The site is not within nor adjacent to either of these designations and is not considered to result in a harmful impact in this regard.

CONDITIONS

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan Type	Reference	Version	Date Received
Proposed Plans	211413/TS/01	A	05.10.2021
Proposed Plans	211413/TS/02	A	05.10.2021
Site Layout Plan	21008-A-PL-042	A	08.02.2022
Block Plan	21008-A-PL-031	B	02.02.2022
Site Layout Plan	21008-A-PL-033	B	02.02.2022
Location Plan	21008-A-PL-045	B	02.02.2022
Street Scene	21008-A-PL-039	B	02.02.2022
Site Layout Plan	21008-A-PL-032	B	02.02.2022
Proposed Plans	21008-A-PL-035	B	02.02.2022
Street Scene	21008-A-PL-040	B	02.02.2022
Proposed Plans	21008-A-PL-036	B	02.02.2022

Agenda Item 8

Planning Committee
6th July 2022

Agenda Item: 8
21/02108/F

Proposed Plans	21008-A-PL-037	B	02.02.2022
Other Plan	21008-A-PL-012	B	02.02.2022
Section Plan	21008-A-PL-038	B	02.02.2022
Block Plan	21008-A-PL-044	B	02.02.2022
Proposed Plans	21008-A-PL-034	A	07.02.2022
Arboricultural Plan	TCP 01		09.08.2021
Location Plan	21008-A-PL-001	A	02.08.2021
Combined Plan	21008-A-PL-014	A	02.08.2021

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

3. No development shall take place until the developer obtains the Local Planning Authority's written approval of details of both existing and proposed ground levels and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

Reason: To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Reigate and Banstead Development Management Plan DES1.

4. No development shall commence until a Construction Transport Management Plan, to include details of:
- (a) parking for vehicles of site personnel, operatives and visitors
 - (b) loading and unloading of plant and materials
 - (c) storage of plant and materials
 - (d) programme of works (including measures for traffic management)
 - (e) provision of boundary hoarding behind any visibility zones
 - (f) vehicle routing
 - (g) measures to prevent the deposit of materials on the highway
 - (h) before and after construction condition surveys of the highway and a commitment to fund the repair of any damage caused
 - (i) on-site turning for construction vehicles
- has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and DES8 Construction Management of the Reigate and Banstead Local Plan Development Management Plan September 2019.

5. No development shall commence until a Construction Management Statement, to include details of:
- a) Prediction of potential impacts with regard to water, waste, noise and vibration, dust, emissions and odours, wildlife. Where potential impacts are identified, mitigation measures should be identified to address these impacts.

- b) Information about the measures that will be used to protect privacy and the amenity of surrounding sensitive uses; including provision of appropriate boundary protection.
- c) Means of communication and liaison with neighbouring residents and businesses.
- d) Hours of work.

Has been submitted to and improved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The condition above is required in order that the development is managed in a safe and considerate manner to help mitigate potential impact on the amenity and safety of neighbours and to accord with Reigate and Banstead Development Management Plan 2019 policy DES8.

6. Notwithstanding the submitted plan numbered 21008 A PL 012 Rev B the development shall not be commenced until the proposed belmouth vehicular access to Shelveys Way and the first 20 metres of the new access road have been provided with, parking restrictions in accordance with a revised scheme to be submitted to and approved in writing with the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

7. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plan numbered 21008 A PL 012 Rev B for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking and turning areas shall be retained and maintained for their designated purposes.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

8. The development hereby approved shall not be occupied unless and until each of the proposed dwellings are provided with a fast charge socket (current minimum requirements - 7 kw Mode 3 with Type 2 connector - 230v AC 32 Amp single phase dedicated supply) in accordance with a scheme to be submitted and approved in writing by the Local Planning Authority and thereafter retained and maintained to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other

Agenda Item 8

Planning Committee
6th July 2022

Agenda Item: 8
21/02108/F

highway users, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

9. Notwithstanding the submitted plan 21008 A PL 012 Rev B the development shall not be occupied until a 1.8 metres wide footway has been provided on the east side of the access road north of the termination point of the western footway of the access road and the eastern footway shall extend to the parking spaces numbered 10 to 13 in accordance with a revised scheme to be submitted to and approved in writing with the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

10. No development shall commence including demolition and or groundworks preparation until a detailed, scaled finalised Tree Protection Plan (TPP) and the related finalized Arboricultural Method Statement (AMS) is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas of trees (RPA) shown to scale on the TPP, including the installation of service routings, type of surfacing for the entrance drive and location of site offices. The AMS shall also include a pre commencement meeting, supervisory regime for their implementation & monitoring with an agreed reporting process to the LPA. All works shall be carried out in strict accordance with these details when approved.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and to ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3, DES1 and DES3 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard 5837:2012 Trees in relation to design, demolition and construction

11. No development, groundworks or demolition processes shall be undertaken until an agreed scheme of supervision for the arboricultural protection measures have been submitted to and approved in writing by the local planning authority. The pre commencement meeting, supervision and monitoring shall be undertaken in accordance with these approved details. The submitted details shall include.
 1. Pre commencement meeting between the retained arboricultural consultant, local planning authority Tree Officer and individuals and personnel responsible for the implementation of the approved development

2. Timings, frequency of the supervision and monitoring regime and an agreed reporting process to the local planning authority.
3. The supervision monitoring and reporting process shall be undertaken by a qualified arboriculturist.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and policy NHE3 of the Development Management Plan.

12. No development shall commence on site until a scheme for the soft and hard landscaping (including hard surfacing and any street furniture), including details of existing landscape features to be retained or pruned, has been submitted and approved in writing by the local planning authority. The landscaping scheme shall include details of hard landscaping, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to first occupation of the approved development or in accordance with a programme agreed in writing with the local planning authority

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837. Trees in relation to construction.

Any trees shrubs or plants planted or any existing plants/hedging retained in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason: To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and Meath Green Conservation Area, and to comply with Reigate and Banstead Borough Development Management Plan 2019 policies NHE3 and DES1, British Standards including BS8545:2014 and British Standard 5837:2012.

13. No development shall commence until a strategy for the disposal of surface and foul water is submitted to and approved in writing by the Local Planning Authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDs, NPPF and Ministerial Statement on SuDs The works shall be carried out in accordance with the approved details.

Agenda Item 8

Planning Committee
6th July 2022

Agenda Item: 8
21/02108/F

Reason: To ensure that the site is satisfactorily drained and in order to protect water and environmental quality with regard to Policy CS10 of the Core Strategy 2014, Policy CCF2 of the Development Management Plan 2019 and the NPPF.

14. Prior to the first occupation of the development a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority. This must demonstrate that the drainage system has been constructed as per the agreed scheme, or detail any minor variations, it must provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/area, flow restriction devices and outfalls).

The drainage system shall therefore be retained and maintained in accordance with the agreed details.

Reason: To ensure the drainage system is constructed to the national Non-Statutory Technical Standards for SuDs in order to mitigate against the risk of surface water flooding with regard to policy INF1 and CCF2 of the Reigate and Banstead Development Management Plan 2019.

15. No development shall take place above slab level until written details of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

16. No development above slab level shall take place until details setting out how the applicant will ensure that at least 20%, unless otherwise agreed in writing, of the homes meet the Building Regulations requirements for 'accessible and adaptable dwellings' have been submitted to and agreed in writing by the Local Planning Authority. The development shall be implemented in accordance with the agreed details.

Reason: In order that the scheme provides accessible housing in accordance with Reigate and Banstead Development Management Plan 2019 policy DES7.

17. The development shall not be occupied until a plan indicating the positions, design, materials, and type of boundary treatment to be erected has been submitted to and approved in writing by the Local Planning Authority. The boundary treatment shall be completed before the occupation of the development hereby permitted.

Reason: To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the Reigate and Banstead Development Management Plan 2019 policy DES1 and NHE3

18. Prior to the first occupation of the development full details (and plans where appropriate) of the waste management storage and collection points, (and pulling distances where applicable), throughout the development shall be submitted to and approved in writing by the Local Planning Authority.

All waste storage and collection points should be of an adequate size to accommodate the bins and containers required for the dwelling(s) which they are intended to serve in accordance with the Council's guidance contained within Making Space for Waste Management in New Development.

Each dwelling shall be provided with the above facilities in accordance with the approved details prior to occupation of the relevant dwellings.

Reason: To provide adequate waste facilities in the interests of the amenities of the area and to encourage recycling in accordance with the Development Management Plan 2019 policy DES1.

19. The development hereby approved shall not be first occupied unless and until an Energy and Water Efficiency Statement has been submitted to and approved in writing by the Local Planning Authority. The Statement shall detail how the development will:
- a) Ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day
 - b) Achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations

The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

20. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:
- a) A broadband connection accessed directly from the nearest exchange or cabinet
 - b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Agenda Item 8

Planning Committee
6th July 2022

Agenda Item: 8
21/02108/F

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

21. The first floor windows in the side elevations of the development hereby permitted shall be glazed with obscured glass which shall be fixed shut, apart from a top hung opening fanlight whose cill height shall not be less than 1.7 metres above internal floor level, and shall be maintained as such at all times.

Reason: To ensure that the development does not affect the amenity of the neighbouring property by overlooking with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.info.
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : [Climate Change Information](#).
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, to contact the Council's Neighbourhood Services team to confirm the number and specification of recycling and refuse bins that are required to be supplied by the developer. All developer enquires on recycling and refuse bin ordering, collections and discussing waste matters is via our department email address RC@reigate-banstead.gov.uk . Please also note our website area for developers https://www.reigate-banstead.gov.uk/info/20062/recycling_and_refuse/392/fees_for_recycling_and_refuse_services/3.
4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
 - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;

- (e) There should be no burning on site;
- (f) Only minimal security lighting should be used outside the hours stated above; and
- (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - www.ccscheme.org.uk/index.php/site-registration.

5. The applicant is advised that the essential requirements for an acceptable communication plan forming part of a Method of Construction Statement are viewed as: (i) how those likely to be affected by the site's activities are identified and how they will be informed about the project, site activities and programme; (ii) how neighbours will be notified prior to any noisy/disruptive work or of any significant changes to site activity that may affect them; (iii) the arrangements that will be in place to ensure a reasonable telephone response during working hours; (iv) the name and contact details of the site manager who will be able to deal with complaints; and (v) how those who are interested in or affected will be routinely advised regarding the progress of the work. Registration and operation of the site to the standards set by the Considerate Constructors Scheme (<http://www.ccscheme.org.uk/>) would help fulfil these requirements.
6. The applicant is advised that the Borough Council is the street naming and numbering authority and you will need to apply for addresses. This can be done by contacting the Address and Gazetteer Officer prior to construction commencing. You will need to complete the relevant application form and upload supporting documents such as site and floor layout plans in order that official street naming and numbering can be allocated as appropriate. If no application is received the Council has the authority to allocate an address. This also applies to replacement dwellings. If you are building a scheme of more than 5 units please also supply a CAD file (back saved to 2010) of the development based on OS Grid References. Full details of how to apply for addresses can be found [http://www.reigatebanstead.gov.uk/info/20277/street naming and numberin g](http://www.reigatebanstead.gov.uk/info/20277/street_naming_and_numbering)
7. The use of a suitably qualified arboricultural consultant is essential to provide acceptable submissions in respect of the arboricultural tree condition above. All works shall comply with the recommendations and guidelines contained within British Standard 5837.
8. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above landscaping condition.

Agenda Item 8

Planning Committee
6th July 2022

Agenda Item: 8
21/02108/F

The planting of trees and native hedging shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate structural landscape trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Advanced Nursery Stock sizes with initial planting heights of not less than 4.5m with girth measurements at 1m above ground level in excess of 16/18cm.

9. The applicant is advised that the development should seek to achieve standards contained within the Secured by Design award scheme to ensure that it results in a safe development.
10. Notwithstanding any permission granted under the Planning Acts, no signs, devices or other apparatus may be erected within the limits of the highway without the express approval of the Highway Authority. It is not the policy of the Highway Authority to approve the erection of signs or other devices of a non-statutory nature within the limits of the highway.
11. The permission hereby granted shall not be construed as authority to obstruct the public highway by the erection of scaffolding, hoarding or any other device or apparatus for which a licence must be sought from the Highway Authority Local Highways Service.
12. The permission hereby granted shall not be construed as authority to carry out any works on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a permit and, potentially, a Section 278 agreement must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works on the highway will require a permit and an application will need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road.

Please see <http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme>. The applicant is also advised that Consent may be required under Section 23 of the Land Drainage Act 1991. Please see www.surreycc.gov.uk/people-and-community/emergency-planning-andcommunity-safety/flooding-advice.

13. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
14. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of

any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.

15. The developer is advised that as part of the detailed design of the highway works required by the above condition(s), the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
16. The Highway Authority has no objection to the proposed development, subject to the above conditions but, if it is the applicant's intention to offer any of the roadworks included in the application for adoption as maintainable highways, permission under the Town and Country Planning Act should not be construed as approval to the highway engineering details necessary for inclusion in an Agreement under Section 38 of the Highways Act 1980. Further details about the post-planning adoption of roads may be obtained from the Transportation Development Planning Division of Surrey County Council.
17. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to: <http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.
18. The developer is advised that that the parking restrictions required by the above access condition would need to be approved and advertised through Surrey County Council and then provided by the developer. The aforementioned is all at the developer's own expense.

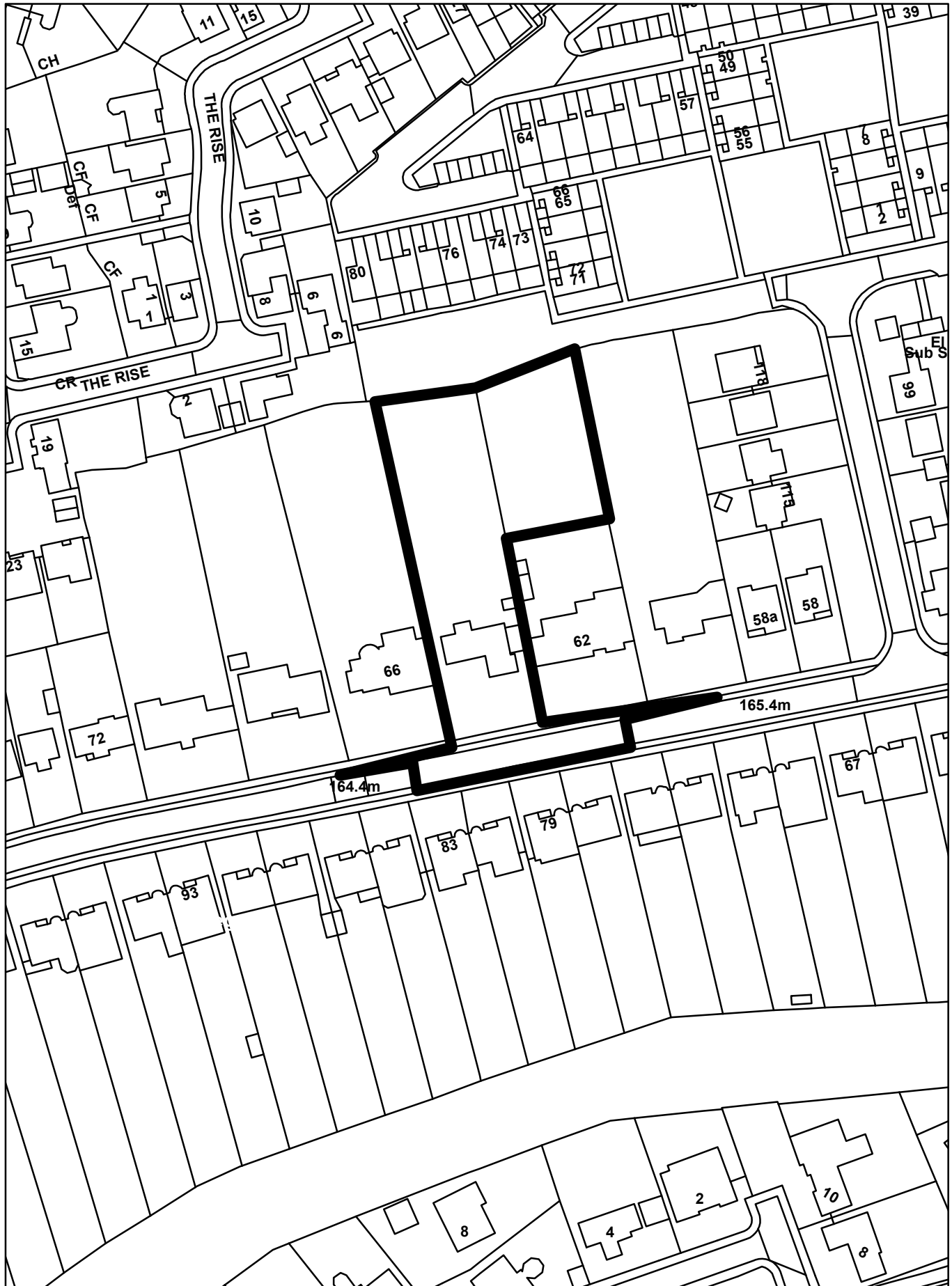
REASON FOR PERMISSION

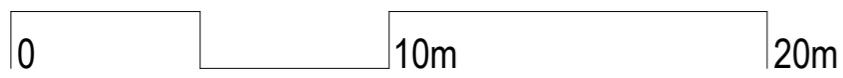
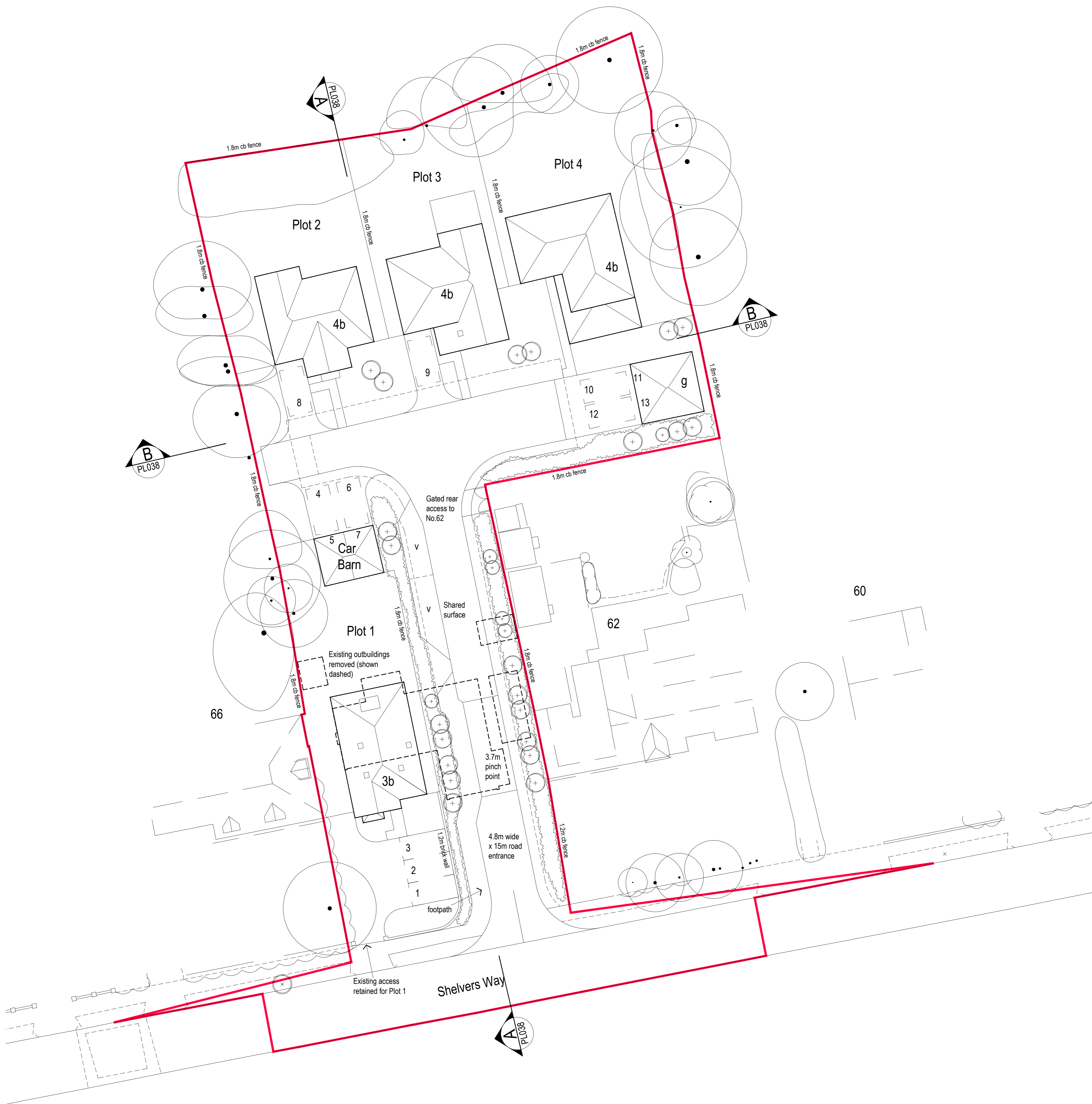
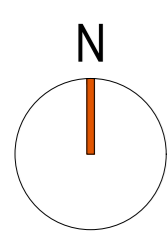
The development hereby permitted has been assessed against development plan policies DES1, DES4, DES5, DES8, DES9, NHE2, NHE3, NHE4, EMP4, TAP1, CCF1, CFF2 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

Proactive and Positive Statements

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

Agenda Item 8
21/02108/F 64 & Rear Of 62 Shelvers Way
Tadworth KT20 5QF





ACCOMMODATION SCHEDULE	
Application Site Area =2998 sq.m / 0.30 hectares / 0.74 acres	
PLOT / TYPE	QUANTITY
Plot 1- 3 Bedroom Chalet Bungalow (124.58 sq.m / 1341 sq.ft)	1
Plot 2- 4 Bedroom House (167.28sq.m / 1800 sq.ft)	1
Plot 3- 4 Bedroom Houses (192.55 sq.m / 2073 sq.ft)	1
Plot 4- 4 Bedroom Houses (196.65 sq.m / 2117 sq.ft)	1
TOTAL	4
Parking	13 spaces+ 2 visitor spaces

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MODEL FILE REFERENCE - 21008-A-P-PL

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B Revised Scheme- Planning issue
 A Revised Scheme- Planning issue
 rev. description

2022.02.01
 2021.12.20
 date

AJC dp
 AJC dp
 by ch

scale: 1:200 @ A1

client: Devine Homes Plc

date: December 2021

project: 62 to 64 Shelvers Way, Tadworth. KT20 5QF.

drawn: AJC

drawing: Proposed Site Layout

checked: dp

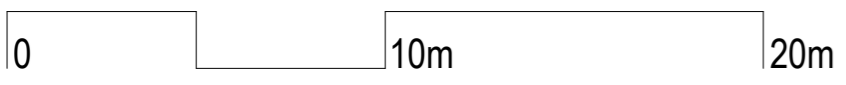
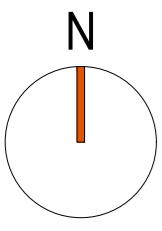
status: PLANNING

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drawing no.: 21008-A-PL-032

revision: B



ACCOMMODATION SCHEDULE	
Application Site Area = 2998 sq.m / 0.30 hectares / 0.74 acres	
PLOT / TYPE	QUANTITY
Plot 1- 3 Bedroom Chalet Bungalow (124.58 sq.m / 1341 sq.ft)	1
Plot 2- 4 Bedroom House (167.28sq.m / 1800 sq.ft)	1
Plot 3- 4 Bedroom Houses (192.55 sq.m / 2073 sq.ft)	1
Plot 4- 4 Bedroom Houses (196.65 sq.m / 2117 sq.ft)	1
TOTAL	4
Parking	13 spaces+ 2 visitor spaces

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Revised Scheme- Planning issue
 A Revised Scheme- Planning issue
 rev. description

2022.02.01
 2021.12.20
 date

AJC dp
 AJC dp
 by ch

scale: 1:200 @ A1
 date: December 2021
 drawn: AJC
 checked: dp

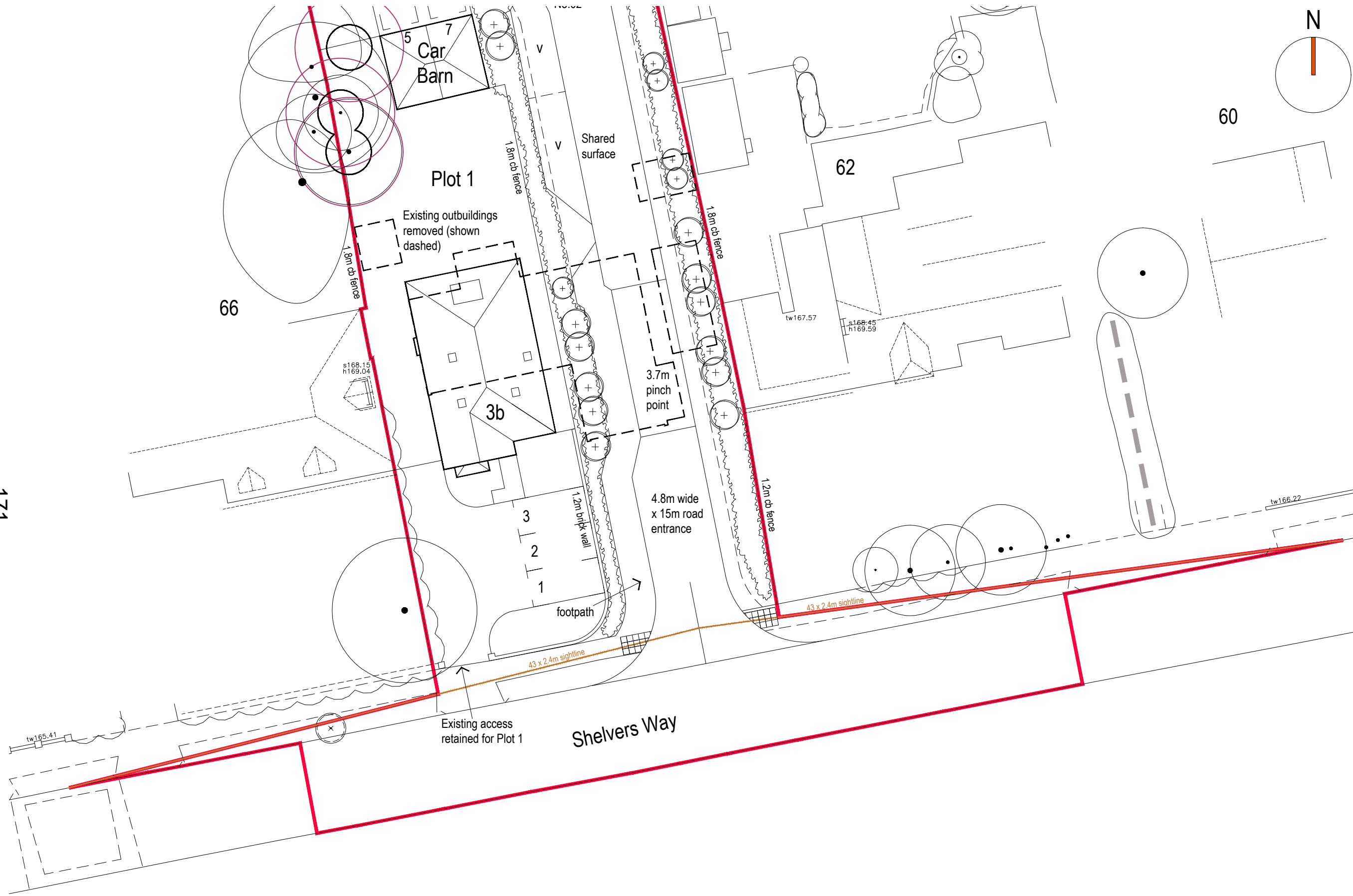
client: Devine Homes Plc
 project: 62 to 64 Shelters Way, Tadworth. KT20 5QF.
 drawing: Proposed Site Layout (coloured)
 status: **PLANNING**

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drawing no.: **21008-A-PL-033** revision: **B**

171



scale:	1:250 @ A3	client:	Devine Homes Plc
date:	December 2021	project:	62 to 64 Shelvers Way, Tadworth, KT20 5QF.
drawn:	AJC	drawing:	Sight Lines Plan
checked:	dp	status:	PLANNING

rev	A	description	Revised Scheme- Planning Issue	date	2021.12.20	by	AJC	dp	ch.
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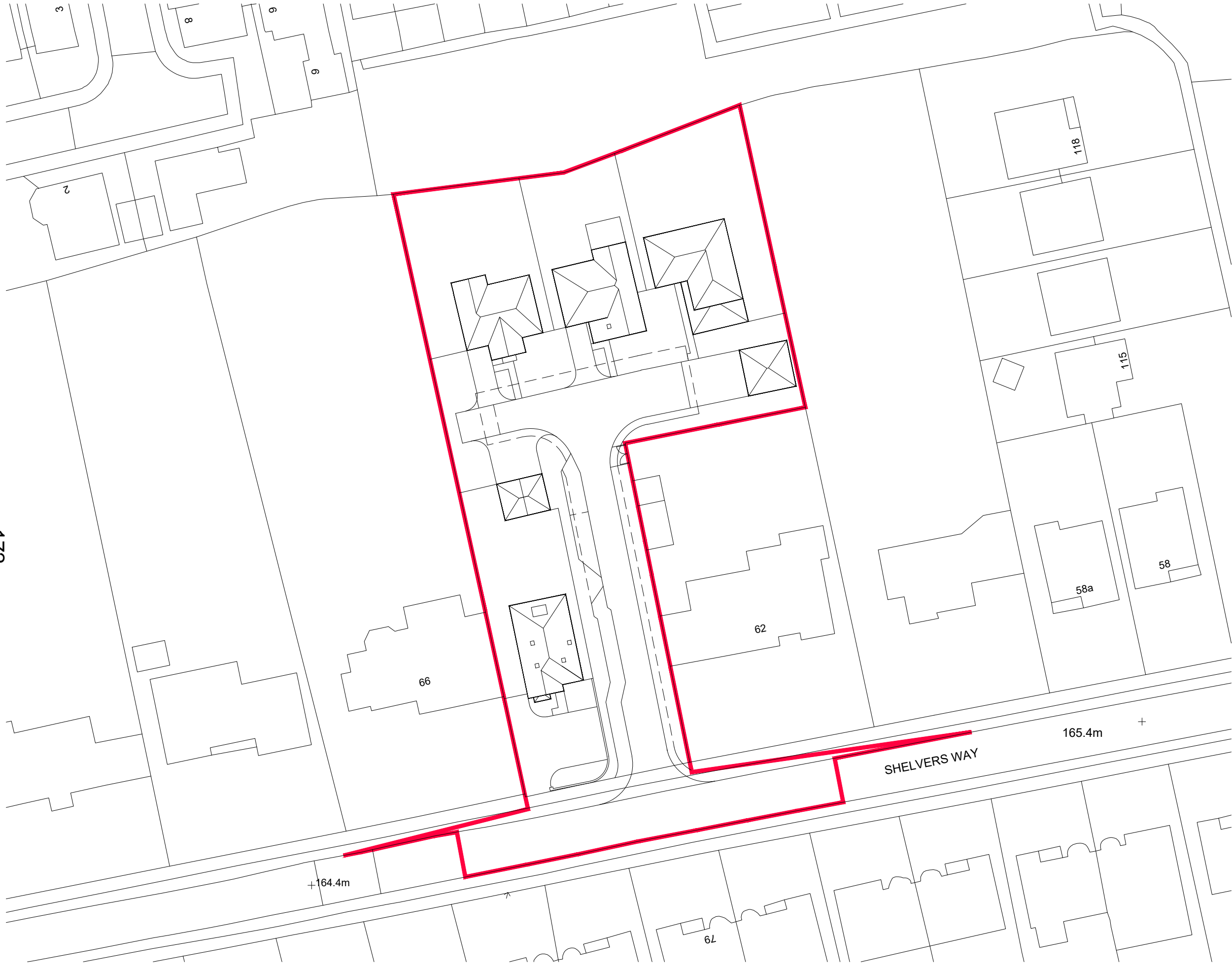
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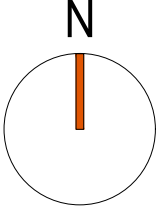
MODEL FILE REFERENCE: 21008-A-PL-042

Agenda Item 8

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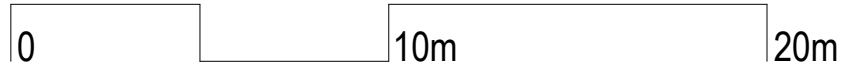
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date:	December 2021	project:	62 to 64 Shelveys Way, Tadworth, KT20 5QF.
drawn:	AJC	drawing:	Block Plan
checked:	dp	status:	PLANNING

rev	description	date	by	ch.
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A	Revised Scheme- Planning Issue	2021.12.20	AJC	dp

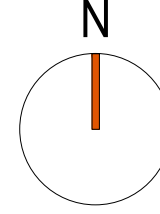
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Datum - 160.00



SECTION BB



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Agenda Item 8

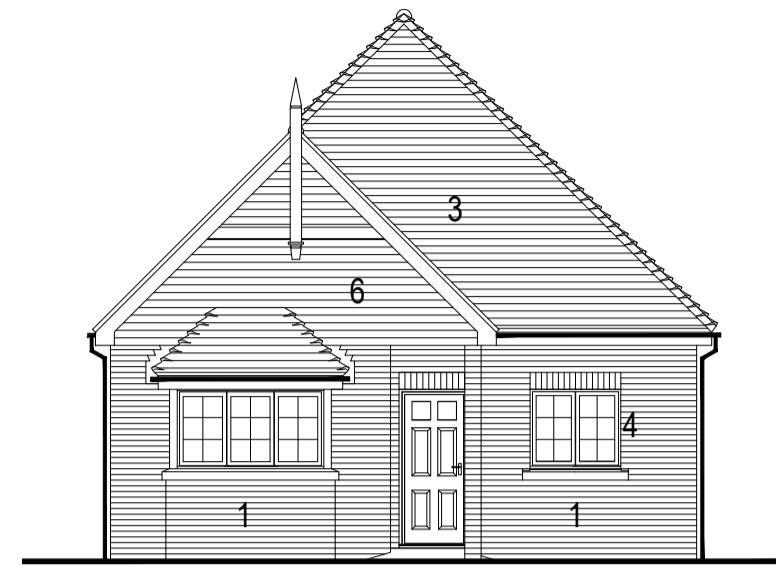
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A	Revised Scheme- Planning issue	2021.12.20	A.J.C	dp

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date:	December 2021	project:	62 to 64 Shelveys Way, Tadworth, KT20 5QF.
drawn:	A.J.C	drawing:	Internal Streetscene (coloured)
checked:	dp	status:	PLANNING

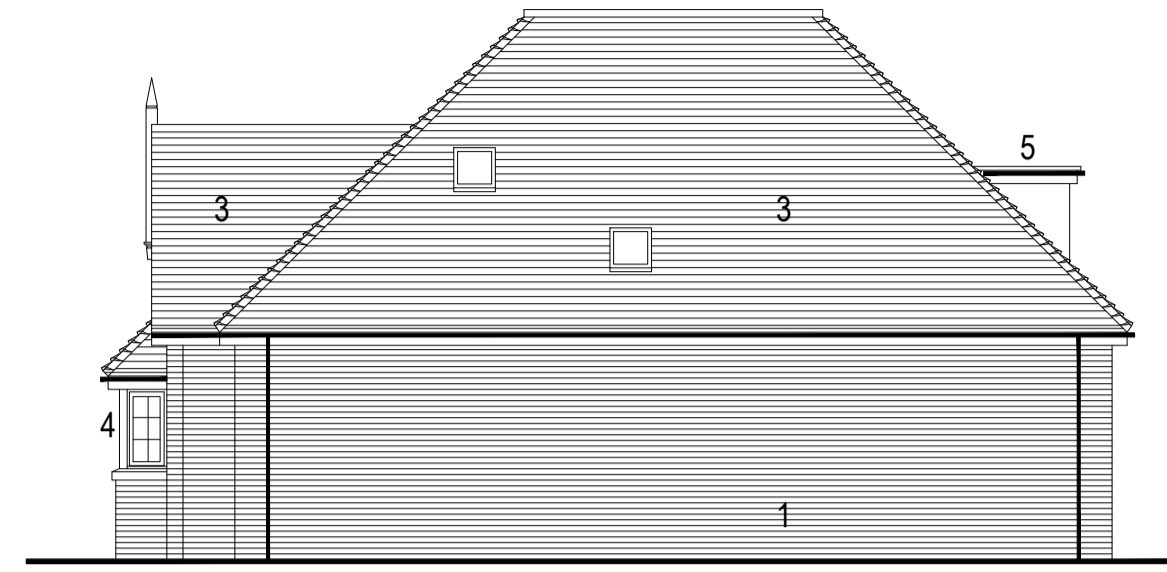
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drawing No: **21008-A-PL-039** revision: **B**



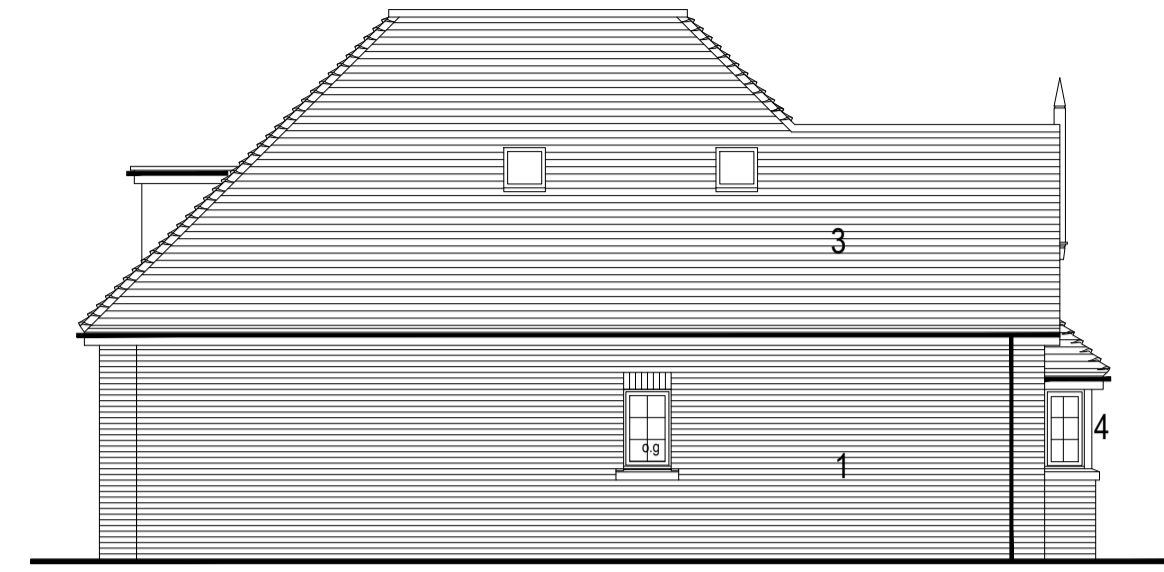
SOUTH ELEVATION



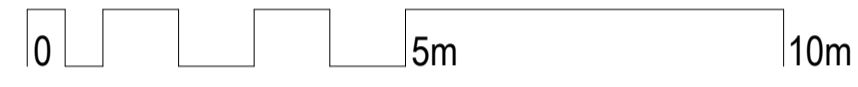
EAST ELEVATION



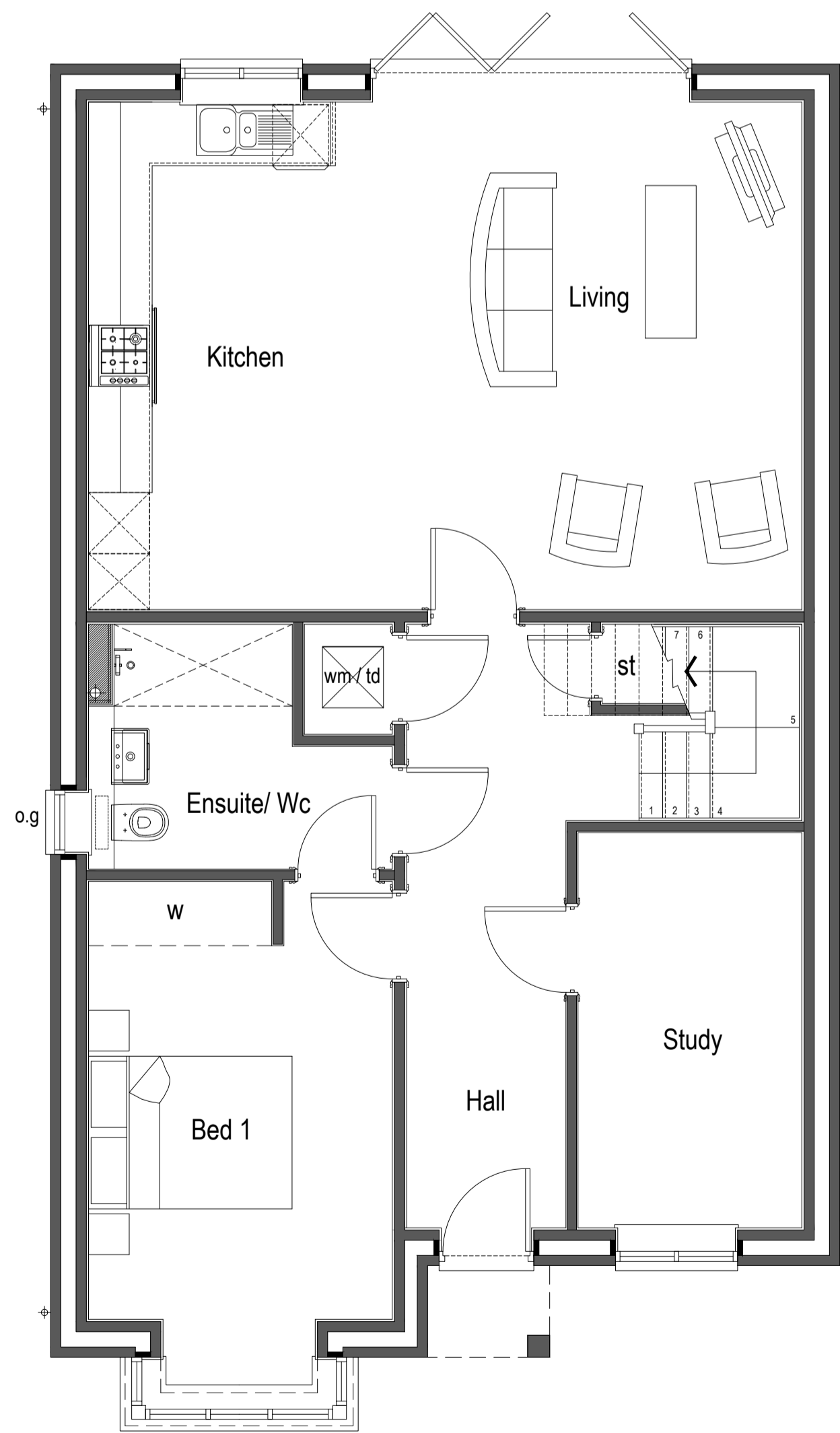
NORTH ELEVATION



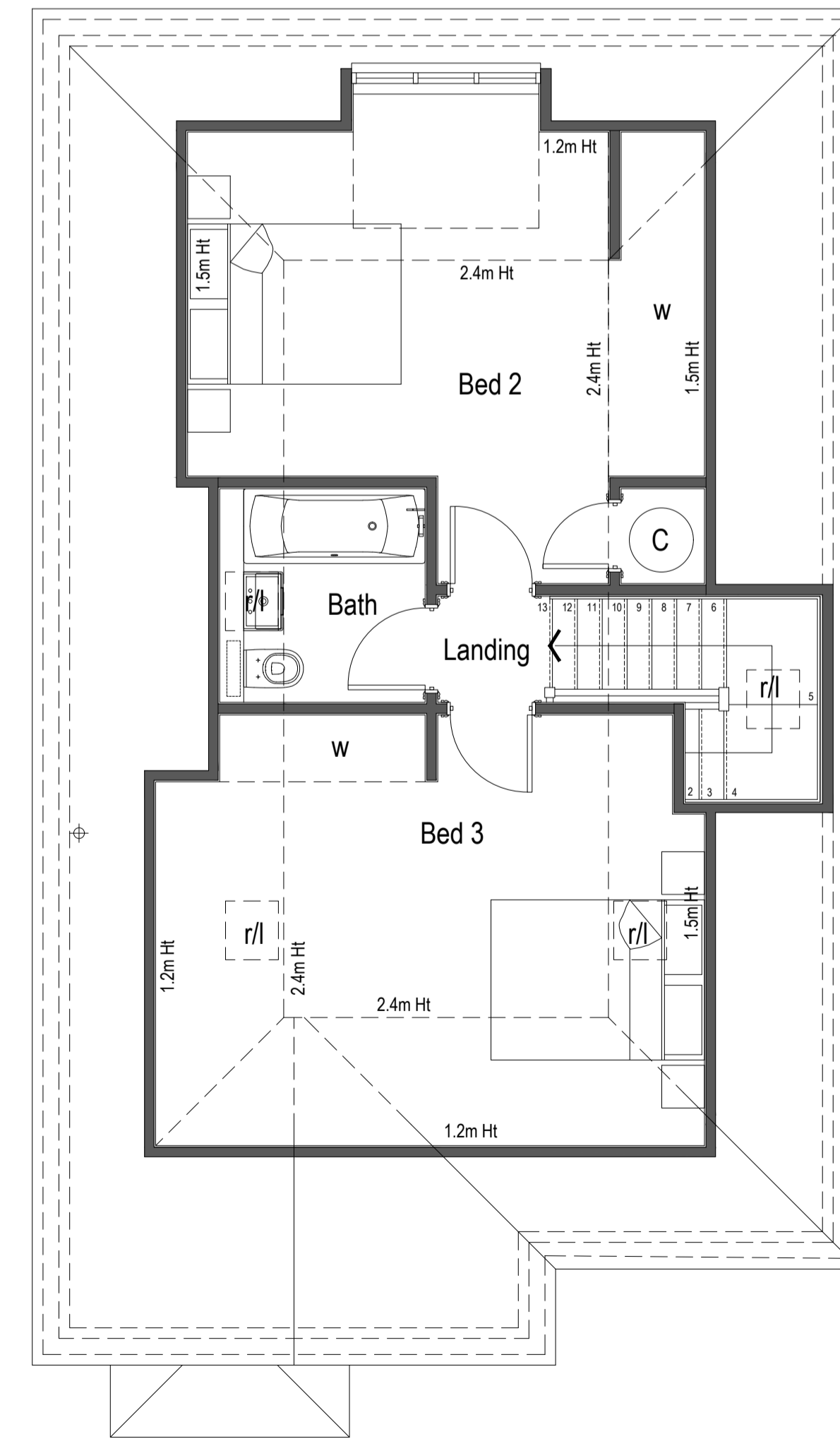
WEST ELEVATION



KEY TO MATERIALS	
1	Facing brickwork
2	Render
3	Plain tiles
4	uPVC windows and doors
5	Single ply membrane
6	Plain tile hanging
7	Timber boarding



GROUND FLOOR PLAN



FIRST FLOOR PLAN

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A Revised Scheme- Planning Issue
 rev. description

2021.12.20
 date

AJC dp
 by

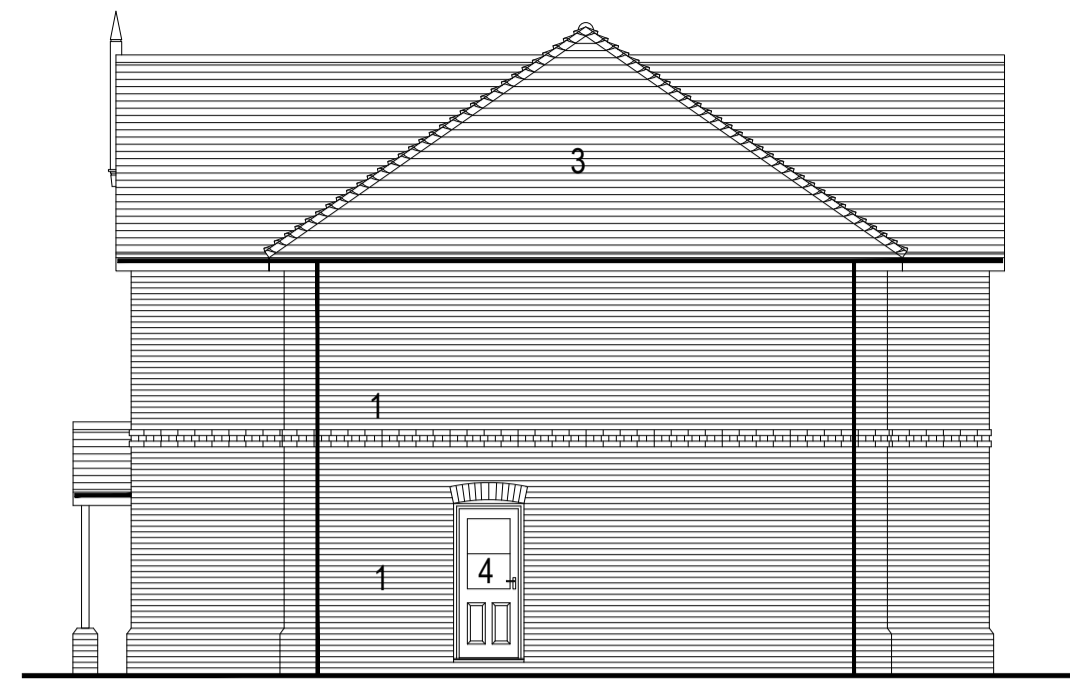
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status: PLANNING

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 drawing: AJC drawing Plot 1 Floor Plans and Elevations
 checked: dp



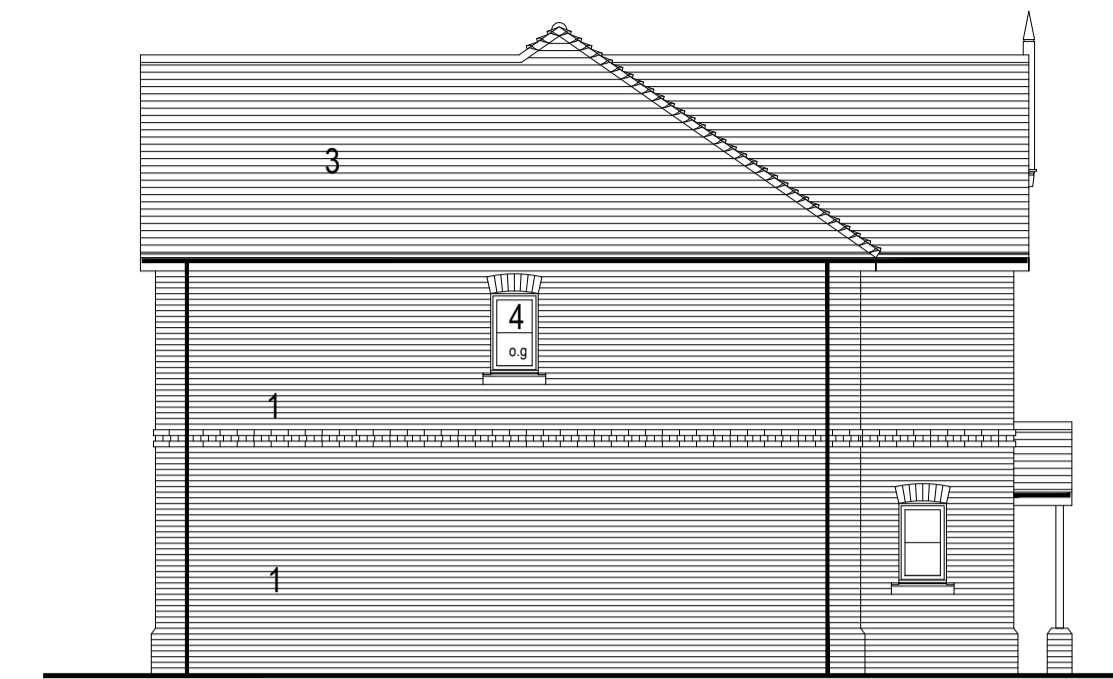
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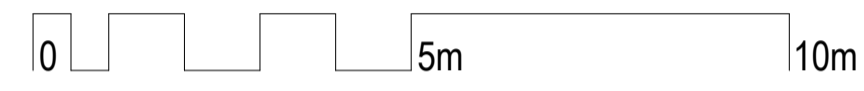
EAST ELEVATION



NORTH ELEVATION



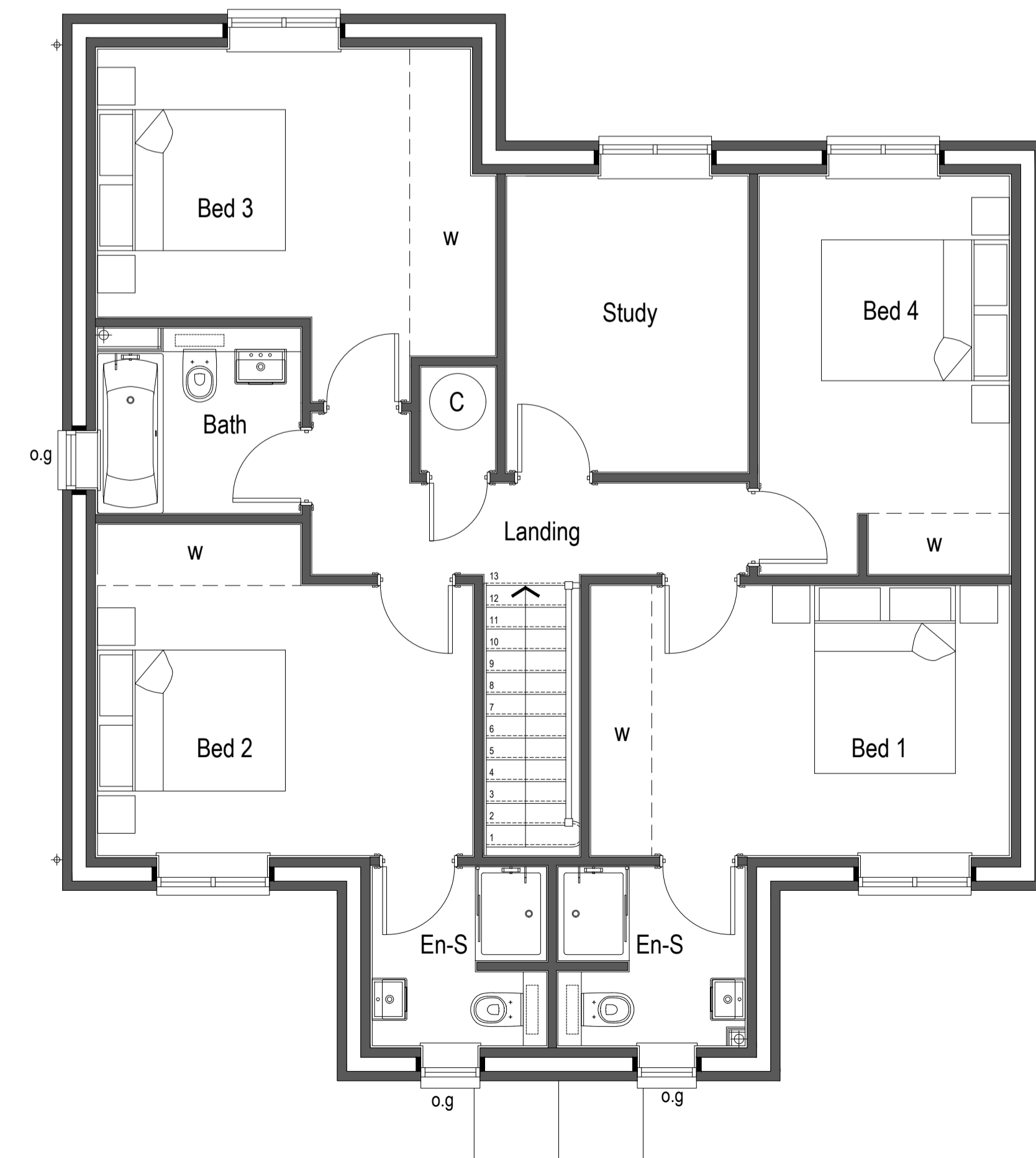
WEST ELEVATION



KEY TO MATERIALS	
1	Facing brickwork
2	Render
3	Plain tiles
4	uPVC windows and doors
5	Single ply membrane
6	Plain tile hanging
7	Timber boarding



GROUND FLOOR PLAN



FIRST FLOOR PLAN

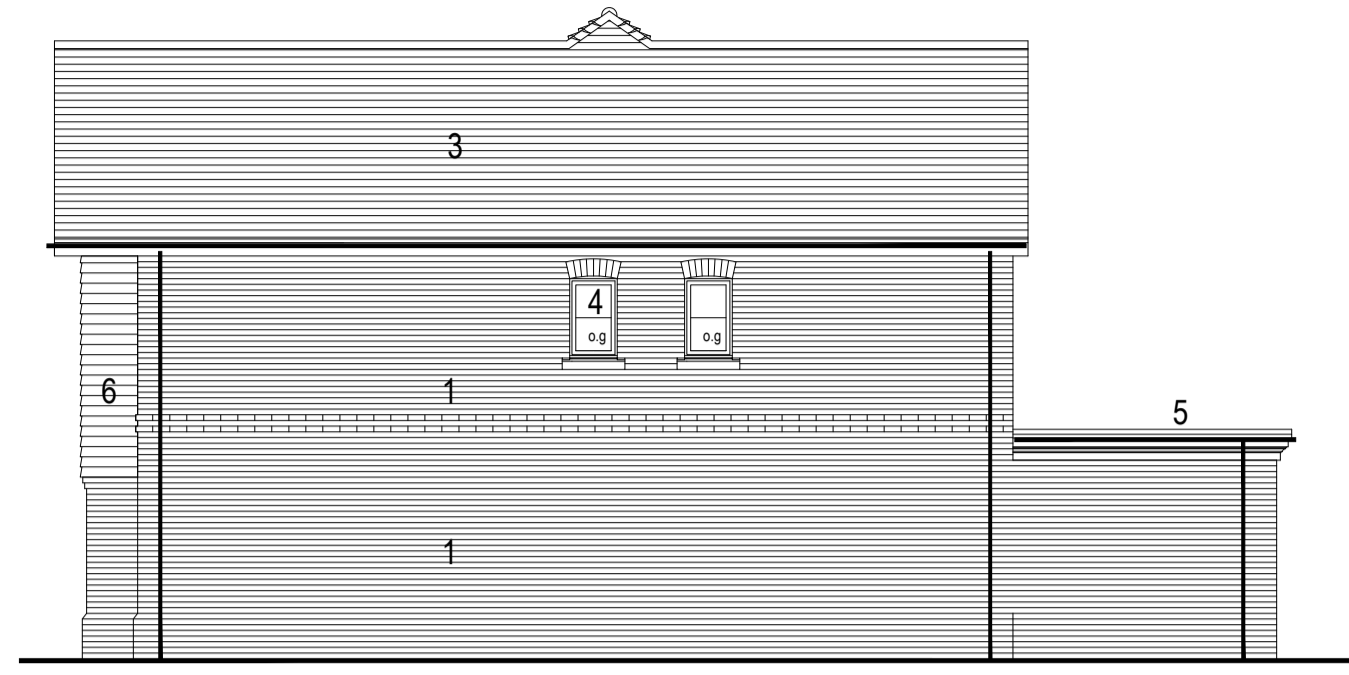
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rev.	description	date	by	ch
B	Revised Scheme- Planning Issue	2022.02.01	AJC	dp
A	Revised Scheme- Planning Issue	2021.12.20	AJC	dp

scale: 1:50 / 100 @ A1
 client: Devine Homes Pic
 project: 62 to 64 Shelvers Way, Tadworth, KT20 5QF.
 date: December 2021
 drawing: Plot 2 Floor Plans and Elevations
 drawn: AJC
 checked: dp
 status: PLANNING



SOUTH ELEVATION



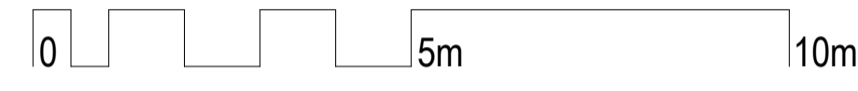
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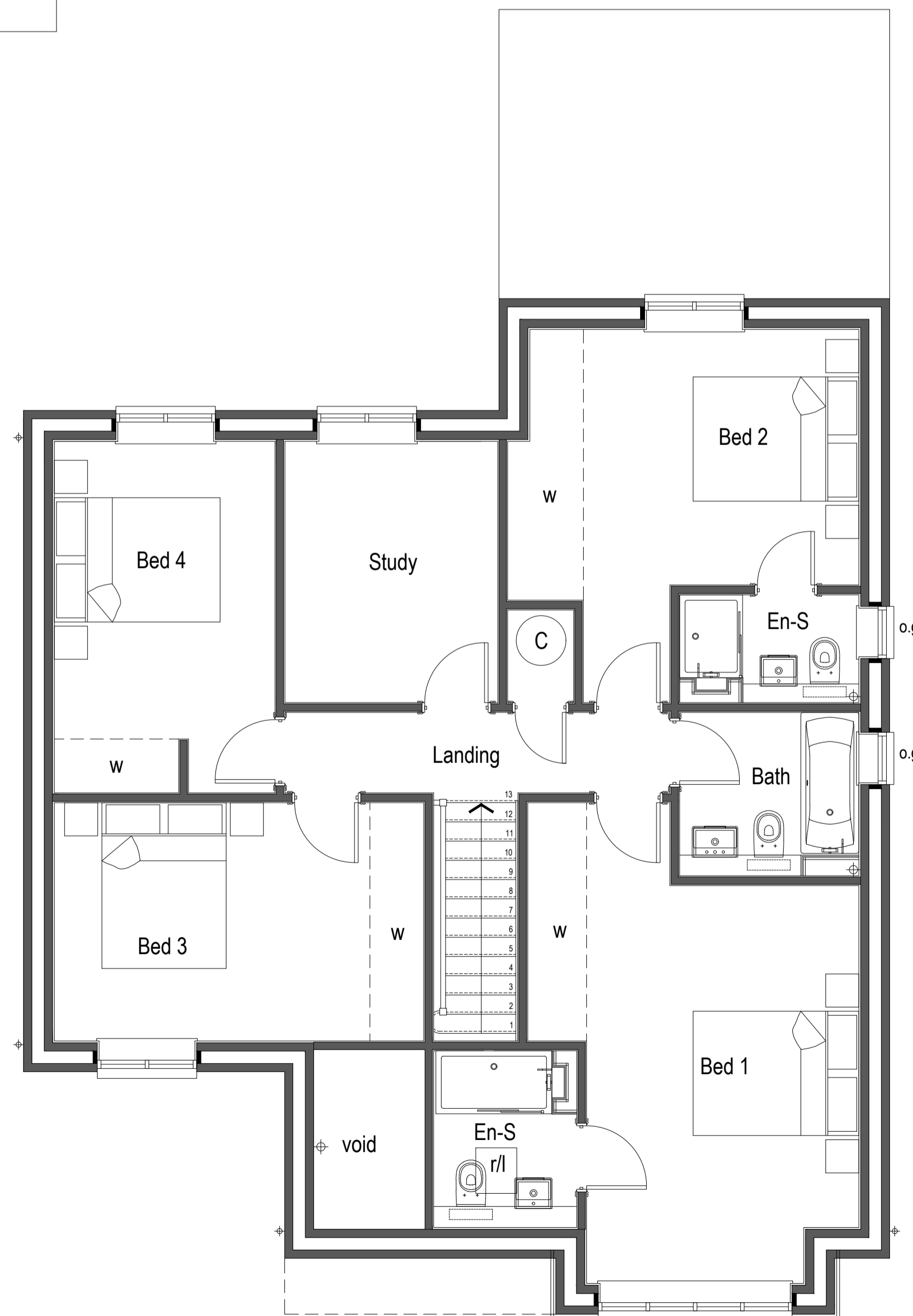
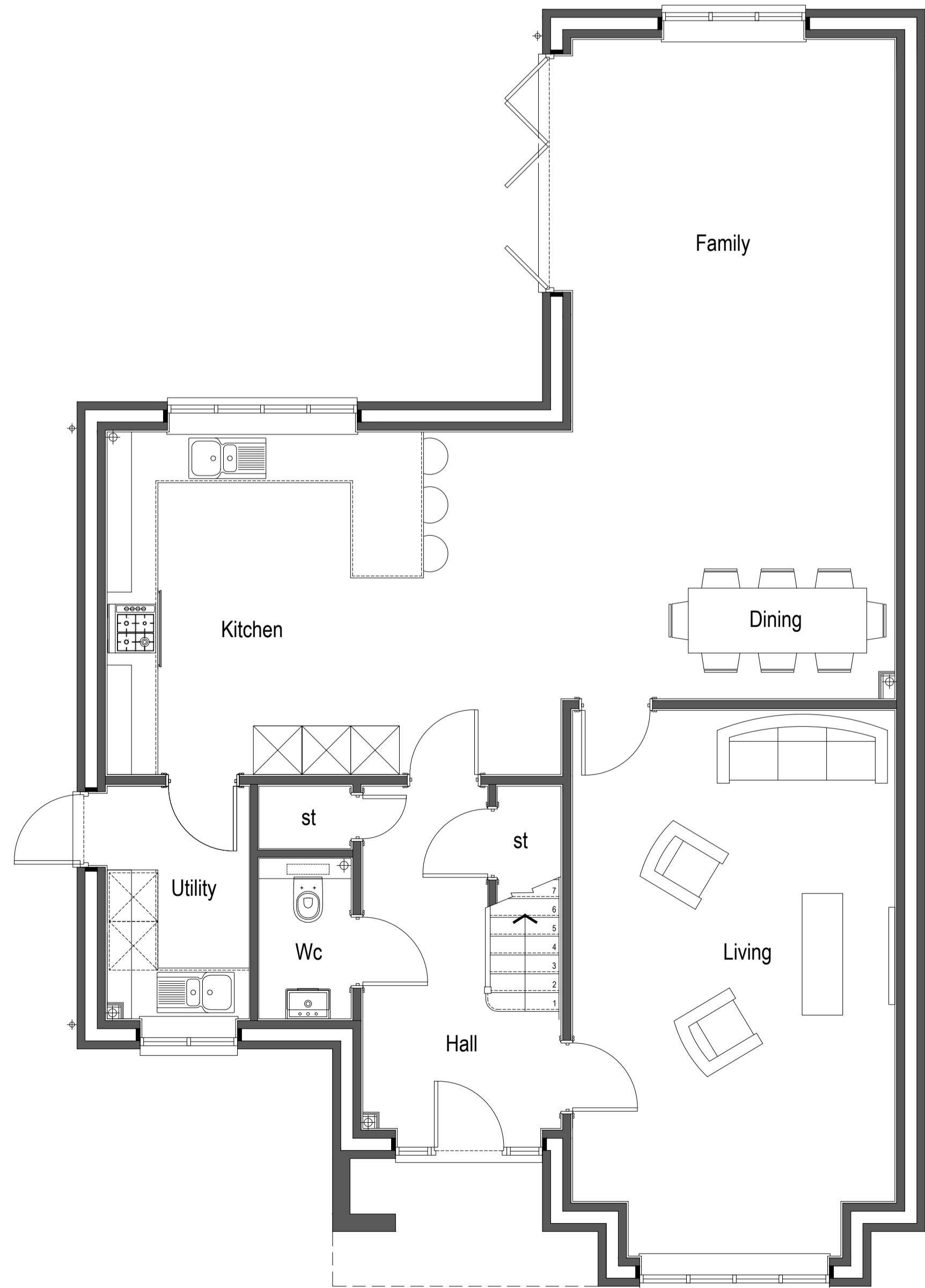
NORTH ELEVATION



WEST ELEVATION



KEY TO MATERIALS	
1	Facing brickwork
2	Render
3	Plain tiles
4	uPVC windows and doors
5	Single ply membrane
6	Plain tile hanging
7	Timber boarding



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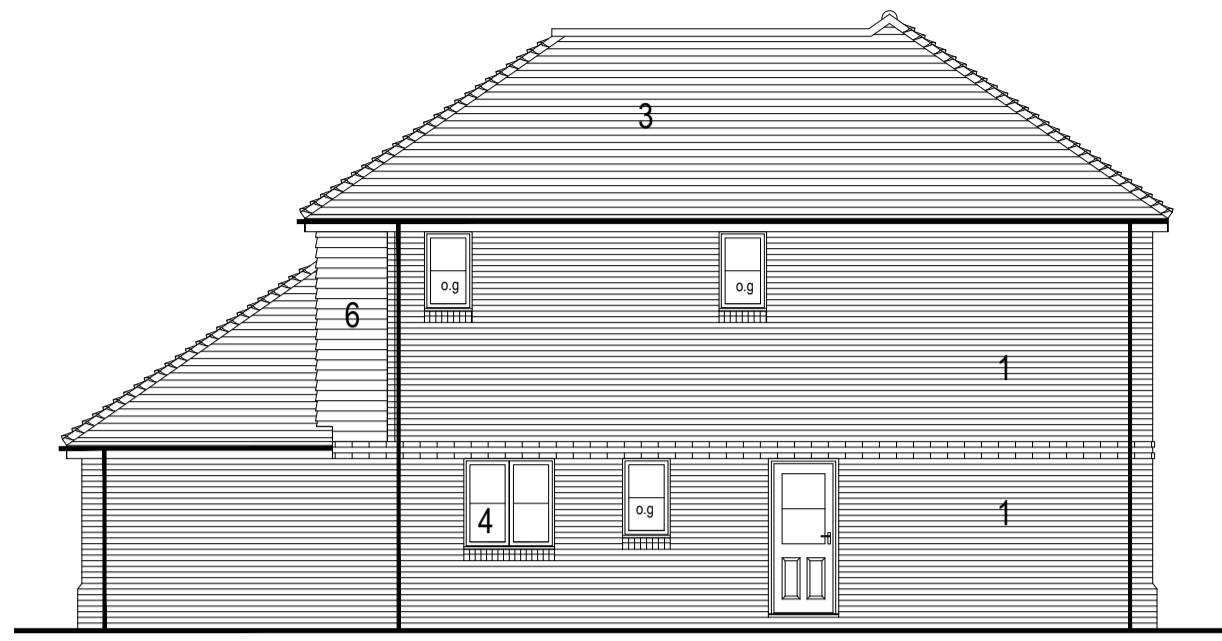
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A	Revised Scheme, Planning Issue	2021.12.20	AJC	dp

scale: 1:50 / 100 @ A1
 client: Devine Homes Plc
 project: 62 to 64 Shelvers Way, Tadworth, KT20 5QF.
 date: December 2021
 drawing: Plot 3 Floor Plans and Elevations
 drawn: AJC
 checked: dp
 status: PLANNING

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SOUTH ELEVATION



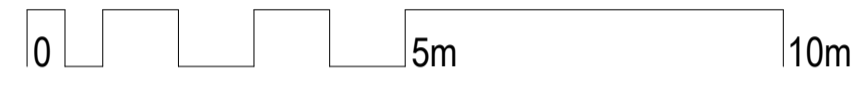
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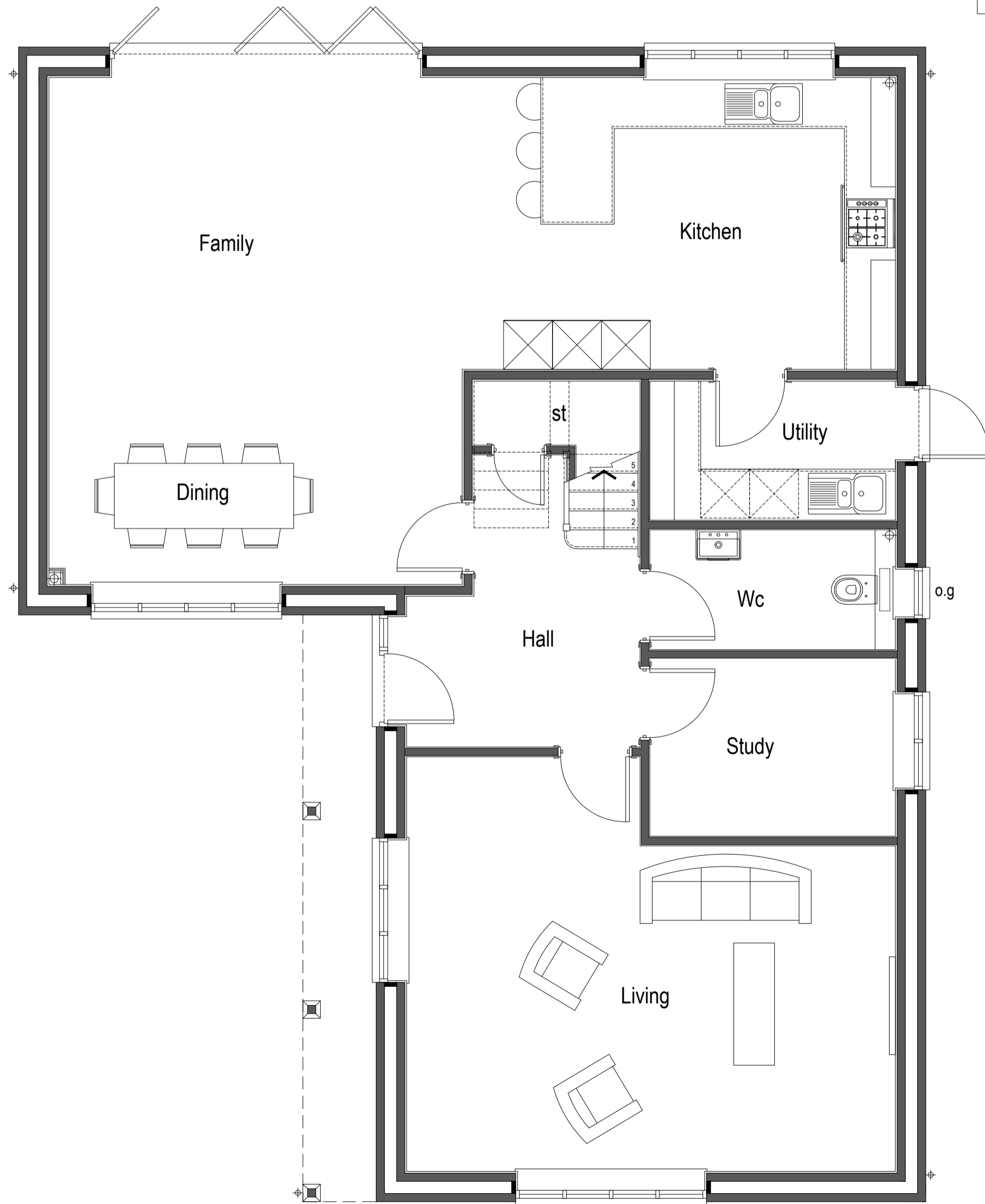
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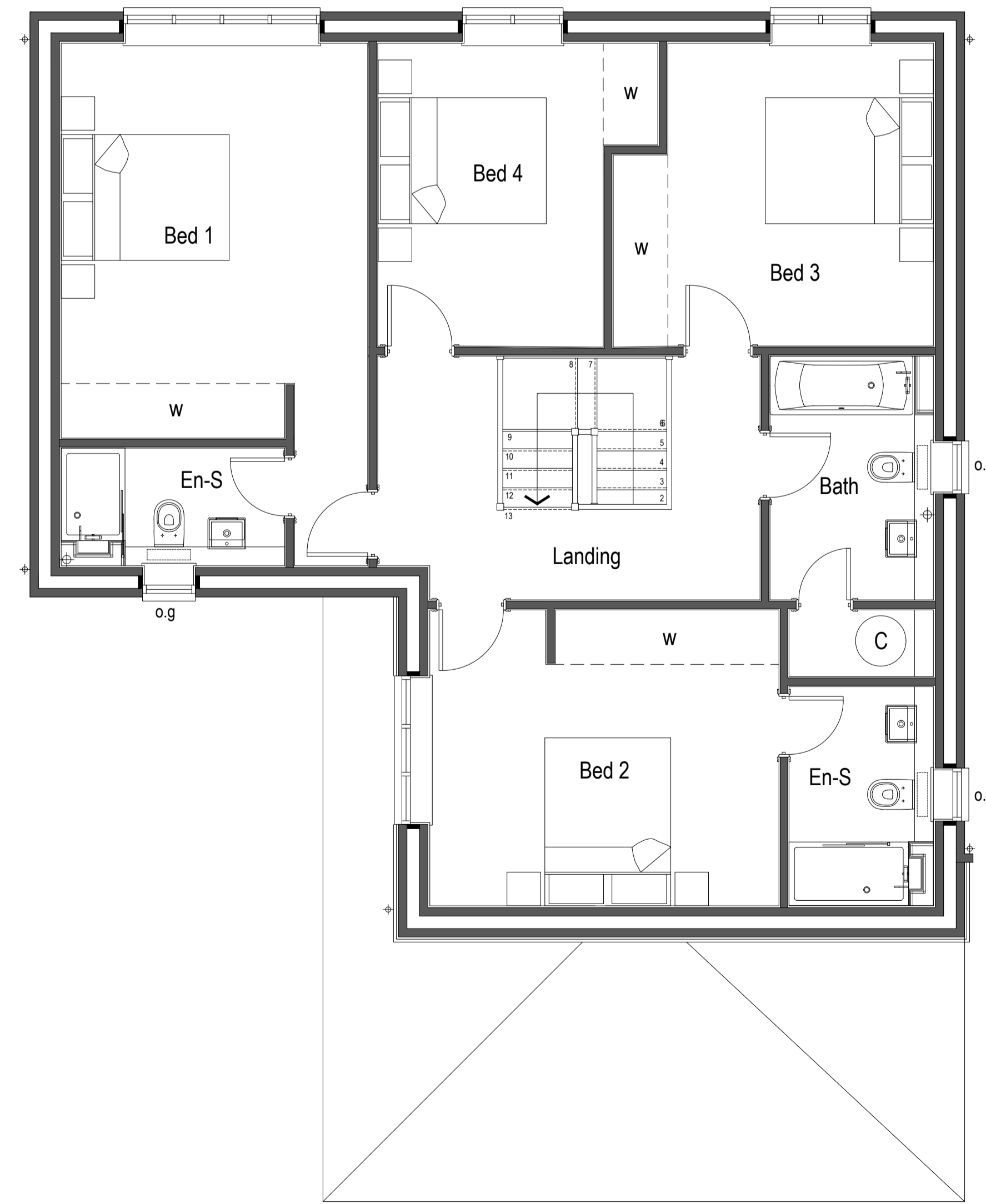
WEST ELEVATION



KEY TO MATERIALS	
1	Facing brickwork
2	Render
3	Plain tiles
4	uPVC windows and doors
5	Single ply membrane
6	Plain tile hanging
7	Timber boarding



GROUND FLOOR PLAN

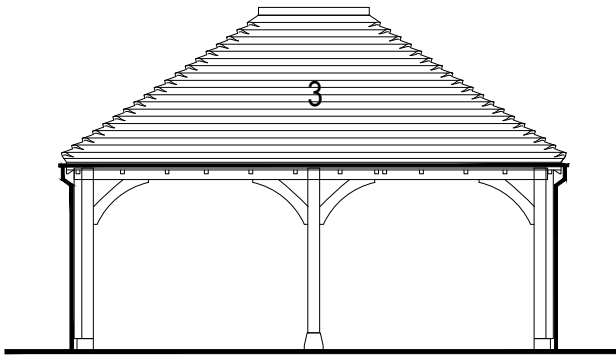


FIRST FLOOR PLAN

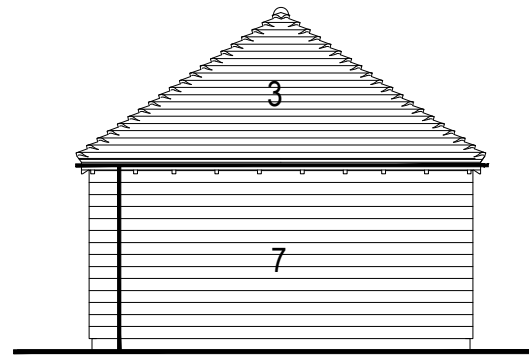


rev.	description	date	by	ch
B	Revised Scheme, Planning Issue	2022.02.01	AJC	dp
A	Revised Scheme, Planning Issue	2021.12.20	AJC	dp

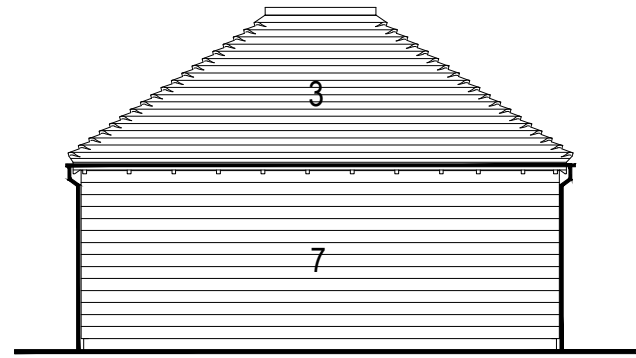
Agenda Item 8



NORTH ELEVATION

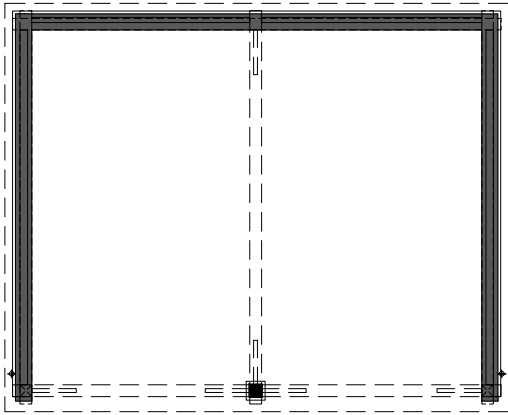


WEST ELEVATION

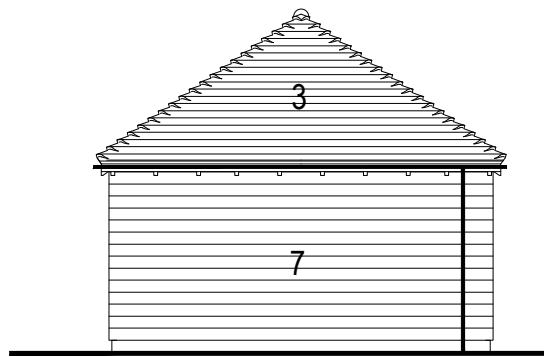


SOUTH ELEVATION

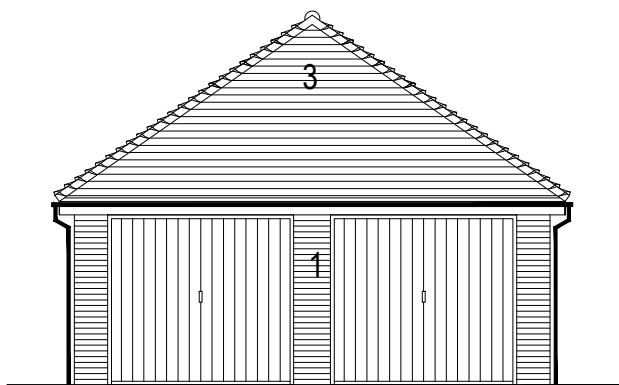
PLOTS 2 & 3 CAR BARN ELEVATIONS



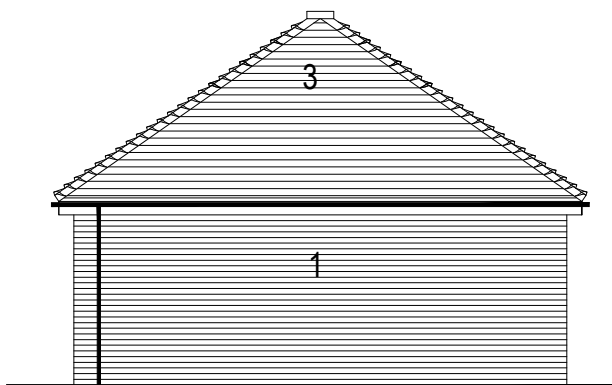
PLOTS 2 & 3 CAR BARN PLAN



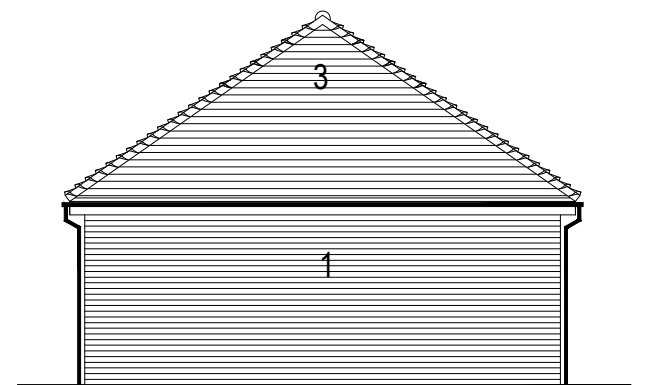
EAST ELEVATION



WEST ELEVATION

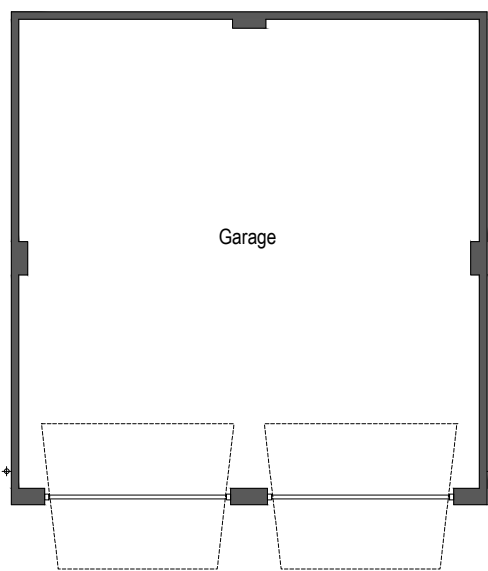


SOUTH ELEVATION

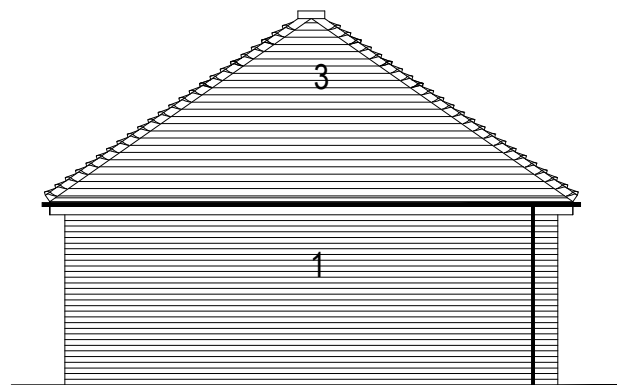


EAST ELEVATION

PLOT 4 GARAGE ELEVATIONS



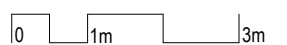
PLOT 4 GARAGE PLAN



NORTH ELEVATION

KEY TO MATERIALS

1	Facing brickwork
3	Plain tiles
7	Timber boarding



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MODEL FILE REFERENCE-21008-A-P-PL			

A	Revised Scheme- Planning Issue
rev	description

2021.12.20	AJC	dp
date	by	ch.

checked: dp

status: PLANNING

scale: 1:100 @ A3

client: DEVINE HOMES PLC

date: December 2021

project: 62 to 64 Shelvers Way, Tadworth, KT20 5QF.

drawn: AJC

drawing: Car Barn and Garage Plans and Elevations

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drawing No: 21008-A-PL-046

revision: A

Agenda Item 9

Planning Committee
6th July 2022

Agenda Item: 9
21/00429/CU

 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	6 th July 2022
	REPORT OF:	HEAD OF PLANNING
	AUTHOR:	Michael Parker
	TELEPHONE:	01737 276339
	EMAIL:	Michael.parker@reigate-banstead.gov.uk
AGENDA ITEM:	9	WARD: <i>Lower Kingswood and Tadworth</i>

APPLICATION NUMBER:	21/00429/CU	VALID:	16/03/2021
APPLICANT:	Land and City Families Trust	AGENT:	WS Planning
LOCATION:	LAND AND CITY FAMILIES TRUST, OLD PHEASANTRY, MERRYWOOD GROVE, LOWER KINGSWOOD, SURREY		
DESCRIPTION:	Change of use of part of the building to a school.		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

This application is referred by Cllr Ashford due to the public interest

SUMMARY

The application seeks permission for the change of use of part of an existing building which has lawful use as a rural activities retreat for disadvantaged children to accommodate a new school use for up to 18 pupils with social, emotional and mental health needs. The use of the main part of this building would remain as a retreat/activities centre for disadvantaged children. The site lies in a rural location within the Green Belt and an Area of Great Landscape Value where the character is one of detached housing set in spacious plots and set within significant levels of mature planting where the natural planted landscape is the dominant character.

The property was granted planning permission in 1985 under reference 85/0601/S.32 for: 'Continued Use as a residential centre for disadvantaged children and adults'. This was granted conditionally, with condition 1 stating "The premises shall be used as a rural centre for a maximum of 20 disadvantaged children and adults and for no other purpose without the prior approval of the Local Planning Authority."

The school use is retrospectively applied for as it has already commenced, bringing children to the site in a mini-bus whilst teachers and other staff drive to the site: any car sharing is on an informal basis only. Parking has been created amongst the trees of the north/east corner of the site adjacent to the public footpath 631. Access is via a private road which runs along the public footpath. The site is not in a sustainable location being too far removed from any public transport to allow such

Agenda Item 9

Planning Committee
6th July 2022

Agenda Item: 9
21/00429/CU

use by the applicants or by nearby residents so the school would rely on the use of the private motor vehicle and minibus for all of its journeys.

The proposals have been amended during the application process to seek to overcome concerns raised by officers with regard to the impact of the proposed parking area and increased activity on the Green Belt. The amendments include a reduction in size of the proposed car parking area from 15 to 9 car parking spaces which has meant that the number of trees to be removed has been reduced from 18 individual trees and 1 group to 11 individual trees and 1 group. These low grade trees which are to be removed are to be replaced by 16 new trees.

In terms of Green Belt impacts the change of use of the building and site can be considered appropriate green belt development provided they preserve its openness and do not conflict with then purposes of including land within it. The assessment of openness requires a number of factors to be taken in to consideration including spatial impact, visual impact, duration of development and degree of activity.

It is considered that the proposed change of use would result in a visual change to the appearance of the site, particularly from a localised point of view due to the addition of the car parking and clearly the proposal would result in an increase in activity at the site. However taking in to account the proposed amendments to the proposed parking area and also reduction in staff car movement, the well screened nature of the site and the historic and lawful use of the wider site it is not considered that the proposed changes would result in such an impact that it would fail to preserve the openness of the Green Belt or conflict with the purposes of the Green Belt.

The site lies within the Area of Great Landscape Value (AGLV) and is rural in character. However taking in to account the amended plans which reduce the amount of formalised car parking and reduce staff car movement and the well screened and contained nature of the proposed school use it is considered that the impact of the proposed use would not cause unacceptable harm to the character of the site, surrounding area and wider AGLV.

The impact on neighbouring amenity is considered to be acceptable due to the distance of the site away from residential properties.

The loss of the trees has been considered by the Tree Officer. The trees to be removed are of low quality and the applicant proposes to replace the trees with 16 trees, which is above the number removed. The Tree Officer has therefore raised no objection subject to conditions to secure finalised tree protection measures and further details of the proposed landscaping.

The level of traffic drawn to the site would be different to that previously experienced, but it is not considered to cause either highway safety concerns nor concerns regarding the free flow of traffic. It would unquestionably be noticeable to residents, but is not considered to be so severe an increase as to justify a reason for refusal, particularly as the County Highway Authority has raised no objection with

regard to highway safety or capacity and the County Rights of Way Officer has also raised no objection to the proposals.

Significant concerns have been raised by third parties regarding a recent increase in parking which takes place on the grass verge adjacent to the main part of the building. There is no doubt that parking does take place along this road. This parking appears to be related to the lawful existing use of the remainder of the building, although it is difficult to identify precisely. The applicant has advised that parking has always taken place along here. This may or may not be the case but the important point to note is that the existing activities and any parking related to the main part of the building are not subject of this application and there is no mechanism in which planning can prevent or control parking along this verge. The issue for this application is whether the proposed change of use impacts on Green Belt, visual amenity, neighbouring amenity and highway safety/parking. As set out above the proposed parking is contained within the site and the level of parking, in combination with the travel plan, is considered acceptable. Therefore the proposed school use does not lead to overspill parking outside the site. This is a management issue of the use of the main use of the building. It is understood that the car park for the school remains open at weekends so there is no reason why the new parking area, once installed, could not be used for parking of visitors to the main part of the building during weekends. But the parking on the verge is not considered to be a matter which could reasonably lead to a reason for refusal on planning grounds when it does not clearly relate to the actual proposed school use.

Officers are satisfied that the scheme does and would continue to provide a school facility that would assist in the provision of sufficient school places to meet the needs of existing and new communities with regard to SEN school provision in accordance with the provisions of the NPPF. Such a benefit should be given weight in favour of approving the application in any required balancing exercise.

In summary, following the reduced parking areas from previously, it is considered that the proposed change of use would constitute an appropriate form of development which does not cause unacceptable harm to the openness of the Green Belt and the character of the area and AGLV. There are no neighbouring amenity concerns or unacceptable impact on trees. There are no highway safety or capacity concerns. The SEN school provision for the borough and wider area also adds weight to the application in terms of the benefit of such a proposal. The application is therefore recommended for approval.

RECOMMENDATION(S)

Planning permission is GRANTED subject to conditions.

Subject to the completion of all documentation required to create a planning obligation under Section 106 of the Town and Country Planning Act 1990 (as amended to secure:

Agenda Item 9

Planning Committee
6th July 2022

Agenda Item: 9
21/00429/CU

- (i) To secure the minibus meeting point at OYO Bridge Hotel, Reigate Hill in accordance with the School Travel Plan
- (ii) The Council's legal costs in preparing the agreement

Planning permission is **GRANTED** subject to conditions.

In the event that a satisfactorily completed obligation is not received by 8 November 2022 or such longer period as may be agreed, the Head of Places and Planning be authorised to refuse permission for the following reason

1. Without a completed planning obligation the proposal fails to secure the minibus meeting point at OYO Bridge Hotel and therefore fails to comply with the Travel Plan and is therefore contrary to the National Planning Policy Framework 2021 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility) and Reigate and Banstead Development Management Plan 2019 Policy TAP1.

Consultations:

County Highway Authority: recommend refusal on sustainability grounds but raise no highway safety concerns. They go on to advise that if local authority minded to grant consent that a number of conditions are recommended as well as a legal agreement to secure the minibus drop off/pick up proposals. Their comments were as follows:

“A significant reduction in car use is unlikely to be achieved in this unsustainable location, even with the implementation of a travel plan.

The proposed school would be located in an area which is well in excess of the preferred maximum commuting walking distance of 2.0 km from bus stops according to The Institution of Highways And Transportation Document "Providing for Journeys on Foot". It is noted that there are also no footways and street lighting in the area so the walk to bus stops would be along routes that would be unsuitable to consider using any local bus services.

The nearest train station to the site is 6.3 km away along routes that are unlit and not overlooked therefore unattractive to use as an every day form of travel by most if not all people.

According to the Reigate and Banstead Accessibly Map the site is in an area with low accessibility. This is apparent when looking at the site which is in a rural location with few if no opportunities to travel by none car modes of transport to reach the site.

It is noted that there is an existing rural retreat centre for disadvantaged children next to the proposed development. This is an existing use. The proposed use would be entirely new and would intensify the use of the site which would require the car in most if not all trips to the site by staff and pupils.

It is noted that sustainable development encompasses not just the environment which underpins the Highway Authority objection, but is also includes economic and social aspects of sustainable development.

If the Local Planning Authority were minded to approve the application, there would be no highway safety concerns.

Access to the Old Pheasantry is along roads that are privately owned and maintained, and provide vehicular access to a number of properties. They also carry rights of way. We have no record of any complaints by the general public about pedestrian conflict with traffic accessing the application site. Any potential conflict would be mitigated by incorporating passing places but consent would be required from the relevant landowners where the land is not owned by the applicant. I note that the applicant has provided a passing place on footpath. 631 within land they control. This should be retained and maintained by the applicant.

Increased traffic will increase wear on the existing road surface and any resulting damage (eg.potholes) may pose a hazard to public users. Maintenance of the

Agenda Item 9

Planning Committee
6th July 2022

Agenda Item: 9
21/00429/CU

surface to enable vehicle use is the responsibility of the landowner and/or those exercising a private vehicular rights and they must ensure it is safe for the public. Any changes to the surface of rights of way must be in consultation with Countryside Access Team to ensure it is suitable for public traffic (eg. New tarmac can be very slippery for horses). Applicants are reminded that the granting of planning permission does not authorise obstructing or interfering in any way with a public right of way. This can only be done with the prior permission of the Highway Authority (Surrey County Council, Countryside Access Group).

As Highway Authority, Surrey County Council is only required to maintain a surface to the standard required for the intended users of different types of right of way. Where a higher private right is exercised (such as cars as is the case here) it is the responsibility of the landowner and residents/frontagers to provide and maintain a surface suitable for their vehicles. This is usually delivered via a Roads Association or similar.

There are local concerns about the use of the verge for parking of cars on footpath 631 next to the eastern boundary of the site. The verge is not part of the footpath. If this was part of the footpath as shown in the definitive map, there would be adequate space to pass the parked cars, therefore it would be unlikely that enforcement action would be taken.

The daily number of trips to/from the school site itself is low – 1 x minibus, 1 x cyclist, 1 x taxi, and up to 14 staff cars - although car sharing takes place on an informal basis and staff can chose to use the mini bus service, so this number maybe less on some days - and a small number of trips per month generated by part time staff. Paragraph 3.8 of the proposed travel plan states that there are 15 full time members of staff, who arrive between 7.30am-8.15am and who leave between 4.00-6.30pm. 1 part-time member of staff works in the afternoons only, and another (cleaner) works on Thursday and Sunday evenings only; the remaining 3 work as therapists, but only work at the school for half a day per month for one part time members of staff and 1.5 hours per week for the remaining two members of part time staff. There are nine parking spaces proposed given that 8 staff members are likely to drive to the site, while the remaining 7 staff are likely to use the proposed mini bus service. However it is not clear what would happen to the mini bus service if the drop off pick up service at OYO Bridge House Hotel, Reigate Hill, Reigate, RH2 9RP were to cease.

A travel statement will mean that the school commit to building on their existing proposals, eg to provide cycle storage, encourage staff to car share and to provide road safety training for pupils (as appropriate to their special needs) without committing to targets which would be rather meaningless given the location of the site.”

If the planning authority is minded to approve the application conditions are recommended to address the following matters:

- Submission of a travel statement
- Provision/retention of a passing place on footpath 631
- Plus legal agreement to secure drop off and pick up service

Surrey CC Countryside Access - Access to the Old Pheasantry is along roads that are privately owned and maintained and provide vehicular access to a number of properties. They also carry the above rights of way. Would be concerned that any increase in traffic may result in conflict with public users, (pedestrians, equestrians and cyclists), although they have no record of any such complaints about conflict with traffic accessing the application site. Any potential conflict might be mitigated by incorporating passing places, but consent would be required from the relevant landowners where the land is not owned by the applicant. It may be that an alternative access into the western boundary of the site would reduce this problem. I would recommend that the applicants undertake to mow the verges either side of the tarmac and as long as there is plenty of room for pedestrians to step off the path I would be willing to accept this in addition to the passing place.

To be clear it is Public Footpath 631 where I think there might be conflict between public users and vehicles.

Increased traffic will increase wear on the existing road surface and any resulting damage (e.g. potholes) may pose a hazard to public users. Maintenance of the surface to enable vehicle use is the responsibility of the landowner and/or those exercising a private vehicular right and they must ensure it is safe for the public. Any changes to the surface of rights of way must be in consultation with Countryside Access to ensure it is suitable for the public traffic (e.g. new tarmac can be very slippery for horses). Applicants are reminded that the granting of planning permission does not authorise obstructing or interfering in any way with a public right of way. This can only be done with the prior permission of the Highway Authority (Surrey County Council, Countryside Access Group).

NATS Safeguarding:
No objections

Representations:

Letters were sent to neighbouring properties on 19th March 2021 and a site notice was posted 25th March 2021. Neighbours were re-notified on the revised plans for a 14 day period commencing 25th May 2021 and again in November 2021 and 16 May 2022 following the submission of the amended Travel Plan, Tree Information and additional statement.

151 responses have been received to date (121 objections, 28 letters of support and 2 letters of representation) across the revised application including many from the same residents raising the following issues and comments of support:

Issue	Response
Inadequate parking	See paragraph 6.31-6.35
No need for the development	See paragraph 6.39-6.45
Noise & disturbance	See paragraph 6.28-6.30

Agenda Item 9

Planning Committee
6th July 2022

Agenda Item: 9
21/00429/CU

Inconvenience during construction	See paragraph 6.28-6.30
Out of character with surrounding area	See paragraph 6.22-6.27
Increase in traffic and congestion	See paragraph 6.31-6.35
Overdevelopment	See paragraph 6.22-6.27
Hazard to highway safety	See paragraph 6.31-6.35
Harm to Conservation Area	The site is not located within a conservation area.
Harm to listed building	The building is not listed.
Overlooking and loss of privacy	See paragraph 6.28-6.30
Crime fears	See paragraph 6.28-6.30
Loss of/harm to trees/wildlife habitat	See paragraph 6.36-6.38
Harm to Green Belt/countryside	See paragraph 6.3-6.21
Property devaluation	This is not a material planning consideration
Alternative location or proposal preferred	Each application must be assessed on its own merits.

These comments include 28 responses expressing support for the proposed change of use.

Support - Community/regeneration benefit	See paragraph 6.39-6.45
Support - Economic growth / jobs	See paragraph 6.39-6.45
Support - Visual amenity benefits	See paragraph 6.22-6.27

1.0 Site and Character Appraisal

1.1 The application site is situated on the south side of Merrywood Grove in Mogador, Tadworth. The site comprises part of a two-storey building with rooms in the roof, with grounds located to the north. The site is currently owned by Land and City Families Trust and is occupied by Merrywood House School who since 1988, have used The Old Pheasantry, including the main building and the grounds to provide accommodation for groups of disadvantaged children. The larger part of the main building, which is not the subject of this application, continues to be used as accommodation for short periods of time (weekends or weekday uses) by groups bringing disadvantaged children for educational or recreational breaks. The applicants state that the building sleeps up to 20 children and has a sitting room, dining room, garden room, kitchen and utility room, together with dormitories, bathrooms and shower rooms. Off site parking for this part of the building will

continue to be along the grass verge adjacent to the footpath no 631 which runs along the front of the property.

- 1.2 The part of the building that is the subject of this application was previously occupied by the Site Manager and is located principally at first and second storey (roof space) levels with access to the ground floor. The applicants state that this use ceased in 2019 when the Manager left, and the Trustees decided to appoint an off-site Manager.
- 1.3 Merrywood House is located in spacious grounds which include open areas laid to grass as well as areas of woodland.
- 1.4 The site is located in open countryside just to the south of the M25 motorway. The site is accessed from Merrywood Grove, a private road which is designated as bridleway in part and as a public footpath. Merrywood Grove is partially surfaced, and its upkeep is undertaken by the residents who use it for access to their properties.

2.0 Added Value

- 2.1 The applicants did not engage in pre-application discussions with the Council
- 2.2 Improvements secured during the course of the application: updated Travel Plan Statement, amended parking plan showing reduction in number of spaces.
- 2.3 Further improvements via condition: conditions to limit the maximum number of pupils. Conditions to secure implementation of the parking layout and tree protection. Condition to secure updated Travel Plan Statement and retention of passing pace on footpath 631.

3.0 Relevant Planning and Enforcement History

- | | | | |
|-----|--------------|---|--|
| 3.1 | 94/08100/F | Erection of a stable block | Approved 20
September 1994 |
| 3.2 | 85/06010/F | Continued use as rural centre for disadvantaged children and adults | Granted 29 July
1985 |
| 3.3 | 85P/0601/S32 | Continued use as rural centre for disadvantaged children and adults | Approved with
conditions 29 July
1985 |
| 3.4 | 77P/0106 | Continued use as a rural centre for deprived and handicapped children | Approved with
conditions 21 April
1977 |
| 3.5 | 75P/1111 | Renewal of temporary permission for use of The Old Pheasantry as a | Approved with
conditions 14 |

Agenda Item 9

Planning Committee
6th July 2022

Agenda Item: 9
21/00429/CU

rural centre for handicapped
children for a further 2 years

January 1976

- 3.6 74P/0468 Temporary use as a rural centre for up to 20 educationally handicapped children Approved with conditions 29 November 1974

4.0 Proposal and Design Approach

- 4.1 This is a full application for the change of use of part of the building at Merrywood House to a school. The application relates to the part of the school which was formerly occupied by the Site Manager and is located principally at first and second storey (roof space) levels with access to the ground floor.
- 4.2 The Trustees of the site have leased this part of the building to Merrywood House School, to be used for a special school for primary children with complex educational needs. It is noted that Merrywood House School is an independent special school for pupils aged 6 to 11 with complex educational and social needs. Merrywood House School offers places to children throughout the local area and surrounding boroughs (see supporting letter from Merton Council). In addition to the part of the building leased to Merrywood House School, an area of the grounds is also leased to the School, providing an area for parking and outdoor space for the children (parking to the north/east of the house).
- 4.3 The applicants note that Ofsted gave their approval to use the premises as a school in May 2020. The opening of the school was delayed by the COVID-19 restrictions, but it operated for 6 weeks before the summer holidays, housing a total of 6 children. The school reopened in September 2020 where the number of children increased to 16 children in total. The School's maximum capacity would be for 18 children. The School has 15 full-time staff members and 7 part-time staff members. It should be noted that Merrywood House School is a day school only, and there is no requirement for overnight stays.
- 4.4 In October 2020, the Council's Senior Enforcement Officer contacted the owners of The Old Pheasantry to clarify the use of the site. It was confirmed that part of the building is being used as a fee-paying school, which the Enforcement Officer advised requires planning permission. Therefore, this application has been submitted following requests from the Council's Enforcement Officer.
- 4.5 The School has a minibus which picks up the children from an agreed collection point, with the exception of one pupil who cannot travel with a group and is delivered and collected by taxi.
- 4.6 The application site currently has an informal parking forecourt arrangement, that provides space for the minibus and 15 cars. The applicants state that the parking arrangements are not ideal and have the potential to harm the existing trees on site. Therefore, the layout of the parking forecourt is

proposed to be rearranged with a reduction in the number of spaces provided from 15 to 9 cars. The rearrangements are proposed in order to create a more practical layout and to minimise the impact on the existing trees. The new parking layout would include the removal of several low value tree, as detailed in the submitted Arboricultural Report prepared by David Archer Associates. The reduction of proposed spaces from 15 to 9 under the amended proposals does however mean that less trees are to removed (11 & 1 group compared to 18 and 1 ground). The removal of these low value trees would not be detrimental to the character or appearance of the site, and proposals for replacement tree planting are being considered and could be achieved by means of an appropriate condition. A passing place along footpath 631 would be maintained as part of an agreement with the school if permission were granted

4.7 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:
Assessment;
Involvement;
Evaluation; and
Design.

4.8 Evidence of the applicant’s design approach is set out below:

Assessment	The character of the surrounding area is assessed as being semi-rural and already in a quasi-educational use.
	Site features meriting retention are listed as a number of trees.
Involvement	No community consultation took place.
Evaluation	The statement does not include any evidence of other development options being considered.
Design	The statement does not explain in detail why the proposal site was chosen. In subsequent submissions the applicant states: “The Old Pheasantry was chosen primarily due to its quiet and tranquil location, plus the classroom sizes which provide calming locations to educate the severely impacted special needs children who receive an Ofsted triple A rated education. The style of accommodation provided at the Old Pheasantry is unique in terms of its countryside setting which forms a comprehensive easy learning environment”

4.9 Further details of the development are as follows:

Site area	0.34 ha
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Agenda Item 9

Planning Committee
6th July 2022

Agenda Item: 9
21/00429/CU

Existing use	Residential Centre for children
Proposed use	Mixed residential centre and School
Existing parking spaces	Ad-hoc/Informal
Proposed parking spaces	10 (9 cars and 1 minibus space)

5.0 Policy Context

5.1 Designation

Metropolitan Green Belt
Area of Great Landscape Value

5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)
CS2 (Valued Landscapes and Natural Environment),
CS3 (Green Belt)
CS5 (Valued People/Economic Development),
CS10 (Sustainable Development),
CS12 (Infrastructure Delivery),
CS17 (Travel Options and accessibility)

5.3 Reigate and Banstead Development Management Plan 2019

DES1 (Design of New development)
TAP1 (Access, Parking and Servicing)
NHE1 (Landscape Protection)
NHE2 (Protecting and enhancing biodiversity and areas of geological importance)
NHE3 (Protecting trees, woodland areas and natural habitats)
NHE5 (Development within the Green Belt)
INF1 (Infrastructure)
INF2 (Community Facilities)

5.4 Other Material Considerations

National Planning Policy Framework
National Planning Practice Guidance
Supplementary Planning Guidance Local Distinctiveness Design Guide
Vehicle and Cycle Parking
Guidance 2018
Other Human Rights Act 1998

6.0 Assessment

- 6.1 The site is located within the Metropolitan Green Belt. National and Local Policy requires that in order to preserve the openness of the Green Belt, planning permission should not be granted for development that is inappropriate unless justified by very special circumstances.
- 6.2 The main issues to consider are:
- The principle of development in the Green Belt
 - Design appraisal
 - Neighbour amenity
 - Highway matters
 - Trees and Landscaping

The principle of development in the Green Belt

- 6.3 The site is located in the Green Belt and Core Strategy Policy CS3 and DMP Policy NHE5, in line with the NPPF (2021), state the construction of new buildings will be regarded as inappropriate in the Green Belt unless they fall within one of the listed exceptions.
- 6.4 Para. 147 of the NPPF states that inappropriate development is, by definition, harmful to the Green Belt and should not be approved except in very special circumstances. Para.149 sets out a number of exceptions to this, whilst para 150 states that certain other forms of development are also not inappropriate in the Green Belt provided they preserve its openness and do not conflict with then purposes of including land within it, including at part (d) the re-use of buildings provided that the buildings are of permanent and substantial construction and (e) material changes in the use of land (such as changes of use for outdoor sport or recreation, or for cemeteries and burial grounds).
- 6.5 The existing building at Merrywood House is one that is of permanent and substantial construction. With regards to the impact on openness, the National Planning Practice Guidance published advice on the assessment of openness in the Green Belt in July 2019. It states that *“assessing the impact of a proposal on the openness of the Green Belt, where it is relevant to do so, requires a judgment based on the circumstances of the case. By way of example, the courts have identified a number of matters which may need to be taken into account in making this assessment. These include, but are not limited to:*
- *openness is capable of having both spatial and visual aspects – in other words, the visual impact of the proposal may be relevant, as could its volume;*
 - *the duration of the development, and its remediability – taking into account any provisions to return land to its original state or to an equivalent (or improved) state of openness; and*
 - *the degree of activity likely to be generated, such as traffic generation.”*

Agenda Item 9

Planning Committee
6th July 2022

Agenda Item: 9
21/00429/CU

6.6 Each of these issues is discussed in turn below.

Spatial Impact

6.7 The proposed development would not result in an increase in built form on the site, as the proposed school would be accommodated within the existing building. No changes are proposed to the external elevations of the building. Whilst some ancillary outdoor structure have been installed in the garden area these do not appear to be structures which constitute development and therefore could be installed irrelevant of the change of use of the site. In any case these elements do not form part of this change of use application. In spatial terms, therefore, the proposed development would have no greater impact on the openness of the Green Belt than existing development.

Visual Impact

6.8 The proposal involves the change of use of part of the existing building to a school. The proposal does not include any external alterations or extensions and hence the change of use would not impact the character and appearance of the site or the surrounding area in this regard.

6.9 Merrywood House School currently use an area of the grounds located to the north of the building as outdoor space for the children. The use of this area of the site is not dissimilar to the previous use as part of the rural centre for disadvantaged children. Furthermore, the site is relatively well screened from the road and from surrounding properties by mature planting. Therefore, the proposal would have little impact upon the character and appearance of the site or the surrounding area in this respect.

6.10 The school currently use an area in front of the building for informal parking and it is proposed that this would be rearranged in order to create a more practical layout and to minimise the impact on the existing trees. This parking has only arisen since the school started occupation of the site in 2019. Prior to the start of the school at this time, the only parking that took place in this area was by a single vehicle by the Site manager.

6.11 The use of part of the woodland area for parking undoubtedly has an impact on the character and visual amenities of part of the site which prior to the occupation by the school was open and free from parking. The area used for parking has therefore changed in character and appearance of this part of the site, particularly in short views through the trees. However it is proposed to formalise the parking area in front of the school with a proper surface and by concentrating of vehicles in a smaller area, including in tandem formation.

6.12 Whilst this would lead to a reduction in the open soft landscaped rural quality of the environment in front of the building such impact would be localised. The submission of amended plans reducing the amount of proposed spaces by 5 has further concentrated the proposed parking area and reduced the impact on the existing trees and wooded area. The surface is stated to be a "above existing soil, no dig, low invasive, permeable free draining

construction” which on the submitted Arboricultural Method Statement is shown to be a Geo-grid ground stabilisation system. Such systems are less urbanising than tarmac or other hardsurfacing as they are normally filled with a gravel or similar material or allow grass to grow through. The car parking area is gated and this is closed during school hours and the site is well screened from the wider area by fencing and existing trees. Whilst trees are proposed to be lost due to the car park works additional trees are proposed to replace the lost poor quality trees and which will over time help to further screen any impact from the car park area. As such whilst the proposal would result in some localised impact to the woodland area for the reasons set out above it is considered that on balance the impact is not such that it is considered to have an unacceptable visual impact on the site or result in such a reduction in the openness of the green belt in this location that it fails to preserve its openness.

- 6.13 We are advised that the parking associated with the previous/existing use of the main part of the building resulted/results in parking along the grass verge lying adjacent to the public footpath no. 631 in front of the building. This grass verge will continue to be used for parking associated with the use of the main part of the building. Photographs have been submitted to show some parking along this verge previously and the applicant has confirmed that this verge is used for parking and has historically been used as such. Such use does not block the actual footpath and whilst this would have some impact on the appearance of the verge such an impact would be temporary due to the transitory use of the parking. It is also of note that as this parking relates to the main use of the building and not the proposed school use it is not considered that this parking, which appears to be independent from the school activities should be counted against the school use in terms of impact on openness.

Duration of Development

- 6.14 The PPG refers to the duration of the development, and its remediability – taking into account any provisions to return land to its original state or to an equivalent (or improved) state of openness. In this case, the proposed development would comprise the change of use of an existing building and therefore on this point, the proposed development would have a neutral impact on the Green Belt.

Degree of activity likely to be generated

- 6.15 The existing site is in use as a residential centre for underprivileged children. Information submitted with the application states that the school currently accommodates 16 pupils. Of these 15 arrive in a mini-bus whilst the other pupil is brought to school by taxi. The school state that there are generally 15 members of staff on site at any one time. The number of part-time staff has just recently reduced to 5 staff members, and these people are rarely on site together and at times work virtually. The applicants also note that the part time staff comprise the School’s therapy team and one part-time daily staff member. One therapist only comes on site once a month for half of a day.

Agenda Item 9

Planning Committee
6th July 2022

Agenda Item: 9
21/00429/CU

The other two therapists work on different days and work 1.5 days each. The other staff member works in the PM for two hours a day. The final person is the School's cleaner and works Sunday and Thursday evening. The applicants also state that 6 members of staff currently car-share and 1 member of staff does not have a vehicle and travels by bike. In addition the updated Travel Plan states that 7 members of the full time staff have committed to travel in the school minibus using the meeting point at OYO Hotel, Reigate Hill. Which has further reduced the number of daily car movements.

- 6.16 The use of part of the building at Merrywood House has undoubtedly resulted in an increase in the amount of activity at the site, evidenced by the number of cars that arrive and leave along Merrywood Grove and which are parked at the site. This increase in activity has generated a large volume of objection from local residents who have raised concerns about the impact of the additional traffic on Merrywood Grove, which is a private road, and which is designated a bridleway over part of its length and a footpath over part, although the concerns mainly relate to highway safety issues. The increase in activity generally happens on weekdays during the school term although evidence from local residents would appear to show vehicles attending the site on weekends, bank holidays and during the school holidays.
- 6.17 It is noted that the applicant has stated that the use of the main part of the building for residential trips in line with the lawful use of the building has re-started so it is not clear whether the increase in activity is associated solely with the school or with the residential centre or a mixture of the two. It is likely in reality to be a combination of the two, particularly any activities at weekends and bank holidays. This needs to be taken in to consideration when assessing the impact of the activity generated by this application.
- 6.18 Therefore the question is whether the increase in activity, when considering the overall activity and use at the whole site is such that it results in an unacceptable increase in activity which fails to preserve the openness of the Green Belt. Taking in to consideration of the above factors and the relatively small size of the school, maximum 18 pupils and approximately 15 staff at any one time, and the fact that the CHA advises that "The daily number of trips to/from the school site itself is low" it is not considered that the degree of activity generated by the use is such that it could be considered significant or excessive and would in the view of officers have an acceptable impact on openness.
- 6.19 It is noted that concerns have been raised about whether pupils from another school use the site and also about the travel plan and how this is implemented. The applicant has advised that pupils from another school site do not visit the site and also that they have an agreement with the OYO Hotel to ensure the operation of the minibus service. Were the application to be approved it is recommended to use condition to limit the amount of pupils on site at anyone time to 18 (the maximum capacity of the school) and also to secure the minibus pick up point through a S106. This will ensure that the

activity of the site will not increase beyond the approved level without further consent.

- 6.20 The use of the site for parking for the school, due to its contained nature (with the now reduced level of parking) would also not result in encroachment in to the countryside. As such it would not conflict with the purposes for including land within the Green Belt.
- 6.21 In light of these comments, it is considered that the proposals would preserve the openness of the Green belt and would not conflict with the purposes of including land within it. As a result the proposal is considered to represent appropriate Green Belt development and 'very special circumstances' are not required.

Design appraisal

- 6.22 DMP Policy DES1 relates to the Design of New Development and requires new development to be of a high quality design that makes a positive contribution to the character and appearance of its surroundings. New development should promote and reinforce local distinctiveness and should respect the character of the surrounding area. The policy states that new development will be expected to use high quality materials, landscaping and building detailing and have due regard to the layout, density, plot sizes, building siting, scale, massing, height, and roofscapes of the surrounding area, the relationship to neighbouring buildings, and important views into and out of the site.
- 6.23 The proposal involves the change of use of part of the existing building to a school. The proposed plans do not include any external alterations or extensions and hence the change of use would not impact the character and appearance of the site or the surrounding area in this regard.
- 6.24 However, as discussed above, the location of the school to Merrywood House inevitably means that the majority of trips to the school by both pupils and staff are by private car. To date the school have used areas beneath trees in front of the school building for parking but are proposing to formalise the parking arrangements as part of this application.
- 6.25 The use of part of the woodland area for parking has undoubtedly altered the character and visual amenities of part of the site which prior to the occupation by the school was open and free from parking. The area used for parking has therefore changed in character, and the appearance of this part of the site, particularly in short views through the trees, is adversely affected. It is proposed to formalise the parking area in front of the school with a proper surface and the concentration of vehicles in a smaller area, including in tandem formation.
- 6.26 Whilst this would lead to a reduction in the open soft landscaped rural quality of the environment in front of the building such impact would be localised. The submission of amended plans reducing the amount of proposed spaces

Agenda Item 9

Planning Committee
6th July 2022

Agenda Item: 9
21/00429/CU

by 5 has further concentrated the proposed parking area and reduced the impact on the existing trees and wooded area. The surface is stated to be a “above existing soil, no dig, low invasive, permeable free draining construction” which on the submitted Arboricultural Method Statement is shown to be a Geo-grid ground stabilisation system. Such systems are less urbanising than tarmac or other hardsurfacing as they are normally filled with a gravel or similar material or allow grass to grow through. The car parking area is gated and this is closed during school hours and the site is well screened from the wider area by fencing and existing trees. Whilst trees are proposed to be lost due to the car park works additional trees are proposed to replace the lost poor quality trees and which will over time help to further screen any impact from the car park area. As such whilst the proposal would result in some localised impact to the woodland area for the reasons set out above it is considered that on balance the impact is not such that it is considered to have an unacceptable impact on the character of the Old Pheasantry Site or wider area and AGLV and would therefore comply with DMP Policy DES1 and NHE1.

- 6.27 Significant concerns have been raised by third parties regarding a recent increase in parking which takes place on the grass verge adjacent to the main part of the building. There is no doubt that parking does take place along this road. This parking appears to be related to the lawful existing use of the remainder of the building but again it is impossible to differentiate. The applicant has advised that parking has always taken place along here. This may or may not be the case but the important point to note is that the existing activities and any parking related to the main part of the building are not subject of this application and there is no mechanism in which planning can prevent or control parking along this verge. The issue for this application is whether the proposed change of use impacts on character and as set out above the parking is contained within the site and therefore the proposed school use does not lead to overspill parking outside the site. This is a management issue of the use of the main building. It is understood that the car park for the school remain open at weekends so there is no reason why the new parking area, once installed, could not be used for parking of visitors to the main part of the building during weekends. But the parking on the verge is not considered to be a matter which could reasonably lead to a reason for refusal on character grounds when it does not relate to the actual proposed school use.

Neighbour amenity

- 6.28 In addition to the comments noted above DMP Policy DES1 also requires new development to provide an appropriate environment for future occupants whilst not adversely impacting upon the amenity of occupants of existing nearby buildings, including by way of overbearing, obtrusiveness, overshadowing, overlooking and loss of privacy.
- 6.29 The nearest residential property to the school is located some 80m to the east at Pilgrims Corner and is separated from Merrywood House by areas of woodland and by the road in front of the school. In this regard, the proposed

change of use would be unlikely to result in overlooking, or a loss of privacy, nor would there be an overbearing impact. The main concerns of residents relate to the additional traffic generated by the school and the increase in activity on Merrywood Grove, although concerns have been expressed about increased crime.

- 6.30 In this regard the comments from the Highways Authority are clear that this is a private road and there have been no complaints regarding potential highways conflicts/accidents. However it is equally clear that the increase in traffic is a matter of concern for local residents. Whilst acknowledging the change in character that the increased use has created for local residents, it is not considered that this is so severe as to justify a refusal of permission on these grounds. Likewise the parking of vehicles on the grass verge and round the site would be unsightly but there is no evidence to suggest that it harms neighbours amenities in a manner such as to justify a refusal of permission on these grounds. It is acknowledged that some inconvenience may arise during the works to layout the car park but these would not be considered such as to justify a refusal. There is no evidence to suggest that this scheme would in any way affect crime in the area.

Highway matters

- 6.31 Policy TAP1 of the Development Management Plan 2019 requires new development to demonstrate that it would not adversely affect highways safety or the free flow of traffic, that it would provide sufficient off-street parking in accordance with published standards and that it would constitute development in a sustainable location.
- 6.32 It is clear that the site is not in a sustainable location and that the scheme increases the amount of vehicle movements to the site and parking required at the site. However there is no evidence to support a refusal based upon the level of traffic drawn to the site, nor the impacts of that traffic on highways safety and the free flow of traffic. A passing place has been installed that has helped to alleviate the difficulties of the use of these rural lanes. Parking within the site is provided although as noted elsewhere in this report that needs to be more formally laid out but is considered to provide sufficient parking for the staff and the school mini bus subject to the Travel Plan solutions being fully implemented – i.e. a minibus operating to pick up students and staff from the pick up point at the OYO Bridge Hotel, Reigate Hill. The pick up point is proposed to be secured by legal agreement to ensure it can be retained on third party land. Surrey County Council has considered the submitted plans and Travel Plan and has raised no objection with regard to highway safety and capacity subject to conditions to secure an updated Travel Statement and retention of the passing place.
- 6.33 The issue of the sustainability of location is one that was considered as part of an appeal relating to a change of use of a house to an independent school for children with autism and special educational needs and disabilities in a property in Coulsdon Lane Chipstead (ref 19/02269/F). In that instance permission was refused because of the unsustainable location of the site,

Agenda Item 9

Planning Committee
6th July 2022

Agenda Item: 9
21/00429/CU

expected trip generation and travel modes of pupils and staff. Pupils would generally use either the school minibus or taxi and staff part school minibus and part private car. The site is similar to this insofar as it lies beyond walking distance from public transport, pupils would be taken to school mainly by minibus and taxi and there are no footpaths to allow pedestrian access to the school. The differences lie in that the school lies on a metalled road rather than unmade lane/public footpath as does this site and that staff were also expected to largely use the school minibus or to be taken to the school by two cars designated for that purpose from specified drop off/collection points twice daily. A copy of the appeal decision is attached.

- 6.34 The Inspector concluded that given the needs of the children that most journeys would need to be bespoke and that public transport would not be a practical option even if it were available. He also concluded that accessibility and modes of transport to be used are but one aspect of sustainable development and that whilst there would be tension with one of the criterion of Core Strategy Policy CS10 that the proposed use would meet many of the economic and social aspects of sustainable development. He referenced the fact that the NPPF (para 95) encourages a sufficient choice of school places (with which the Council do not take issue) and finally that the NPPF recognises that opportunities to maximise sustainable transport solutions will vary between urban and rural areas and this should be taken into account in decision making.
- 6.35 It is officers' view that this summary applies equally well to this application proposal and site and that whilst this site is clearly not in a sustainable location, the nature of the use would prevent full use of public transport even were it close to the site by the children. Further under the amended proposals set out in the Travel Plan now 7 of the 15 full time staff members would now travel to the school by minibus reducing the number of car movements to and from the site. Therefore, overall it is not considered that the lack of sustainability would in this instance be unacceptable. For these reasons there is no objection from the highway Authority and it is to be noted that the private nature of the road and its upkeep would not be a planning matter. Subject to an appropriate passing place there is considered to be no safety risk arising from the proximity to the public footpath for the reasons outlined also.

Trees and Landscaping

- 6.36 Policy NHE3 advises that unprotected but important trees, woodland and hedgerows with ecological or amenity value should be retained as an integral part of the development.
- 6.37 The tree officer has been consulted on the amended proposals in order to assess the proposed development against impact upon existing trees and vegetation. The application is supported by an arboricultural method statement that identifies trees to be removed to facilitate the new parking bays, which are low quality and will not have an adverse impact on the local canopy cover or the character of the local landscape. Under the amended proposals 11 individual trees and 1 group are to be removed compared to the

original proposals which were for the removal of 18 individual trees and 1 group. The additional parking bays will be located in the root protection areas (RPA) and to prevent soil compaction cellular web system is shown to be used. The Tree Officer has provided the following comments on the updated information: "The arboricultural information submitted with the application identifies low quality trees that will be removed to allow the geo grid system to be installed to protect the rooting environment from becoming compacted and therefore ensuring the retained trees will continue to mature and contribute to the local landscape. The report makes reference to the installation of protective fencing at pinch points but is not shown where the fencing will be located on the plan, this information is essential to ensure the retained trees are adequately protected." Therefore, whilst the Tree Officer raises no objection to the proposed tree works conditions are recommended to secure a finalised tree protection plan and scheme of supervision prior to any works to the car park. These would be attached were planning permission to be granted.

- 6.38 Also included is a structural planting plan showing location of 16 new trees and species to compensate for those removed. Whilst the amended plans show what size the species will be there is no detail regarding the measures that will be implemented to guarantee their survival such as maintenance and aftercare, however this information could be secured by condition if planning permission was to be granted.

Other matters

- 6.39 The application site is located within the Metropolitan Green Belt. It is considered that the proposed use of part of Merrywood House as a school would not on balance have a harmful impact on the openness of the green belt as a result of the increase in activity and limited effect of the parking generated by the school. Previously, with a larger area of parking it was considered that the proposal did tip towards impacting openness and as such was regarded as inappropriate, however the reduction in the area of parking proposed has, on balance, addressed this.
- 6.40 Accordingly, the proposal is considered to comprise appropriate development within the green Belt. As such no Very Special Circumstances are not required. It is however worth emphasising the benefits of such a proposal in terms of provision of specialist schooling.
- 6.41 It should be noted that the UK has a serious deficit of Special Education Needs (SEN) Schools, where appropriate facilities located in secure and safe environments are difficult to come across. The demand for SEN Schools is growing exponentially, and the retention of Merrywood House School in this location would provide an important contribution to meeting these needs within the Reigate & Banstead Borough, and within the wider Surrey area and southern London Boroughs. The applicant has stated that "Due to the countryside surrounding The Old Pheasantry, the application site is situated within an ideal location, providing high risk children with much needed outdoor amenity space. The positioning of the School within this location is

Agenda Item 9

Planning Committee
6th July 2022

Agenda Item: 9
21/00429/CU

essential as it provides a unique learning opportunity for the children, 95% of which are funded by and live within Surrey County Council. To consider relocating the School to another location would remove the facility from the catchment area of these vulnerable children. Merrywood House School offer an essential, therapeutic, home like environment for children with special needs, the benefits of which would undoubtedly outweigh any harm perceived by the Council.”

- 6.42 The applicant also goes on to state that “The Old Pheasantry was chosen primarily due to its quiet and tranquil location, plus the classroom sizes which provide calming locations to educate the severely impacted special needs children who receive an Ofsted triple A rated education. The style of accommodation provided at the Old Pheasantry is unique in terms of its countryside setting which forms a comprehensive easy learning environment...The School itself is limited in size and will not grow under any circumstances as all of the requisite needs of the School, Community, County and national need are satisfied. Recent Ofsted inspections have given the School and its surrounding extremely high ratings, for the holistic education offered by the site.

To consider relocating the School to another location would remove the facility from the catchment area of these vulnerable children, and hence the need would not be met. It is also increasingly difficult to come across alternative sites which are able to provide the quiet environment, the sufficient indoor classroom space and the outdoor amenity space needed for the children. Not to mention that other sites, which are not necessarily owned by Trusts such as Land & City Families Trust, are just not financially viable.”

- 6.43 A letter of support has also been received from Merton Council who currently place a vulnerable child at the school with complex specialist needs for which the school is able to cater for as well as those of other children placed by them
- 6.44 Therefore officers are satisfied that the scheme does and would continue to provide a school facility that would assist in the provision of sufficient school places to meet the needs of existing and new communities in accordance with the provisions of the NPPF. The school use would also make use of a previously vacant part of the building and provides economic benefits in terms of job creation. Such a benefits should be given weight in favour of approving the application in any required balancing exercise.
- 6.45 In summary it is considered that the proposed change of use would constitute an appropriate form of development which does not cause unacceptable harm to the openness of the Green Belt and the character of the area. The application is therefore recommended for approval.

CONDITIONS

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan Type	Reference	Version	Date Received
Location Plan	UNNUMBERED		23.02.2021
Combined Plan	Existing and Proposed Plan		17.03.2021
Other Plan	Old Pheasantry Passing Point		28.10.2021
Survey Plan	2103-S20		24.05.2021

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

- Notwithstanding the submitted Patrick Parsons Travel Plan within 3 months of the date of this decision a Travel Statement without targets shall be submitted for the written approval of the Local Planning Authority in accordance with the sustainable development aims and objectives of the National Planning Policy Framework 2021 and Surrey County Council's "Travel Plans Good Practice Guide".

And then the approved Travel Statement shall be implemented for the site in accordance with a timescale to be agreed in writing with the Local Planning Authority and for each and every subsequent occupation of the development, and thereafter the Travel Statement shall be maintained, reviewed and developed to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

- The passing place adjacent to footpath 631 north of the application site shall be retained and maintained in accordance with the approved plan.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 of the Reigate and Banstead Local Plan Development Management Plan September 2019.

- Within 6 months of the date of this decision the parking shall have been laid out within the site in accordance with the Proposed Parking Improvement Plan included at Appendix B (plan ref A20310/01 RevP3) within the submitted School Travel Plan by Patrick Parsons (v5.0 dated March 2022) for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking/turning areas shall be retained and maintained for their designated purposes.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to accord with the NPPF and Reigate and Banstead Development Management Plan policy TAP1.

Agenda Item 9

Planning Committee
6th July 2022

Agenda Item: 9
21/00429/CU

5. No works to the approved car park area shall commence including groundworks preparation and demolition until all related arboricultural matters, including arboricultural supervision, monitoring and tree protection measures are implemented in strict accordance with the approved details contained in the Arboricultural Method Statement and Tree Protection Plan compiled by David Archer Associates , dated April 2022

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction - Recommendations' and policies NHE3 and DES1 of the Reigate and Banstead Development Management Plan 2019

6. No works to the approved car park area including groundworks or preparation processes shall be undertaken until an agreed scheme of supervision for the arboricultural protection measures have been submitted to and approved in writing by the local planning authority. The pre commencement meeting, supervision and monitoring shall be undertaken in accordance with these approved details. The submitted details shall include.
 1. Pre commencement meeting between the retained arboricultural consultant, local planning authority Tree Officer and individuals and personnel responsible for the implementation of the approved development
 2. Timings, frequency of the supervision and monitoring regime and an agreed reporting process to the local planning authority.
 3. The supervision monitoring and reporting process shall be undertaken by a qualified arboriculturist.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and policies Pc4 and Ho9 of the Reigate and Banstead Borough Local Plan.

7. No works to the approved car park area shall commence on site until a scheme for the landscaping of the site including the retention of existing landscape features and finalised details of the proposed formalised car parking surface has been submitted to and approved in writing by the LPA. Landscaping schemes shall include details of hard and soft landscaping, including any tree removal/retention, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation and management programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to occupation or within the first planting season following completion of the development hereby approved.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, shrubs of the same size and species.

Reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3, DES1 and DES3 of the Reigate and Banstead Development Management Plan 2019.

8. The maximum number of pupils attending the School at any one time shall not exceed 18 and the school shall operate between Monday to Friday and during school term time only.

Reason: To protect the character and amenities of the area and openness of the Green Belt in accordance with the provisions of Policy DES1 and NHE5 of the Reigate and Banstead Development Management Plan (2019) and the provisions of the National Planning Policy Framework (2019).

INFORMATIVES

1. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : Climate Change Information.
2. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
 - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
 - (e) There should be no burning on site;
 - (f) Only minimal security lighting should be used outside the hours stated above; and
 - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

Agenda Item 9

Planning Committee
6th July 2022

Agenda Item: 9
21/00429/CU

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - www.ccscheme.org.uk/index.php/site-registration.

3. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above relevant arboricultural tree conditions and landscaping conditions above. All works shall comply with the recommendations and guidelines contained within British Standard 5837. The planting of trees and shrubs shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate substantial sized trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Extra Heavy Standard size with initial planting heights of not less than 4m, with girth measurements at 1m above ground level in excess of 14/16cm

REASON FOR PERMISSION

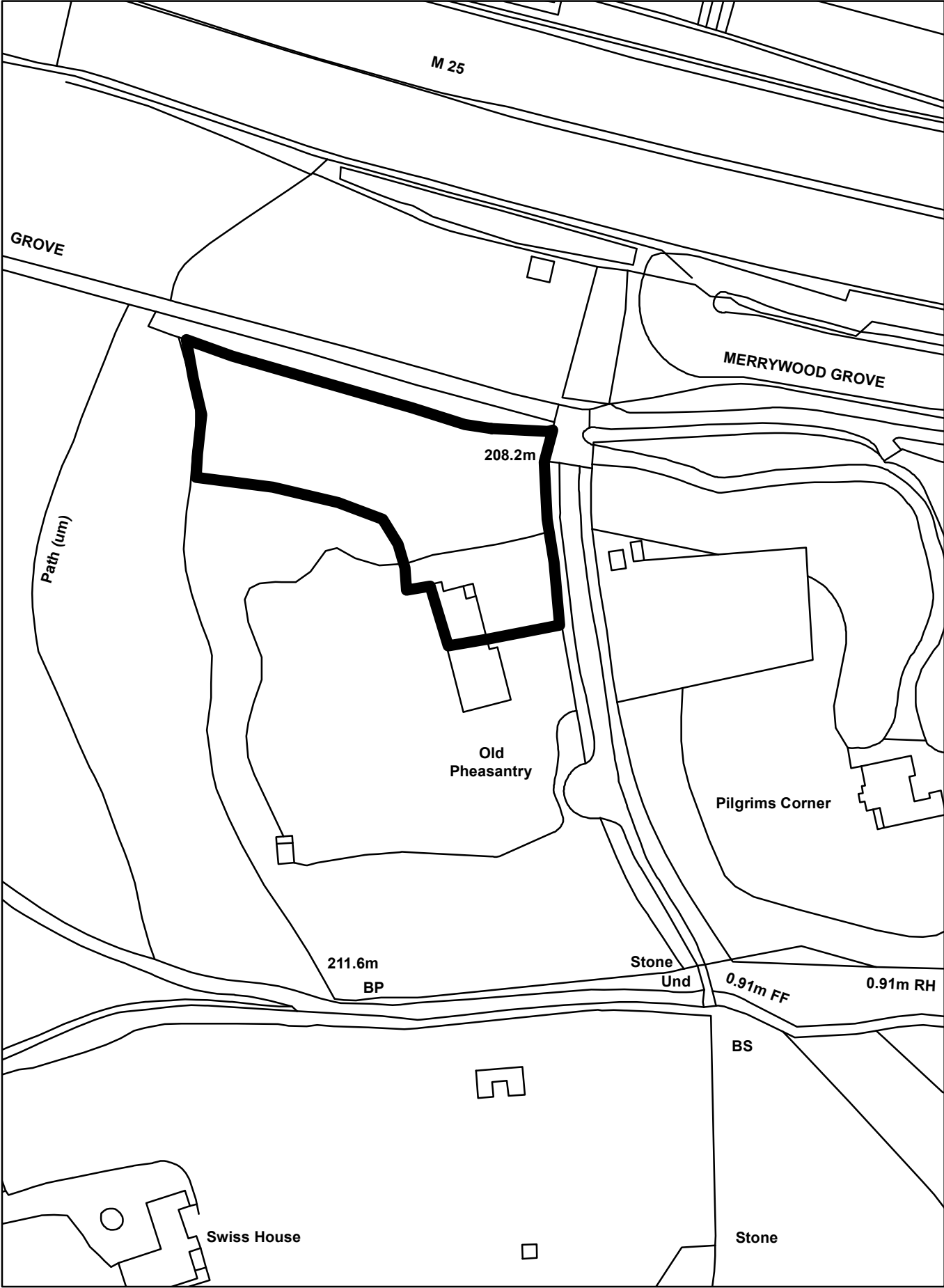
The development hereby permitted has been assessed against the relevant development plan policies as set out in the report and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

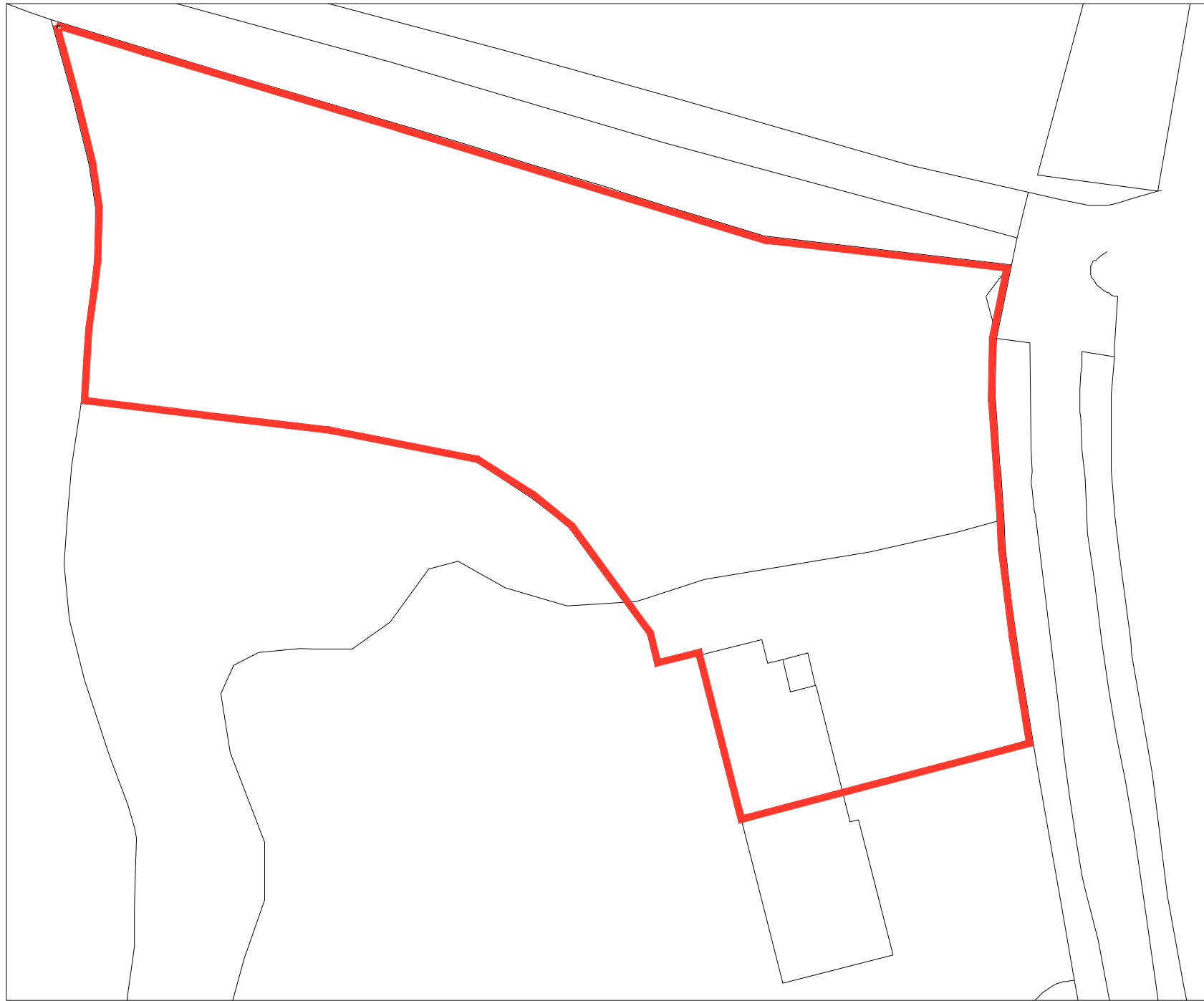
Proactive and Positive Statements

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

21/00429/CU

Old Pheasantry Merrywood Grove Lower Kingswood

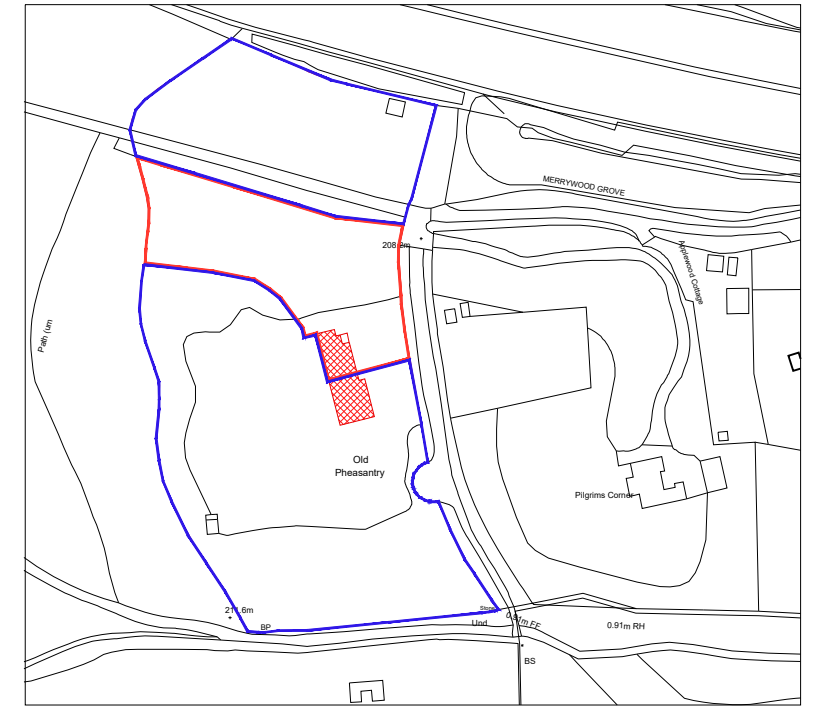




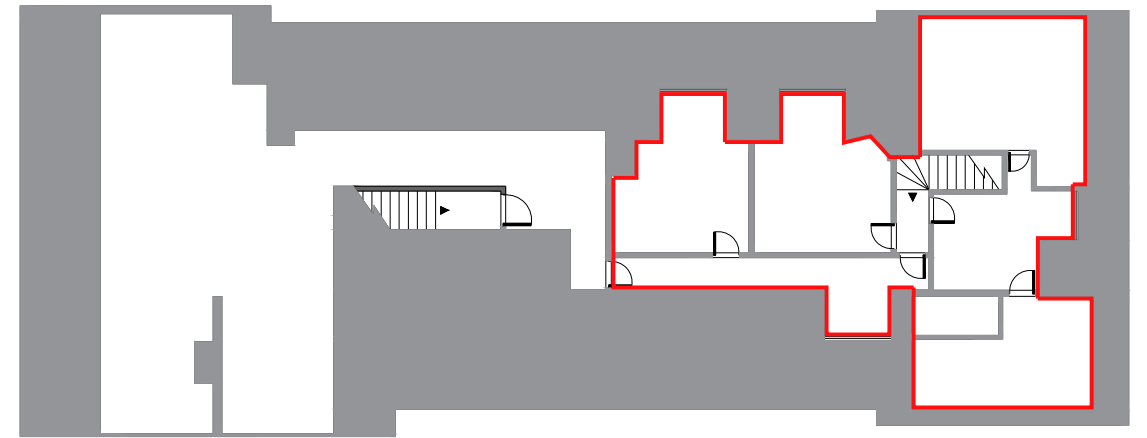
SITE PLAN
SCALE 1:500



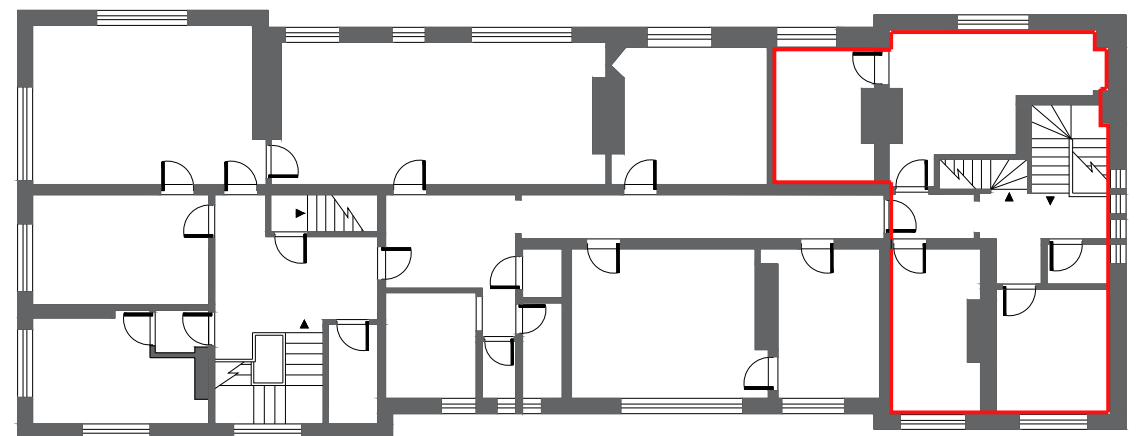
LOCATION PLAN
SCALE 1:2500



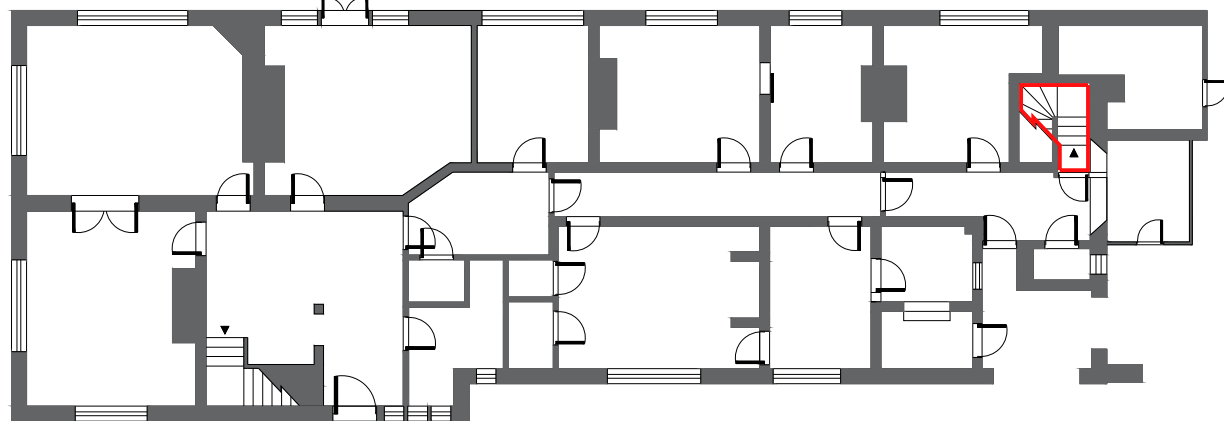
SECOND FLOOR PLAN
SCALE 1:200



FIRST FLOOR PLAN
SCALE 1:200



GROUND FLOOR PLAN
SCALE 1:200





Appeal Decision

Site visit made on 21 January 2021

by Lynne Evans BA MA MRTPI MRICS

an Inspector appointed by the Secretary of State

Decision date: 08 March 2021.

Appeal Ref: APP/L3625/W/20/3258530

Rowans Hill, Coulsdon Lane, Chipstead, CR5 3QG

- The appeal is made under section 78 of the Town and Country Planning Act 1990 against a refusal to grant planning permission.
 - The appeal is made by Mr Gareth McCullough against the decision of Reigate & Banstead Borough Council.
 - The application Ref: 19/02269/F dated 8 November 2019, was refused by notice dated 16 July 2020.
 - The development proposed is change of use to an independent school for children with autism and related special educational needs and disabilities, with the provision of ancillary facilities including a playground, noise barriers and canopy and additional parking.
-

Decision

1. The appeal is allowed and planning permission is granted for change of use to an independent school for children with autism and related special educational needs and disabilities, with the provision of ancillary facilities including a playground, noise barriers and canopy and additional parking at Rowans Hill, Coulsdon Lane, Chipstead, CR5 3QG in accordance with the terms of the application, Ref 19/02269/F dated 8 November 2019, subject to the conditions set out in the schedule at the end of this decision letter.

Preliminary Matters

2. The application was amended a number of times prior to determination and my decision is based on the proposals as determined by the Council. The description of development as set out on the application form provided a considerable amount of detail on the proposed use and development; I have therefore taken the description as set out by the Council on the decision notice and which the Appellant used on the appeal form.
3. As part of the appeal process the Appellant submitted a signed and dated Unilateral Undertaking which provides for payment of a travel plan monitoring fee to the County Council, in the event that planning permission is granted. At the same time a letter was received from the Council advising that as part of the process to complete the unilateral undertaking, a parcel of land was identified as being outside the ownership title of the Appellant and which would be required to enable the improved access to be provided. In the event of permission being granted, pre-commencement conditions have been recommended relating to the provision of the improved access points and

visibility splays. This letter was forward to the Appellant for comment and the Unilateral Undertaking to the Council for comment. No further representations were received.

4. On 1st September 2020 the Town and Country Planning (Use Classes) (Amendment) (England) Regulations 2020 came into force, amending the Town and Country Planning (Use Classes) Order 1987. These Regulations amend and simplify the system of Use Classes and, amongst others, a new class F1 has been created, the Learning and Non-Residential Institutions use class. However, as the application was submitted prior to the new Regulations coming into effect, the Regulations provide that the application should be determined on the basis of the use or use class referenced in the application. That is therefore the basis of my assessment.

Main Issues

5. The main issues in this appeal are:
 - a) Whether the proposed development would be inappropriate development in the Green Belt having regard to the National Planning Policy Framework and any relevant development plan policies, and
 - b) whether the proposal would be a sustainable form of development.

Reasons

Issue a) Whether Inappropriate Development

6. The appeal property is a vacant, large detached residential property in extensive grounds, comprising a lawned area to the rear of the house with planting and woodland to the side and rear boundaries. To the rear and to the side of the main house is a detached garage / outbuilding which appears to have accommodation at the upper level. There are two vehicular access points to the site off Coulsdon Lane with residential properties on both sides of the road. The appeal site is situated within the Green Belt and an Area of Great Landscape Value.
7. The proposed development would change the use of the existing building and site to an independent Special School for Children with Special Educational Needs and Disabilities (SEND). The School would operate as an autism special school with a capacity for 50 boys of secondary school age, with an estimated 15 members of staff. The proposed School would operate in conjunction with an existing school in Croydon, relocating some pupils to the new school and enrolling new pupils.
8. The National Planning Policy Framework (Framework) sets out the government's planning policies to secure sustainable development. Paragraph 133 sets out the great importance that the Government attaches to Green Belts and that the essential characteristics of Green Belts are their openness and their permanence. Paragraph 143 confirms that *inappropriate development is by definition harmful to the Green Belt and should not be approved except in very special circumstances*. Paragraph 146 sets out that a number of forms of development are not inappropriate in the Green Belt providing they preserve its openness and do not conflict with the purposes of including land within it, including at d) the re-use of buildings providing that the buildings are of permanent and substantial construction and e) material changes in the use of

the land (such as changes of use for outdoor sport or recreation, or for cemeteries and burial grounds).

9. I am satisfied that the existing buildings on the site are of permanent and substantial construction and that the principle of the proposed change of use of these buildings and the rear garden area to educational use would not be inappropriate development. There would be some consequential changes to the layout of the grounds to allow for parking and turning as well as to create playground and associated areas, but these would not materially affect the openness and would not conflict with the purposes of including land within the Green Belt. These associated elements would also not be inappropriate development. The Council also drew the same conclusion in respect of the proposed change of use and these layout amendments.
10. Some minor associated operations in the form of acoustic measures would be introduced to protect surrounding residential neighbours. These would include a minimum 2.5 m fence particularly along the western boundary and branching into the site. Given the limited length of the fence line and its siting close to a belt of existing trees I agree with the Council that it would not materially affect the openness of the site; it would not be inappropriate development.
11. The acoustic proposals also include for a canopy cover linking the main house with the outbuilding, but very limited details have been provided as to the nature and materials for this element. The Appellant indicates that the principal purpose of this element would be to serve as an acoustic screen for the neighbouring properties and to serve this purpose would integrate with the boundary wall and rise to a height of 5m.
12. I agree with the Council that this would be considered under Paragraph 145 of the Framework which states that the construction of new buildings are inappropriate with a limited number of exceptions including under sub section c) which refers to the extension or alteration of a building providing that it does not result in disproportionate additions over and above the size of the original building. The Framework does not define the term, 'disproportionate'. The Council has also referenced Policies NHE5 and DES1 of the Council's Local Plan Development Management Plan 2019 (DM Plan). In this regard. Policy NHE5 specifically addresses under 1) extensions and alterations to buildings in the Green Belt but Policy DES1 seeks a high quality of design in all new development which I do not consider is directly relevant to this consideration as to whether the proposal would be inappropriate development within the Green Belt.
13. The Council has referred to previous extensions to the house although no detailed information has been provided. Similarly, the Appellant has referenced the demolition of various structures in the grounds as part of these proposals, but again these have not been set out in detail. Although the details are not before me, the canopy would, as I understand the proposal, be attached to an existing wall and to the sides of the house and former garage building. It would however be open on the side facing into the site. It would not in my view be visually prominent or in a visually prominent part of the site. Given its modest size and footprint in relation to the built form and size of the site and in particular its open sided form, I do not consider that it would be a disproportionate addition to the original building or would harm openness. It would not therefore be inappropriate development.

14. Taking all of these factors together, it is my conclusion that the proposed development would not be inappropriate development for the purposes of the Framework and development plan policy. The development would not harm the openness of the Green Belt in this location. There is therefore no need for the development to be justified by special circumstances.

Issue b) Sustainability

15. There is no dispute between the Council, the Highway Authority and the Appellant that given the location of the site in relation to public transport facilities, most trips would be by private vehicle. However, the Appellants advise that given the specialist nature of the School and the particular needs of the individual pupils, most travel movements are bespoke and public transport, even if accessible, would not be a practical option. Shared transport would be used where possible, including a school shuttle bus service for pupils and staff members.
16. It is my understanding that the School site has been specifically selected because of its location and the opportunities for a bespoke curriculum to meet the needs of the students. I have been provided with no information to suggest that the selected School site would not be a suitable site to meet the needs of the pupils.
17. Paragraph 111 of the Framework promotes the use of Travel Plans and the Appellants have submitted a Travel Plan which sets out in detail the proposed transport arrangements for students and staff. I consider that this could be controlled by condition and a signed and dated Unilateral Undertaking has been submitted to cover the costs of monitoring the Travel Plan. I appreciate that the operator could change over time but given the size of the site, the available accommodation and bespoke form of development, this would be likely to limit the number of potential alternative users.
18. Accessibility to the site and the modes of transport to be used are but one aspect of sustainable development as set out under the Framework, including under paragraph 8 and under Policy CS10 of the Council's adopted Core Strategy (Core Strategy). Whilst there is no dispute that the most trips would be by private vehicle and that therefore there would be a tension with one of the criteria (criterion 6) set out under Policy CS10 of the Core Strategy, the proposed use of the site would meet many of the economic and social aspects of sustainable development. Indeed, paragraph 94 of the Framework notes that it is important that there is a sufficient choice of school places to meet the needs of existing and new communities. Furthermore, the Framework is clear that sustainable transport should be promoted but it does recognise at paragraph 103 that opportunities to maximise sustainable transport solutions will vary between urban and rural areas and this should be taken into account in decision making.
19. In the particular circumstances of this case, and the clear reasons for the location selected, I do not consider that the proposed use would harm the principles of sustainable development. The scheme proposals would not comply with one of the criteria of Policy CS10 of the Core Strategy, but taking all aspects of sustainable development into consideration, there would be no material conflict with the overall objectives of both the Framework and Policy CS10 of the Core Strategy to secure sustainable development.

Other Considerations

20. Both the access points as existing have very restricted sight lines, particularly in a westerly direction. Without improvements to the access points and visibility splays, and given the narrowness of the road, the proposal would not provide a safe access and egress for the site and for other road users on Coulsdon Lane and would therefore be a reason for refusal.
21. The proposals include for works to the access points to improve the access arrangements and the visibility splays. It has, however, transpired that not all the land required for these works is within the control of the Appellant. I therefore agree with the Council that the resolution of this matter and the provision of the required improvements to both access points would require to be undertaken before commencement of development, and in these circumstances consider that a Grampian condition is required. I also agree with the Council that it would be necessary to ensure appropriate measures were put in place to enable parking and related servicing to be within the site for reasons of highways safety for users of Coulsdon Lane.
22. The site is within an Area of Great Landscape Value (AGLV), but given the limited external works proposed and the existing planting and vegetation to the side and rear boundaries, I am satisfied that there would be no material harm to the landscape setting of the site and to the landscape and scenic beauty of the wider AGLV. The Council also raised no concerns in this regard.
23. Although the surrounding uses are primarily residential dwellings, given the very large size of both the site and the existing building, and on the basis of the information before me, the appeal site would be suitable for the proposed use in terms of the accommodation and open space it would offer. A range of acoustic measures are proposed and subject to these being in place, I am satisfied that the proposed use would not be unneighbourly or materially harm the living conditions of surrounding neighbours. Moreover, the School would not be operating at the very times when the residential neighbours would be most likely to wish to enjoy their gardens.

Conditions

24. The Council has suggested a number of conditions in the event that planning permission is granted. I have already set out why I consider that conditions relating to the provision of access improvements and visibility splays must be pre-commencement conditions because of the substandard form of the existing access points, from the point of view of highway safety. For the same highway safety reasons, I also agree with the Council that a Construction Transport Management Plan is both required and needs to be approved and implemented pre-commencement.
25. To improve the sustainability of the proposed use in accessibility terms, a condition to require a travel plan as offered by the Appellant and requested by the Council should be imposed. However, as the submitted Travel Plan includes for a number of the measures to be in place prior to occupation, it is my view that an updated Travel Plan should be submitted and approved prior to first occupation in order that these matters can be controlled and monitored from the outset. The Appellant has offered for individual conditions to be imposed on elements of the Travel Plan but I consider that a holistic approach would be more useful to secure the overall accessibility objectives. In the

interests of sustainability, I also agree that 2 of the parking spaces should be set up for recharging but I consider that the requirements are clear and that there is no need to require further details to be first submitted.

26. A range of conditions are required to ensure that the details of various acoustic measures, to follow on from the information provided by the Appellant's Acoustic reports, are in place to ensure that the living conditions of the neighbours are respected. However, I consider that these measures need to be approved and in place prior to the use commencing rather than the development commencing. For the same reason, that is to protect the amenities of residential neighbours, I also agree that conditions to regulate the proposed use of the site are necessary.
27. Although the application was accompanied by a detailed arboricultural assessment, including with reference to trees to be felled and trees to be retained together with protection measures, this appeared to be based on the previous permitted residential redevelopment scheme. I consider that this should be revisited to ensure that it is fully comprehensive in relation to the development now proposed and in respect of the access and visibility works required to be undertaken in compliance with other conditions on the permission. In order to be effective and protect existing trees and to relate to the access and visibility works, this condition also needs to be pre-commencement. I also agree that a landscaping scheme should be provided but I consider that this can be later in the programme and need not be a pre-commencement condition.
28. Finally, I shall impose a condition to list the approved plans for the avoidance of doubt and in the interests of proper planning.
29. In accordance with Section 100ZA (5) of the Town and Country Planning Act 1990 and The Town and Country Planning (Pre-Commencement Conditions) Regulations 2018, I have requested and received the Appellant's written agreement to the imposition of the several pre-commencement conditions I consider it necessary to impose. In the interests of fairness to both the Appellant and the Council I have also provided the opportunity for comment where I have proposed changes to the conditions proposed by the Council that could be regarded as being more onerous in their requirements. I have taken the further representations into account.

Conclusion

30. For the reasons given above and having regard to all other matters raised, including in representations, I conclude that the appeal should be allowed.

L J Evans

INSPECTOR

Schedule of Conditions (1 – 20 inclusive):

- 1) The development hereby permitted shall begin not later than 3 years from the date of this decision.
- 2) The development hereby permitted shall be carried out in accordance with the following approved plans: location plan (unnumbered); 1067.P01.4; 1067.P01.3; 1067.P01.2; 1067.P01.1; 1067.P01.5 Rev D; 1067.P01.6 Rev A.
- 3) No part of the development hereby approved shall be first commenced unless and until the proposed amended vehicular access points to the site on Coulsdon Lane, with visibility splays, have been constructed and provided to the satisfaction of the Local Planning Authority, in accordance with a detailed scheme to be first submitted to and approved in writing by the Local Planning Authority, based on drawing ref: Feargal Carolan 1067.P01.5 Rev D dated 21 May 2020. Thereafter the vehicular access points shall be retained and maintained as approved and the access visibility splays shall be kept permanently clear of any obstruction over 0.6m high.
- 4) No part of the development hereby approved shall be first commenced unless and until pedestrian inter-visibility zones measuring 2m by 2m have been provided to the satisfaction of the Local Planning Authority, on each side of each access off Coulsdon Lane, the depth measured from the back of the footway (or verge) and the widths outwards from the edges of the access. No obstruction to visibility between 0.6m and 2m in height above ground level shall thereafter be erected within the area of such zones.
- 5) No part of the development hereby approved shall be first commenced unless and until a Construction Transport Management Plan (CTM Plan), has been submitted to and approved in writing by the Local Planning Authority, to include details of:
 - (a) parking for vehicles of site personnel, operatives and visitors
 - (b) loading and unloading of plant and materials
 - (c) storage of plant and materials
 - (d) programme of works (including measures for traffic management)
 - (e) provision of boundary hoarding behind any visibility zones
 - (f) HGV deliveries and hours of operation
 - (g) vehicle routing
 - (h) measures to prevent the deposit of materials on the highway
 - (i) HGV movements to or from the site shall take place between the hours of 9.00 am and 4.30 pm only, nor shall the contractor permit any HGVs associated with the development at the site to be laid up, waiting in Coulsdon Lane outside of these times
 - (j) on-site turning for construction vehicles.

The construction of the development shall be carried out in full accordance with the approved CTM Plan unless otherwise agreed in writing with the Local Planning Authority.

- 6) The use hereby permitted shall not commence until the following facilities have been provided in accordance with a scheme to be first submitted to and approved in writing by the Local Planning Authority for:
 - (a) The secure, level and covered provision for 20 bicycles storage spaces, as outlined on the approved plans.
 - (b) Clear guidance to all visitors and servicing operations (excluding waste collection) that stopping and or parking on Coulsdon Lane is not promoted and a system to ensure that visitors and service operations shall be pre-booked and managed to enter and exit the site in forward gear.
- 7) The use hereby permitted shall not commence until an updated School Travel Plan based on the Travel Plan (Ref: 11356/JT/002/04 dated May 2020 prepared by Sanderson Associates) has been submitted to and approved in writing by the Local Planning Authority, to include the timescales for further survey work to be undertaken. The Travel Plan shall be prepared in accordance with the sustainable development aims and objectives of the National Planning Policy Framework, Surrey County Council's "Travel Plans Good Practice Guide", and in general accordance with comments provided by the Travel Planning Officer. The approved Travel Plan shall be implemented for the site in accordance with a timescale to be agreed in writing with the Local Planning Authority and every subsequent occupation of the development, and thereafter the Travel Plan shall be maintained, reviewed and developed to the satisfaction of the Local Planning Authority.
- 8) The use hereby permitted shall not commence unless and until space has been laid out within the site in accordance with the approved plans for vehicles to be parked and for the loading and unloading of vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking / loading and unloading / turning areas shall be retained and maintained for their designated purpose.
- 9) The use hereby permitted shall not commence unless and until at least 2 of the available parking spaces associated with Education use are provided with a fast charge socket (current minimum requirement: 7kw Mode 3 with Type 2 connector - 230 v AC 32 amp single phase dedicated supply).
- 10) The use of the site hereby approved shall operate as a school only and during weekdays and school term time only and shall not be used for any other purposes during the weekends and holiday periods.
- 11) Notwithstanding Condition 2, the use hereby permitted shall not commence until the following details, based on the plan in Annex D of the Noise Assessment by Civil Engineering Dynamics, Rev A, have been submitted to and approved in writing by the Local Planning Authority:
 - a) detailing of the boundary treatments;
 - b) details of the siting, design, materials and finishes of the minimum 2.5m high acoustic fences;

- c) details of the siting, design, materials and finishes and acoustic performance of the canopy noise barrier between the main building and the garage annexe;
- d) The school amenity space, including the playground area, shown in blue, shall be surfaced and delineated as indicated in para 7.4 of the Noise Assessment by Civil Engineering Dynamics, Rev A.

These details shall be implemented as approved prior to the commencement of the use hereby permitted and shall be retained and maintained thereafter to the satisfaction of the Local Planning Authority.

- 12) The use hereby permitted shall not commence until a Playground and Amenity Space Management Plan has been submitted to and approved by the Local Planning Authority detailing how the amenity spaces will be managed, in particular but not limited to the management and supervision of free play within the playground, organised teaching sessions within the amenity area and management of lunchtime groups in accordance with the details specified in Rowans Hill Noise Assessment by Civil Engineering Dynamics, Rev A. The approved Management Plan shall be implemented as approved prior to the commencement of the use hereby permitted unless otherwise agreed in writing with the Local Planning Authority.
- 13) There shall be no teaching or practicing of musical instruments on the site at any time without the prior approval in writing of the Local Planning Authority. Details of the provision and mitigation will need to be approved in writing by the Local Planning Authority and this should include an adequate design of sealed glazing and suitable ventilation for thermal comfort of future occupiers.
- 14) The forest school area shall only be used within the hours of 08:30 to 14:00 hours Monday to Friday.
- 15) The use hereby permitted shall not commence until details, full specifications and elevational drawings of the kitchen extraction and filtration equipment, and an ongoing maintenance plan, have been submitted to and approved in writing by the local planning authority. The use hereby permitted shall not commence until the approved details are fully implemented. The approved fume extraction and filtration equipment shall thereafter be retained and maintained in working order for the duration of the use in accordance with the approved details.
- 16) The use hereby permitted, or the operation of any building services plant, shall not commence until an assessment of the acoustic impact arising from the operation of all internally and externally located plant has been submitted to and approved in writing by the local planning authority.

The assessment of the acoustic impact shall be undertaken in accordance with BS 4142: 2014 (or subsequent superseding equivalent) and current best practice, and shall include a scheme of attenuation measures to ensure the rating level of noise emitted from the proposed building services plant is 5db less than background.
- 17) The use hereby permitted, or the operation of any building services plant, shall not commence until a post-installation noise assessment has been carried out to confirm compliance with the noise criteria. The scheme shall be implemented in accordance with the approved details and


attenuation measures, and they shall be permanently retained and maintained in working order for the duration of the use and their operation.

- 18) No development hereby permitted shall commence including demolition and groundworks preparation until a detailed, scaled Tree Protection Plan (TPP) and the related Arboricultural Method Statement (AMS) have been submitted to and approved in writing by the Local Planning Authority. These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas of trees (RPA) shown to scale on the TPP. The AMS shall also include a supervisory regime for their implementation & monitoring with a reporting process to the Local Planning Authority. All works shall be carried out in strict accordance with these details when approved.
- 19) Notwithstanding Condition 2, the use hereby permitted shall not commence until a scheme for the landscaping of the site including the retention of existing landscape features has been submitted to and approved in writing by the Local Planning Authority. The landscaping scheme shall include details of hard and soft landscaping, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation and management programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to the use commencing or within the first planting season following completion of the development hereby approved or in accordance with a programme agreed in writing with the local planning authority.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, shrubs of the same size and species.

- 20) Notwithstanding Condition 2, the use hereby permitted shall not commence unless and until full details (and plans where appropriate) of the waste management collection point, (and pulling distances where applicable), have been submitted to and approved in writing by the Local Planning Authority. The waste collection point should be of an adequate size to accommodate the bins and containers required for the approved use. The development shall be provided with the above facilities in accordance with the approved details prior to the use first commencing.

 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	06 April 2022
	REPORT OF:	HEAD OF PLANNING
	AUTHOR:	Matthew Holdsworth
	TELEPHONE:	01737 276752
	EMAIL:	Matthew.Holdsworth@reigate-banstead.gov.uk
AGENDA ITEM:		WARD: Banstead Village

APPLICATION NUMBER:	22/00939/F	VALID:	27/04/2022
APPLICANT:	MD Private Ltd.	AGENT:	
LOCATION:	103B HIGH STREET, BANSTEAD		
DESCRIPTION:	Extension of first floor at rear to form 2 self-contained units of accommodation.		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

This application is referred to Committee in accordance with the Constitution as the application site is for net 2 dwellings

SUMMARY

The application is for the extension of the existing office building at the first floor level to the rear of 103 High Street, Banstead to create two 1 bedroom flats. This follows on from the approved and extant permission 20/02468/F which granted permission for a 2 bedroom duplex apartment. The proposal is slightly larger than that approved, most notably 1500mm wider and 1100mm deeper. There is no objection to the loss of the office space.

It is not considered that the slight increase in size would cause significant or harmful impact to the amenity of neighbouring properties and the amenity space and size of the properties are considered compliant with policy.

No parking is proposed; however the site is within a highly sustainable area, on the high street and within the town centre of Banstead with many local facilities and a good bus service. In addition, the applicant has provided details of a parking stress survey that confirms that there would there is adequate off road capacity existing within local streets. In addition, the proposal would also provide secure bicycle spaces.

RECOMMENDATION(S)

Planning permission is GRANTED subject to conditions.

Agenda Item 10

Planning Committee
06 July 2022

Agenda Item:
22 00939/F

Consultations:

Highway Authority: No objections subject to conditions relating to space for secure bicycle storage and a construction transport management plan.

Representations:

Letters were sent to neighbouring properties on 29 April 2022. Eight letters of representation from local residents have been received raising the following concerns:

Issue	Number	Response
Overlooking and loss of privacy	7	See paragraphs 6.10 – 6.13
Overshadowing	5	See paragraphs 6.10 - 6.13
Out of character	5	See paragraphs 6.4 – 6.6
Poor design	4	See paragraphs 6.4 – 6.6
Overdevelopment	5	See paragraphs 6.4 – 6.6
Inadequate parking	5	See paragraphs 6.16 – 6.18
Noise and disturbance	3	See paragraphs 6.10 – 6.13
Loss of private view	2	Not a material planning consideration

1.0 Site and Character Appraisal

1.1 The property is located on the northern side of Banstead High Street. It is situated within a terraced parade of shops. The building is three stories in height with class E retail at ground floor and two levels of residential above. There is existing access to a parking area at the rear of the parade and an alleyway running along the side of the application itself providing existing access to the first floor office and the residential units.

1.2 There are no significant trees likely to be affected by the proposed development. The site is relatively flat in level.

2.0 Added Value

2.1 Improvements secured at the pre-application stage: None

2.2 Further improvements could be secured: Materials as specified, landscaping, broadband condition, obscure glazing to south facing windows, water and energy efficiency condition, secure bicycle storage, construction transport management plan, bins, privacy screen

3.0 Relevant Planning and Enforcement History

- 3.1 07/00906/F - Conversion of rear office into 1 bedroom flat - refused.
- 3.2 19/02032/F - Extension of first floor office at rear and addition of a flat above - refused and dismissed on appeal.
- 3.3 20/02468/F - Extension to first floor office at rear to form a self contained unit of residential accommodation – approved with conditions.

4.0 Proposal and Design Approach

- 4.1 The application follows on from the approved and extant permission (20/02468/F) which granted permission to extend the existing first floor office to the rear to form a self contained unit of residential accommodation.
- 4.2 The proposal seeks to create two 1 bedroom flats on a similar footprint to the approved scheme. However, the proposal would be 1500mm wider with part of the extension cantilevered over the existing alleyway and it would extend an additional 1m towards the rear of 103 High Street when compared to the previous approval.
- 4.3 The access to the flats would be as per the previous approval, via steps that lead to the flat area at first floor with small amenity areas proposed to the front of the flats.
- 4.4 No parking is proposed but there would be secure cycle storage provided.
- 4.5 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:
 - Assessment;
 - Involvement;
 - Evaluation; and
 - Design.
- 4.6 Evidence of the applicant’s design approach is set out below:

Assessment	The proposal seeks to provide extra housing in an existing urban area with no impact on surrounding green areas or fabric. The character and density of the design and its location to local services and transport links has been carefully considered.
Involvement	No community consultation took place.
Evaluation	The statement does not include any evidence of other development options being considered.

Agenda Item 10

Planning Committee
06 July 2022

Agenda Item:
22 00939/F

Design	The proposal has been designed to respect the character of the surrounding buildings and areas by adopting a similar design to many of the buildings on the high street.
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4.6 Further details of the development are as follows:

Site area	202sqm
Proposed parking spaces	0
Parking standard	2
Net increase in dwellings	2

5.0 Policy Context

5.1 Designation

Urban Area

5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)
CS4 (Valued Townscapes and Historic Environment)
CS10 (Sustainable Development),
CS11 (Sustainable Construction),
CS15 (Affordable Housing)

5.2 Reigate & Banstead Development Management Plan 2019

Economic Development	EMP4
Design, Character, and amenity	DES1, DES5, DES8
Transport, Access, and parking	TAP1
Climate Change resilience	CCF1
Infrastructure to support growth	INF3

5.3 Other Material Considerations

National Planning Policy Framework

Supplementary Planning Guidance	Surrey Design Local Distinctiveness Design Guide Householder Extensions and Alterations
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Other	Human Rights Act 1998
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6.0 Assessment

6.1 The application site is situated within the urban area where there is a presumption in favour of sustainable development and where the principle of such development is acceptable in land use terms.

6.2 The main issues to consider are:

- Principle of change of use
- Impact on local character
- Neighbour amenity
- Highway and parking matters
- CIL
- Affordable housing
- Sustainability and Climate Change
- Other matters

Principle of change of use

- 6.3 It is noted that there would be a loss of office space (Class E) measuring 29sqm from the proposed development. Development Management Plan (DMP) Policy EMP4 refers to employment use outside of employment areas and states:
1. The loss of employment land and premises will only be permitted if:
 - a. it can be clearly demonstrated that there is no reasonable prospect of (or demand for) the retention or redevelopment of the site for employment use.
- 6.4 Annexe 3 of the DMP sets out a number of requirements that the proposed marketing of the site must undergo and it must be shown to the council's satisfaction that marketing has been unsuccessful for all relevant floorspace proposed to be lost through redevelopment or Change of Use.
- 6.5 The applicants have provided marketing details which show that the unit has been on the market since 13 January 2020 which is now over a year and is still currently a live listing. The listing is on Rightmove Commercial and the agents own website. No marketing board has been used but given the location of the property to the rear of an alleyway the site is not visible from the high street pavement and so the lack of a board in this instance is considered reasonable. The Council's Asset Manager has considered the submitted information. He has stated that based on the evidence provided, the marketing is considered sufficient that there is no likely prospect of the building being used as office space and consequently the proposal complies with policy EMP4 and a change of use from office space to residential is acceptable in principle.

Impact on local character

- 6.4 The application follows on from the previous approved application for the extension of the first floor office to create a duplex apartment (20/02468/F). This application carries significant and substantial weight when assessing this application as the principle, appearance, scale, bulk and massing are very similar to that previously approved.
- 6.5 The proposal would create two additional dwellings to the rear of 103 High Street. The proposal would be 1500mm wider with part of the extension cantilevered over the existing alleyway and it would extend an additional 1m towards the rear of 103 High Street when compared to the previous approval.

Agenda Item 10

Planning Committee
06 July 2022

Agenda Item:
22 00939/F

- 6.6 The design of the extension is relatively utilitarian and would have a flat roof as per the previous approval. However, this would be in keeping with the rear elevations of this part of the high street which has numerous flat roofed extensions of varying heights and styles; the majority of which are flat roofed. The proposal shows that the external materials would generally match the existing rear extensions to the property and would be broadly in keeping with the rear of the shops in this part of the High Street.
- 6.7 The proposed dwellings are proposed to have a small amenity area in the flat roofed section between the proposed new extension and the existing flat above the shop; whilst small, this is considered acceptable and is similar to other arrangements to the rear of the properties in the high street. A condition will be added to the permission requiring details of the enclosure of the amenity area.
- 6.8 The two flats are 37sqm in size and these would comply with nationally prescribed space standards and complies with policy DES5.
- 6.9 It is considered therefore when assessing alongside the approved planning application that the quantum of development and the design of the building is appropriate on this site and the proposal complies with policy DES1 in this regard.

Neighbour amenity

- 6.10 The proposal would increase the massing and bulk of the existing office significantly with an extra residential storey and the increase in floor space. It would bring the current building 2.1m closer to the flat above 103 High Street, which is an increase of just over 1m from the previous approved and extant application. Concern has been raised from this flat (103A) in terms of overlooking and loss of privacy as well as dominance and loss of light. It is noted that the proposed windows that serve habitable rooms of the proposed dwellings face away from directly overlooking this property. The windows that do face no. 103A are shown on the plans to be obscure glazed and high level, and a condition will be added to the permission to ensure that the windows remain obscure glazed.
- 6.11 It is noted that the windows that serve habitable rooms of the flat at 103A are in the second floor and these would not suffer from a significant loss of light as shown on the plans which show the 25 degree line drawn from those windows clearing in the vertical axis. However, whilst the outlook from these windows and the first floor windows would change, it is not considered that this would cause such harm as to warrant refusal on this ground. It is considered, on balance, that this proposal would not cause significant harm to the amenity of the flat at 103A.
- 6.12 Turning to other local residential properties, it is noted that 105 High Street has a large first floor external amenity area above the flat roofed section of the building. This property would not be overlooked by the new proposal and it is

not considered that this would cause significant or material harm to that property. A condition has been added requiring details of the enclosure of the amenity area to that property in order to prevent overlooking or a material loss of amenity to no.105.

- 6.13 Concern has been raised from overlooking from residents in properties to the rear in Garden Close. These properties are approximately 39m from the rear of those properties. Whilst it is noted that there could be some minor overlooking of the rear gardens of those properties (the boundaries of which are around 22m from the rear windows), this is not considered to cause significant or material harm to those properties. In addition, the rear facing windows are of the same design and scale to that previously approved.
- 6.14 Consequently, it is considered that the proposal would not cause significant or material harm to the amenity of neighbouring properties and therefore, the proposal complies in this regard with policy DES1.

Highway and Parking Matters

- 6.15 The County Highways Authority has assessed the proposed development on safety, capacity and policy grounds and have recommended that conditions should be imposed on the permission relating to space being laid out for secure bicycle storage, and a construction transport management plan.
- 6.16 DMP policy TAP1 states that new residential development should: "Include car parking and cycle storage for residential and non-residential development in accordance with adopted local standards (see Annex 4) unless satisfactory evidence is provided to demonstrate that non-compliance would not result in unacceptable harm. Such evidence could include on-street parking surveys, evidence of parking demand, and/ or further information on accessibility. Development should not result in unacceptable levels of on-street parking demand in existing or new streets."
- 6.17 The applicant has provided a parking stress survey as part of the planning application and this has been carried out using the Lambeth Methodology. The parking stress survey demonstrates relatively low levels of overnight parking stress (less than 60% within the study area) and this demonstrates that adequate capacity existing within neighbouring streets to accommodate overnight parking demands without unduly prejudicing the existing levels of parking stress.
- 6.18 The lack of on-site parking is supported by the County Highways Authority as there is no requirement or necessity for future occupiers to own a private motor vehicle. In addition, the site is located in central Banstead with relatively good public transport opportunities and local services within walking and cycling distance - the site is considered sustainable in transport terms.

CIL

Agenda Item 10

Planning Committee
06 July 2022

Agenda Item:
22 00939/F

- 6.19 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, roads, public transport and community facilities which are needed to support new development. This development would be CIL liable although the exact amount would be determined and collected after the grant of planning permission.

Affordable Housing

- 6.20 Development Management Plan DES6 states that on developments providing 11 or more homes, 30% of the homes on site should provide affordable housing. This supersedes the Core Strategy policy CS15 in its entirety.
- 6.21 In view of this, the Council is not presently requiring financial contributions from applications such as this resulting in a net gain of 10 units or less.

Sustainability and Climate Change

- 6.22 Policy CCF1 of the Councils Development Management Plan 2019 seeks to ensure that all new development contributes to reducing carbon emissions. New development will be encouraged to incorporate passive and active energy efficiency measure and climate change resilience measures and renewable energy technologies. In order that the proposed development contributes to achieving these aims, in the event that planning permission is granted, conditions requiring demonstration that it will meet the national water efficiency standard of 110litres/person/day and achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations would be attached.

Other Matters

- 6.23 Electronic communication networks: Policy INF3 criteria 1 states that "The Council will require all new development to be connected with high speed and reliable broadband". A condition has been added to the permission to this effect.

CONDITIONS

1. The development hereby permitted shall be carried out in accordance with the following approved plans.

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

Note: Should alterations or amendments be required to the approved plans, it will be necessary to apply either under Section 96A of the Town and Country Planning Act 1990 for non-material alterations or Section 73 of the Act for minor material alterations. An application must be made using the standard

application forms and you should consult with us, to establish the correct type of application to be made.

Plan Type	Reference	Version	Date Received
Roof Plan	BHS/P/04		27.04.2022
Location Plan	BHS/LP/01		22.04.2022
Block Plan	BHS/BP/01		22.04.2022
Floor Plan	BHS/P/01		22.04.2022
Elevation Plan	BHS/E/03		22.04.2022
Floor Plan	BHS/P/02	C	22.04.2022
Elevation Plan	BHS/E/01	C	22.04.2022
Elevation Plan	BHS/E/02	D	22.04.2022
Elevation Plan	BHS/E/02	B	22.04.2022
Section Plan	BHS/E/05	C	22.04.2022

- The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004

- The development shall be carried out using the external facing materials specified in the application and no others without the prior written consent of the Local Planning Authority.

Reason: To ensure that the development hereby permitted is only constructed using the appropriate external facing materials or suitable alternatives in the interest of the visual amenities of the area with regard to Reigate and Banstead Development Management Plan policy DES1.

- The windows in the south-eastern elevations of the development hereby permitted shall be glazed with obscured glass and shall be non-opening unless the parts of the window which can be opened are more than 1.7 metres above the floor of the room in which the window is installed and shall be maintained as such at all times.

Reason: To ensure that the development does not affect the amenity of the neighbouring property by overlooking with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

- The development hereby approved shall not be first occupied unless and until the following facilities have been provided in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority for the secure parking of bicycles within the development site, and thereafter the said approved facilities shall be provided, retained and maintained to the satisfaction of the Local Planning Authority.

Reason: In recognition of Section 9 'Promoting Sustainable Transport' in the National Planning Policy Framework 2019 and to meet the objectives of the

Agenda Item 10

Planning Committee
06 July 2022

Agenda Item:
22 00939/F

NPPF (2019), and to satisfy policies DES8 and TAP1 of the Reigate and Banstead Development Management Plan (2019).

6. No development shall commence until a Construction Transport Management Plan, to include details of:
- (a) parking for vehicles of site personnel, operatives and visitors
 - (b) loading and unloading of plant and materials
 - (c) storage of plant and materials
 - (d) programme of works (including measures for traffic management)
 - (e) provision of boundary hoarding behind any visibility zones
 - (f) measures to prevent the deposit of materials on the highway
 - (g) before and after construction condition surveys of the highway and commitment to fund the repair of any damage caused
 - (h) on-site turning for construction vehicles
- has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019, Reigate and Banstead Core Strategy 2014 Policy CS17 and Reigate and Banstead Development Management Plan 2019 policies TAP1 and DES8.

7. The development hereby approved shall not be first occupied unless and until the following facilities have been provided in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority for the storage of bins and thereafter the said approved facilities shall be provided, retained and maintained to the satisfaction of the Local Planning Authority.

Reason: In order that the residential development hereby permitted has suitable storage for bins and to comply with policy DES1 of the Development Management Plan 2019.

8. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:
- a) A broadband connection accessed directly from the nearest exchange or cabinet,
 - b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

9. The development hereby approved shall not be first occupied unless and until an Energy and Water Efficiency Statement has been submitted to and approved

in writing by the Local Planning Authority. The Statement shall detail how the development will:

- a) Ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day,
- b) Achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations.

The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

INFORMATIVES

1. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
 - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
 - (e) There should be no burning on site;
 - (f) Only minimal security lighting should be used outside the hours stated above; and
 - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - www.ccscheme.org.uk/index.php/site-registration.

2. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.info.

Agenda Item 10

Planning Committee
06 July 2022

Agenda Item:
22 00939/F

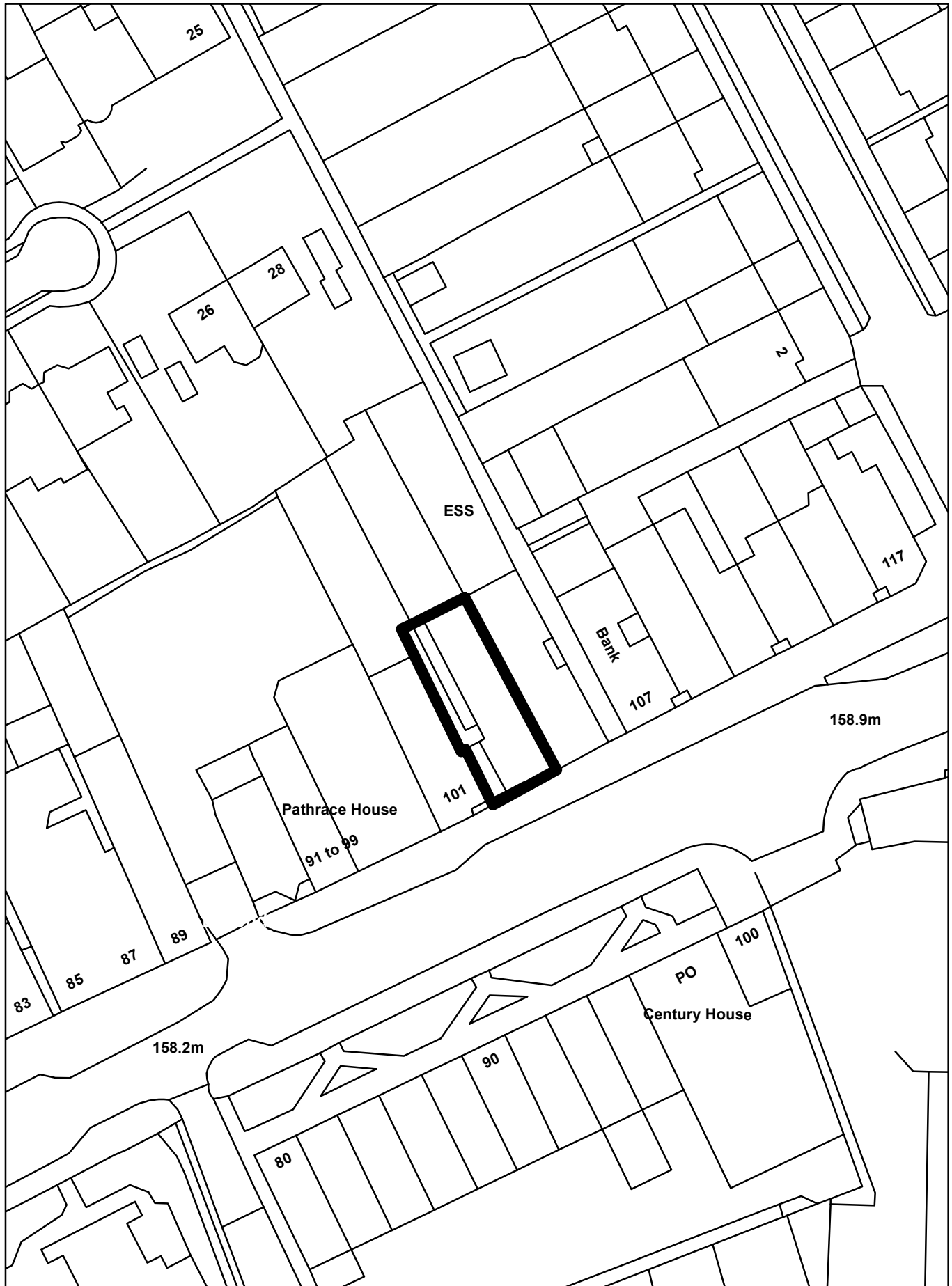
3. The applicant is advised that the Borough Council is the street naming and numbering authority and you will need to apply for addresses. This can be done by contacting the Address and Gazetteer Officer prior to construction commencing. You will need to complete the relevant application form and upload supporting documents such as site and floor layout plans in order that official street naming and numbering can be allocated as appropriate. If no application is received the Council has the authority to allocate an address. This also applies to replacement dwellings. If you are building a scheme of more than 5 units please also supply a CAD file (back saved to 2010) of the development based on OS Grid References. Full details of how to apply for addresses can be found http://www.reigate-banstead.gov.uk/info/20277/street_naming_and_numbering
4. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, to contact the Council's Neighbourhood Services team to confirm the number and specification of recycling and refuse bins that are required to be supplied by the developer. All developer enquires on recycling and refuse bin ordering, collections and discussing waste matters is via our department email address RC@reigate-banstead.gov.uk . Please also note our website area for developers https://www.reigate-banstead.gov.uk/info/20062/recycling_and_refuse/392/fees_for_recycling_and_refuse_services/3.
5. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
6. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
7. The developer would be expected to agree a programme of implementation of all necessary statutory utility works associated with the development, including liaison between Surrey County Council Streetworks Team, the relevant Utility Companies and the Developer to ensure that where possible the works take the route of least disruption and occurs at least disruptive times to highway users.
8. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : [Climate Change Information](#).

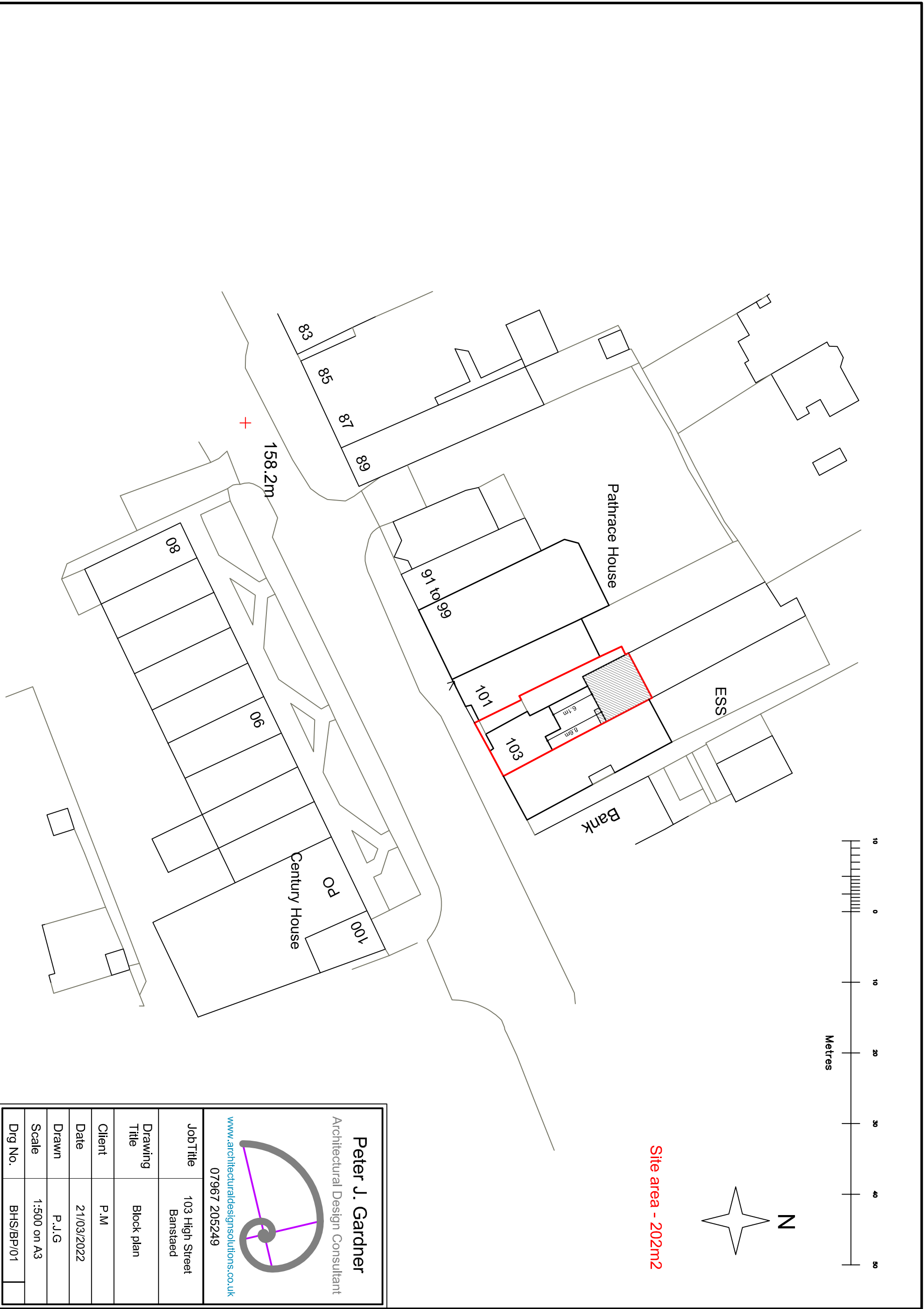
REASON FOR PERMISSION

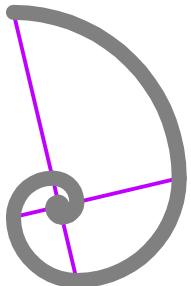
The development hereby permitted has been assessed against development plan policies DES1, DES5, DES8, TAP1, EMP4, CCF1, INF3, and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

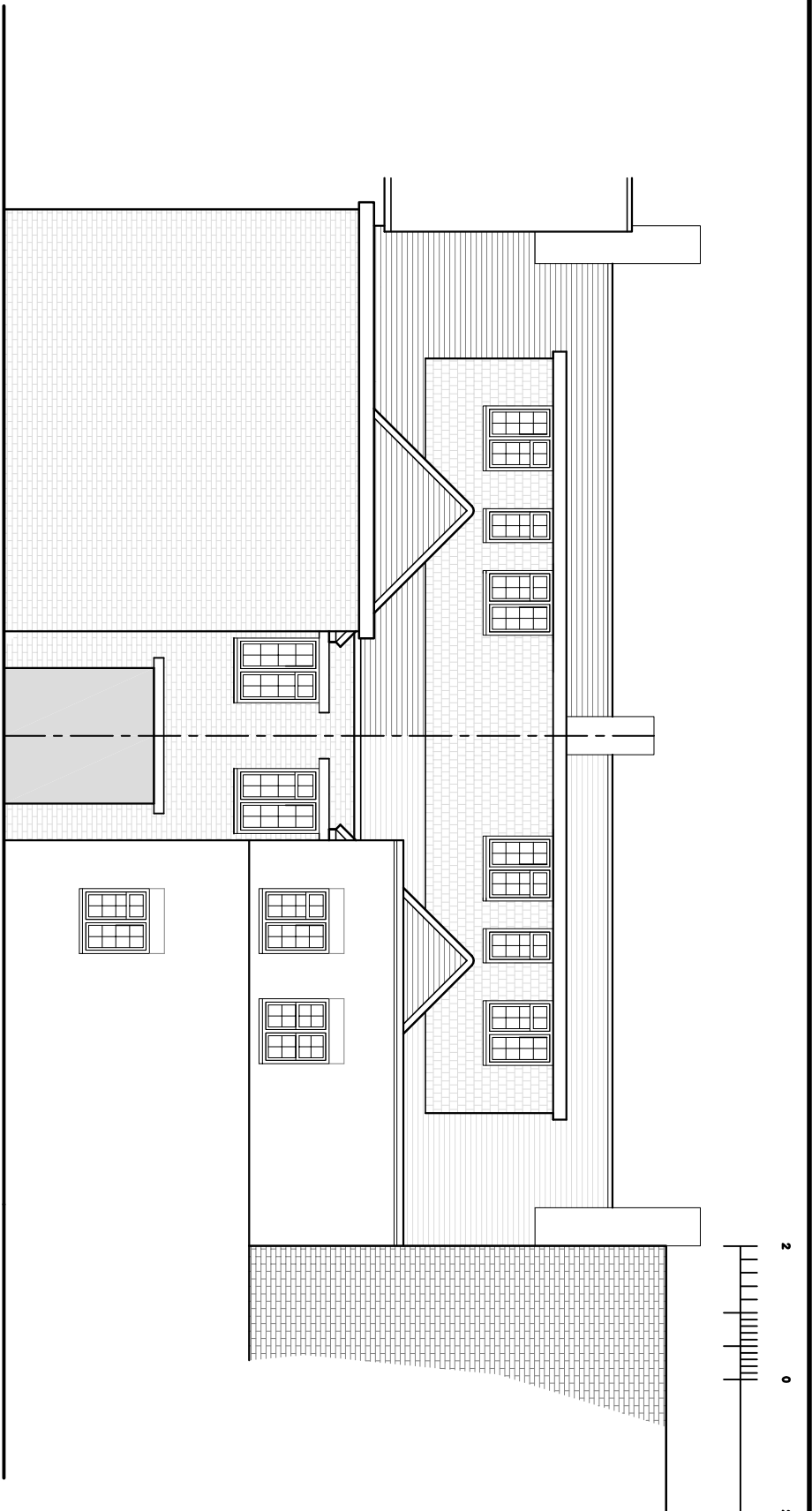
The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

Agenda Item 10
22/00939/F 103B High Street Banstead SM7 2NL

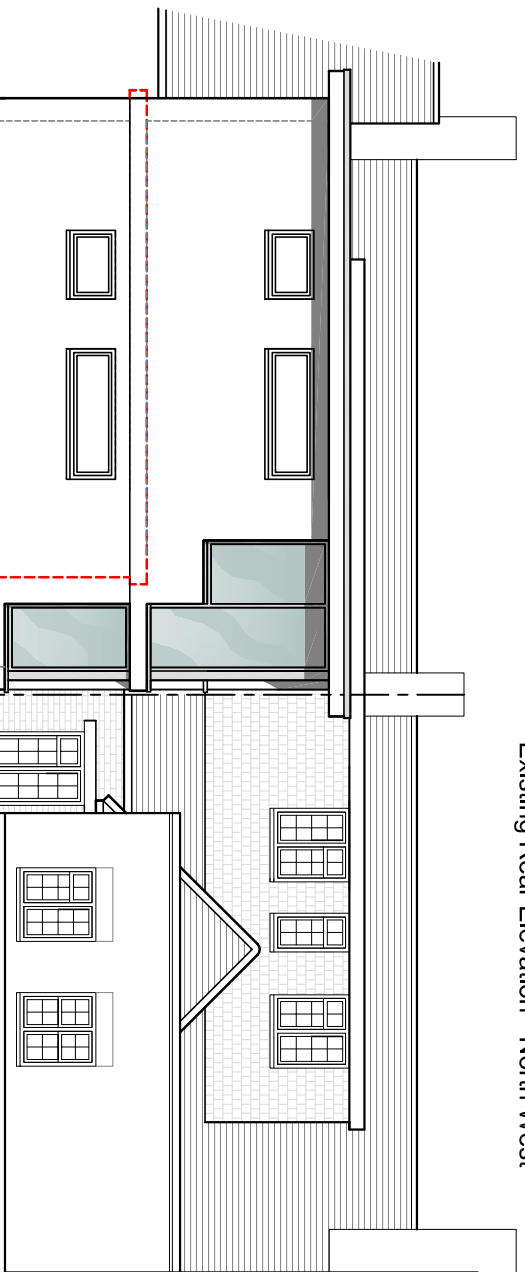




<p>Peter J. Gardner Architectural Design Consultant</p>  <p>www.architecturaldesignsolutions.co.uk 07967 205249</p>	
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Drawing Title	Block plan
Client	P.M
Date	21/03/2022
Drawn	P.J.G
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Drng No.	BHS/BP/01

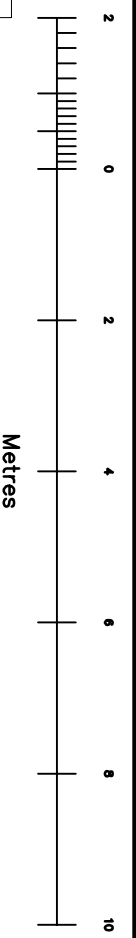


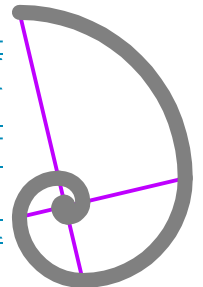
Existing Rear Elevation - North West

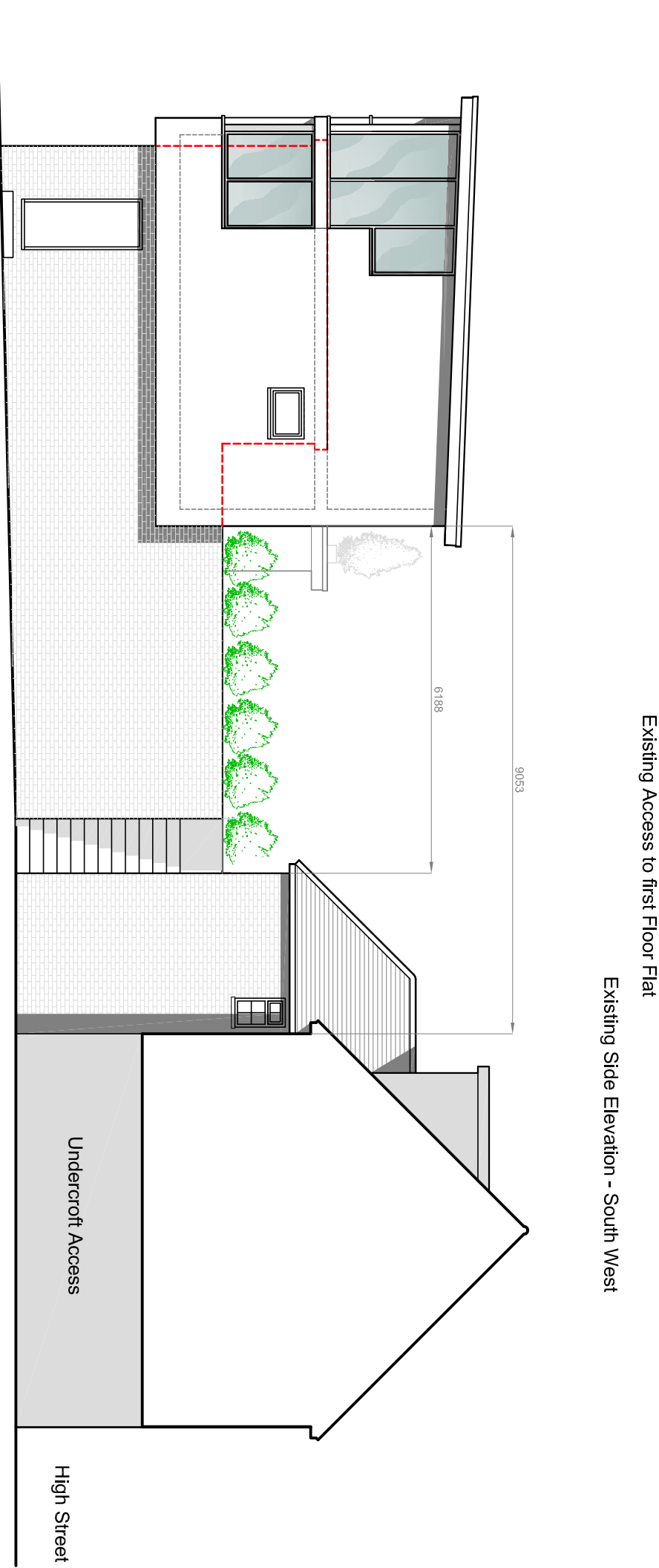
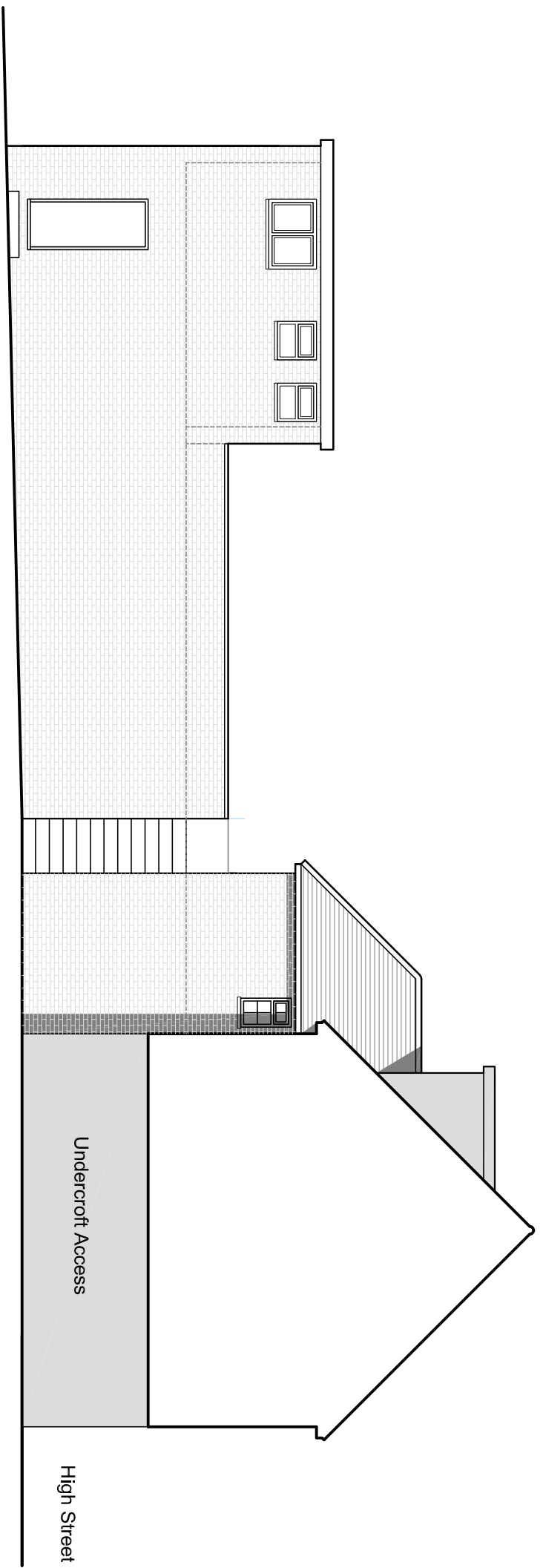
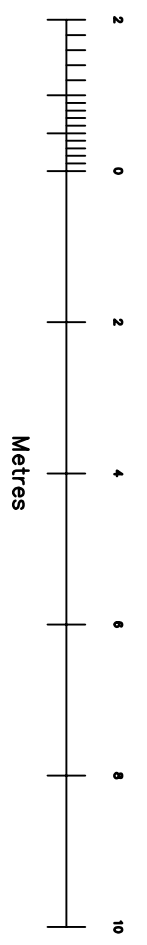


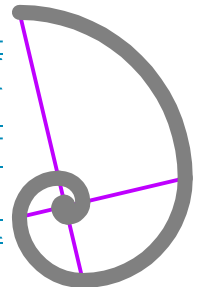
Proposed Rear Elevation - North West

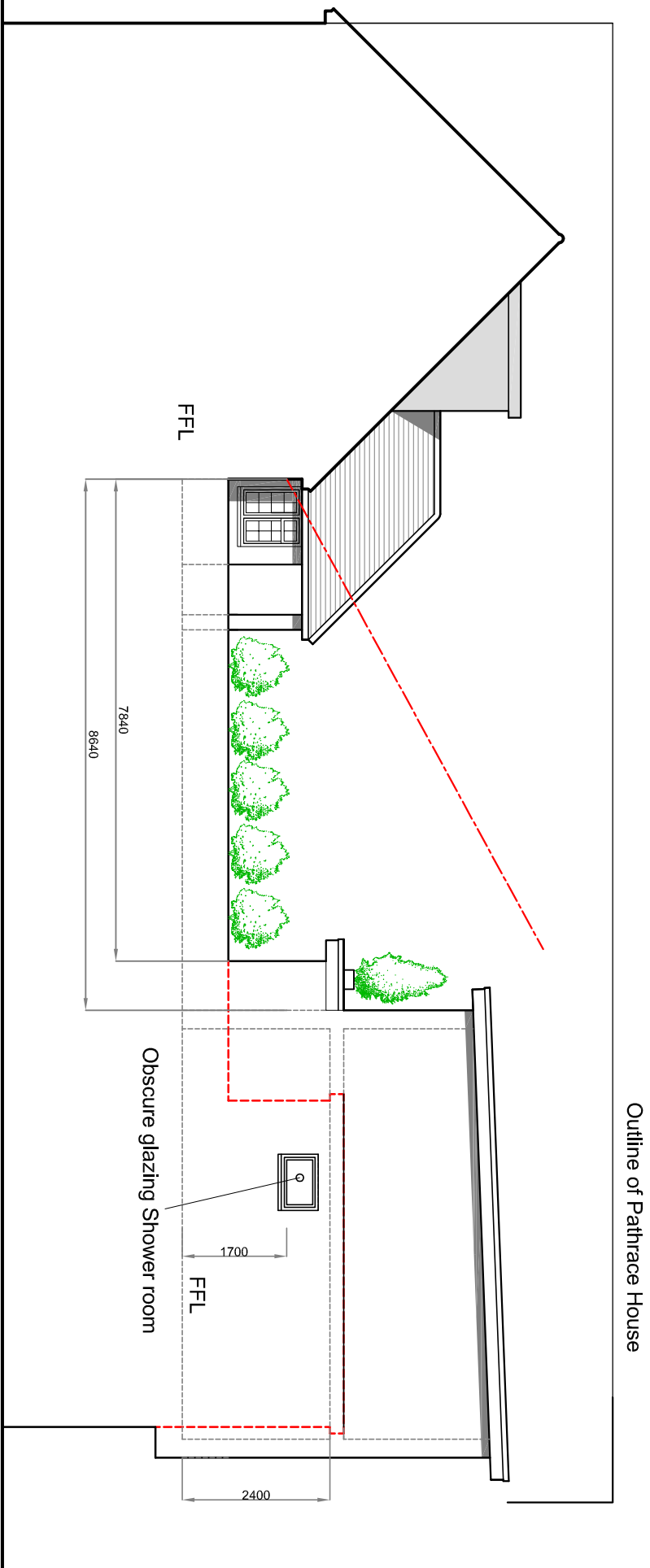
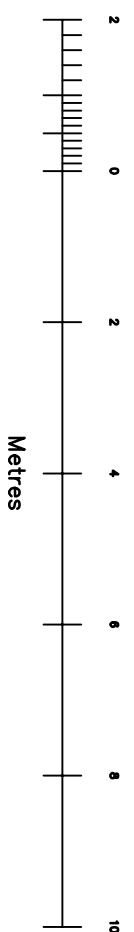
No. 103 High Street



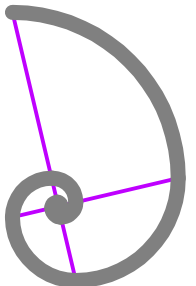
 <p>Peter J. Gardner Architectural Design Consultant</p> <p>www.architecturaldesignsolutions.co.uk 07967 205249</p>	
Job Title	103 High Street Banstead
Drawing Title	Rear Elevation - NW
Client	P.M
Date	21/03/2022
Drawn	P.J.G
Scale	1:100 on A3
Drwg No.	BHS/E/02
	B

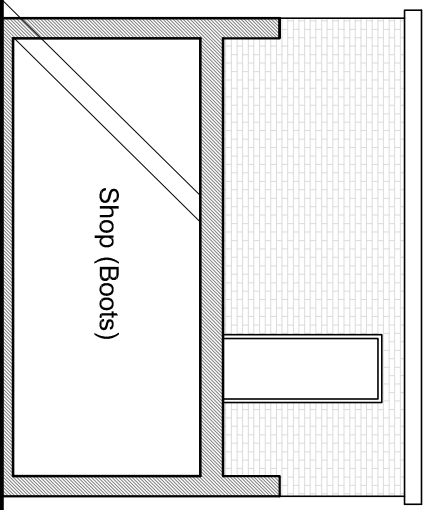
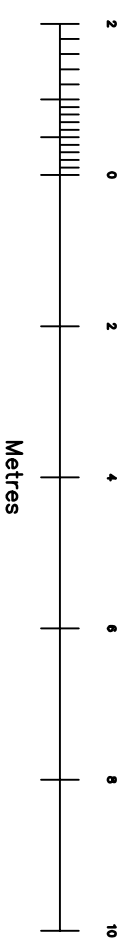


 <p>Peter J. Gardner Architectural Design Consultant</p> <p>www.architecturaldesignsolutions.co.uk 07967 205249</p>	
Job Title	103 High Street Banstead
Drawing Title	Proposed Side Elevation - SW
Client	P.M
Date	12/04/2022
Drawn	P.J.G
Scale	1:100 on A3
Drwg No.	BHS/E/02
	D

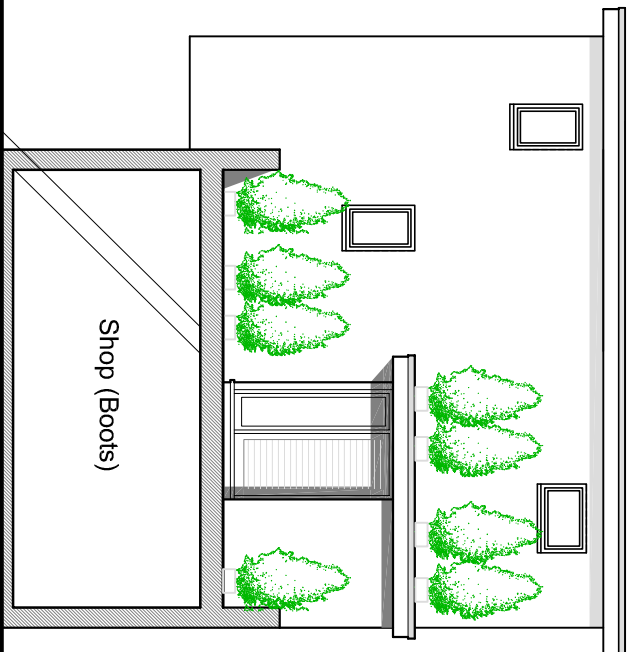


Proposed Side Elevation - North East

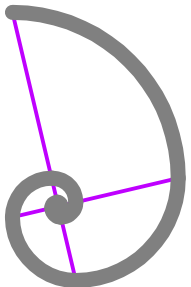
 <p>Peter J. Gardner Architectural Design Consultant</p> <p>www.architecturaldesignsolutions.co.uk 07967 205249</p>	
Job Title	103 High Street Banstead
Drawing Title	Proposed Side Elevation - NE
Client	P.M
Date	12/04/2022
Drawn	P.J.G
Scale	1:100 on A3
Dwg No.	BHS/E/01
	C



Existing Front Elevation/Section - South East



Proposed Front Elevation/Section - South East


 <p>Peter J. Gardner Architectural Design Consultant</p>	
<p>www.architecturaldesignsolutions.co.uk 07967 205249</p>	<p>103 High Street Banstead</p>
Job Title	103 High Street Banstead
Drawing Title	Front Elevations - SE
Client	P.M
Date	21/03/2022
Drawn	P.J.G
Scale	1:100 on A3
Drg No.	BHS/E/05
	C

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Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	6 th July 2022
	REPORT OF:	HEAD OF PLANNING
	AUTHOR:	Michael Parker
	TELEPHONE:	01737 276339
	EMAIL:	Michael.parker@reigate-banstead.gov.uk
AGENDA ITEM:	11	WARD: <i>Banstead</i>

APPLICATION NUMBER:	21/03311/F	VALID:	20/01/2022
APPLICANT:	Proceed Capital Ltd	AGENT:	Fredrick Adam Ltd
LOCATION:	ALVIS HOUSE PARK ROAD BANSTEAD SURREY SM7 3EF		
DESCRIPTION:	A change of use of land to class c3, the removal of the existing areas of hardstanding, retention and restoration of bunker 4, the demolition of the remaining structures, and redevelopment to provide ten detached dwellings accessed via an internal circuit road framing a central water body. To include associated works for the purpose of landscaping. As amended on 25/03/2022 and on 12/04/2022.		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

SUMMARY

The proposed development seeks planning permission for the removal of the existing areas of hardstanding, retention and restoration of bunker 4, the demolition of the remaining structures, and redevelopment to provide ten detached dwellings – on the site which is within the Metropolitan Green Belt between Banstead and Chipstead.

Permission was granted in 2017 under application ref. 16/01013/F for the redevelopment of the site for 9 detached houses. All relevant pre-commencement conditions were discharged and demolition works have already taken place on the site with the majority of the existing bunkers already removed. As such officers are satisfied that this site application has been lawfully commenced and is therefore an extant permission which is a material consideration.

In view of the case presented by the applicant, including the 2003 Lawful Development Certificate, and an inspection of the site, and the extant permission it is considered to be previously developed for the purposes of the National Planning Policy Framework (NPPF). In these circumstances, the provisions of paragraph 149 of the NPPF are engaged; this allows for limited infilling or the partial or complete redevelopment of previously developed land (brownfield land), whether redundant or in continuing use (excluding temporary buildings), which would not have a greater

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

impact on the openness of the Green Belt than the existing development. The development is therefore appropriate in principle subject to it not having a greater impact on the openness of the Green Belt and the assessment of all other planning issues.

Overall, the proposal would bring about a reduction in the overall footprint, volume and number of buildings and structures present on the site. The scheme would also significantly reduce the extent of hardstanding, with large areas given back over to soft landscaping, albeit as part of sub-divided private curtilages. Development will also bring an end to the vehicle storage operations on the site which in themselves are considered harmful to the Green Belt. It is therefore considered that, on balance, in accordance with paragraph 149 of the Framework, the development of this brownfield site would not have a greater impact on the openness of the Green Belt than the existing development. Conditions are recommended to secure further details of the extent of the proposed garden areas, the extent and design of the proposed boundary treatments and also landscaping to ensure that the visual impact of the proposal are as expected and to limit the impact on openness of the proposed garden areas.

The loss of all but 1 of the original eight Second World War ammunition store bunkers and some associated paraphernalia were considered in detail during the consideration of the 2016 application as well as the potential archaeological impacts. Under the 2016 application the loss of all but one of the bunkers was approved subject to the reinstatement of the bunker and an undertaking to enable public access to the bunker and the relocation of two air raid shelters. Following the granted of the 2016 permission demolition works were carried out which have removed all but the 1 retained bunker. The current application also proposes to retain the bunker in line with the agreed details under the 2016 application. The Conservation has raised no objection to the proposals subject to the re-imposition of the conditions on the 2016 which secure the retention and reinstatement of the retained bunker. The County Archaeologist has confirmed that there are no further archaeology requirements or concerns at this site.

The design and layout of the site is considered to be acceptable and would create a distinctive and high quality development which fits into its landscaped, woodland setting. Whilst contemporary in nature, the design approach is considered to be appropriate, particularly in view of the unique position of the site and the contained nature and character of the site. A Landscape and Visual Evidence and Appraisal (LVEA) has been submitted to support the application. This considers the impact of the proposed higher dwellings and their design (including light spill) on the wider area taking in to account the topography of the surrounding area and the proposed tree works and replacement landscaping. The report demonstrates that whilst trees are being removed the impact on longer views and wider landscape is acceptable and the proposed replacement planting will ensure that, in the longer term, there is a neutral to minor beneficial impact.

The relationship of the development to, and separation distances with, neighbouring properties are such that the proposed development would have no adverse impact on the amenity of neighbouring properties.

The site is separated from the built up area, albeit in reasonable proximity to services and facilities in Banstead Town Centre. It is however acknowledged that due to the nature of the site residential development would be unlikely to promote sustainable travel choices and that occupants would be reliant on private car. In this respect there would be some conflict with policy which weighs against the scheme; however, the harm arising is not considered to be so significant and prejudicial as to outweigh the positive benefits arising from a development which is compliant with local and national policy in other respects. This was also the conclusion reached for the 2016 scheme which proposed 9 dwellings. The current scheme for 10 dwellings is therefore materially very similar in this respect.

The scheme is considered to be acceptable, subject to conditions, with regard to highway safety, impact on trees and ecology, crime, surface water drainage, contamination and sustainable construction measures.

The proposed development is therefore considered to strike a balance between retaining and restoring the heritage interest of the site in a way which is consistent and appropriate with its significance and providing housing through the effective redevelopment of a brownfield site in a way which is appropriate to its location within the Green Belt.

RECOMMENDATION

Planning permission is **GRANTED** subject to conditions.

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

Consultations:

Highway Authority: No objection on highway safety or capacity grounds but does have concerns regarding sustainability/accessibility grounds

Minerals and Waste Planning Authority: No objection subject to adequate waste storage facilities

Surrey County Council Lead Local Flood Authority (LLFA): satisfied that the proposed drainage scheme meets the requirements of the NPPF, accompanying nPPG and the Non-Statutory Technical Standards for sustainable drainage systems.

Surrey Police – Request that a condition be imposed for require Secured by Design accreditation for the development.

Environmental Protection Officer – require contamination conditions due to historic use of the site.

Neighbourhood Services – No concerns raised subject to the access being constructed to Surrey Highway Standards, correct access being provided if gate added to entrance and adequate provision of bins to residents

Park Road Residents Association – in principle no objection but make comments in relation to pedestrian safety and the preservation of the bunker, including provision of an information board

Surrey County Council Archaeological Officer – Confirms that no additional recording or investigation will be required. Supports the proposals to secure the repair and reinstatement of the retained bunker and relocation of metal air raid shelters.

Surrey Wildlife Trust – No objection subject to conditions in relation to Construction Environmental Management Plan (CEMP) and Landscape and Ecological Management Plan (LEMP)

Representations:

None received to date

1.0 Site and Character Appraisal

- 1.1 The application site is the former Banstead Anti-Aircraft Ammunition Depot together with the long private access road linking the site to Park Road.
- 1.2 The site consists of the Second World War military storage bunkers along with a number of more recent agricultural/industrial structures and extensive hardstanding which has until recently been lawfully used for vehicle storage. The bunkers and the site generally, are somewhat in a state of disrepair.

- 1.3 The site is well screened by tree cover on the outer boundaries, giving the site a secluded and largely isolated feel and surrounded by open fields. Adjacent to the site and served by the same shared access road are three detached properties.
- 1.4 To the south on Park Road are a number of substantial detached properties set within large plots. Further north on Park Road is the Park Road/Mint Road Conservation Area, including Mint Cottages, the public house and the Queen Elizabeth Foundation Rehabilitation complex. The site is distinct from the wider locality.
- 1.5 As a whole, the main Courtlands Farm site, excluding the access road, comprises a site area of approximately 2.85ha.
- 1.6 Permission was granted in 2017 under application ref. 16/01013/F for the redevelopment of the site for 9 detached houses. All relevant pre-commencement conditions were discharged and demolition works have already taken place on the site with the majority of the existing bunkers already removed. As such officers are satisfied that this site application has been lawfully commenced and is therefore an extant permission which is a material consideration.

2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: Pre-application advice was provided under application PAM/21/00294. Concerns were raised regarding the impact on openness due to spread of development and also heritage matters with regard to retention of retained bunker and its setting. The pre-application response also set out details of the required technical details for any subsequent application.
- 2.2 Improvements secured during the course of the application: further information provided regarding heritage matters and surface water drainage.
- 2.3 Further improvements could be secured: Condition would control landscaping, materials and other improvements as well as ensuring the restoration and maintenance of the bunker are secured in full accordance with the submitted details.

3.0 Relevant Planning and Enforcement History

03/00700/CLE	For an existing use of land, excluding former ammunition bunkers and buildings hatched on 'document 2', for a mixed use of agriculture and the storage of motor vehicles and caravans, not including repairs or dismantling	Approved 23 May 2003
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Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

03/01952/OUT	Demolition of emergency ammunition bunkers and storage buildings. Erection of 76 new dwellings plus new internal access road and parking	Refused 24 November 2003 Appeal dismissed
07/01743/OUT	Demolition of the existing buildings and erection of 12 x two storey detached dwelling and 2 x chalet style bungalows	Refused 19 October 2007 Appeal dismissed
16/01013/F	Removal of hardstanding and buildings, erection of 9 detached houses and retained bunker with associated access, parking and landscaping. As amended on 21/10/2016 and as amended on 7/12/2016.	Approved 20 January 2017
19/01811/S73	Removal of hardstanding and buildings, erection of 9 detached houses and retained bunker with associated access, parking and landscaping. Variation to condition 10 of permission 16/01013/f. Amendment to demolition of the bunkers.	Approved 21 November 2019

4.0 Proposal and Design Approach

- 4.1 This is full application for the change of use of land to class c3 (residential), the removal of the existing areas of hardstanding, retention and restoration of bunker 4, the demolition of the remaining structures, and redevelopment to provide ten detached dwellings accessed via an internal circuit road framing a central water body. To include associated works for the purpose of landscaping.
- 4.2 The mix of dwellings would be 3 x 4 bedroom units and 7 x 5 bedroom units. The dwellings would be two storeys and have a modern design 3 x mono-pitched roofs and 7 x asymmetrical pitched roofs. The top floors would be cantilevered and finished in timber cladding. The ground floor elements would be finished in brick. The garages would have sedum roofs. Each property would have two external off street car parking spaces and an integral garage which provides space for 1 car and bike storage.
- 4.3 The layout proposes the 10 units to be positioned in a circular fashion around a central green space, which includes a pond. Bunker 4 located in the north-western part of the site would be reinstated and repaired to ensure its long term retention as well as providing the opportunity for members of the public

to visit the site. 4 parking spaces are provided outside the bunker which provide parking for visitors to the site and the bunker. Two existing air raid shelters are also proposed to be re-located to

4.4 A design and access statement (DAS) should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:
Assessment;
Involvement;
Evaluation; and
Design.

4.5 Evidence of the applicant’s design approach is set out below:

Assessment	The DAS includes a detailed assessment of the site context, analysis of the site including climate, man-made features, access, visibilities, opportunities and constraints. It also include an assessment of the extant permission.
Involvement	No evidence of community consultation is known to have taken place.
Evaluation	The DAS states that the new proposal “draws from the existing conditions and layout of the site; the principles of the extant permission; and the needs of the client to create a scheme that is befitting of the site whilst also being financially viable. The resultant proposal is an example of high quality contemporary architecture which defines a new future for Courtlands Farm whilst embracing the vernacular and heritage of its context.
Design	<p>The DAS sets out the key details of the proposals including masterplan, site layout, house types, typical floor plans and elevations and appearance/materiality. More details is then given regarding house types, sustainability and landscape and ecology.</p> <p>The DAS also including a comparison of the scheme against the site as it existed prior to demolition and against the extant permission.</p> <p>Lastly the DAS provide a number of visualisations to show what the scheme is likely to look like if implemented</p>

4.6 Further details of the development are as follows:

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

Site area	2.85
Existing use	Mixed agricultural, commercial and vehicle storage
Proposed use	Residential (10 x 4+bed dwellings)
Proposed parking spaces	34 (3 spaces per residential unit, 4no visitor)
Parking standard	27 (25 for residential, 2no visitor)
Number of affordable units	0
Net increase in dwellings	10
Proposed site density	4 dph
Density in surrounding area	Park Road (south of site) – 2 dph Park Road/Yewlands Close – 18 dph

5.0 Policy Context

5.1 Designation

Metropolitan Green Belt
Locally Listed Building
Site of Archaeological Importance
Parking accessibility score – 1 (low)

5.2 Reigate and Banstead Core Strategy (CS)

CS1(Sustainable Development)
CS2 (Valued Landscapes and Natural Environment)
CS3 (Green Belt)
CS4 (Valued townscapes and the historic environment)
CS5 (Valued People/Economic Development),
CS10 (Sustainable Development),
CS11 (Sustainable Construction),
CS12 (Infrastructure Delivery),
CS13 (Housing delivery)
CS14 (Housing Needs)
CS17 (Travel Options and accessibility)

5.3 Reigate and Banstead Development Management Plan 2019 (DMP)

DES1 (Design of new development)
DES4 (Housing mix)
DES5 (Delivering high quality homes)
DES8 (Construction Management)
DES9 (Pollution and contamination land)
TAP1 (Access, Parking and Servicing)
CCF1 (Climate Change Mitigation)

CCF2 (Flood Risk)
INF3 (Electronic communication networks)
NHE2 (Protecting and Enhancing Biodiversity)
NHE3 (Protecting trees, woodland areas and natural habitats)
NHE5 (Development within the Green Belt)
NHE9 (Heritage Assets)

5.4 Other Material Considerations

National Planning Policy Framework
2021 (NPPF)

National Planning Practice Guidance
(NPPG)

Supplementary Planning
Guidance/Documents

Surrey Design
Local Character and Distinctiveness
Design Guide SPD 2021
Climate Change and Sustainable
Construction SPD 2021
Vehicle and Cycle Parking
Guidance 2018
Affordable Housing

Other

Human Rights Act 1998
Community Infrastructure Levy
Regulations 2010

6.0 **Assessment**

6.1 The application site comprises a previously developed site within the Green Belt which is identified as a locally listed building and designated as a site of archaeological importance. The site is not adjacent to the existing built up area.

6.2 The main issues to consider are therefore:

- Impact upon the Metropolitan Green Belt
- Effect on the historic interest of the site and design considerations
- Housing mix and standard of accommodation
- Accessibility, parking and traffic implications
- Effects on the amenity of neighbouring properties
- Flooding and surface water drainage matters
- Trees and ecology
- Contamination
- Energy, sustainability and broadband
- Crime
- Community Infrastructure Levy (CIL)

Impact upon the Metropolitan Green Belt

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

- 6.3 Being within the Green Belt, paragraph 149 of the NPPF applies. This allows for limited infilling or the partial or complete redevelopment of previously developed land (brownfield land), whether redundant or in continuing use (excluding temporary buildings), which would not have a greater impact on the openness of the Green Belt than the existing development. The site meets the definition of previously developed land by virtue of the extent of non-agricultural use of several of the structures and the significant areas of hardstanding (as confirmed by the 2003 lawful development certificate). This is the position taken by the Council during its consideration of the extant 2016 permission.
- 6.4 The test is therefore whether the proposal would have a greater impact on the openness of the Green Belt than the existing development. In this regard, the site comprised a number of buildings, some which are substantial in size and scale, the bunker structures and significant areas of hard landscaping. In addition, until recently, the site was lawfully used (granted in 2003 under 03/00700/CLE) for open storage of a significant number of vehicles, a use which in itself, represents an intrusion on the openness of the Green Belt. Overall, the existing buildings and lawful use are considered to have a significant impact on the openness of the Green Belt, albeit it is relatively well screened from view by tree cover.
- 6.5 There is no definitive test by which to consider the openness of the Green Belt. The National Planning Practice Guidance published advice on the assessment of openness in the Green Belt in July 2019. It states that “assessing the impact of a proposal on the openness of the Green Belt, where it is relevant to do so, requires a judgment based on the circumstances of the case. By way of example, the courts have identified a number of matters which may need to be taken into account in making this assessment. These include, but are not limited to:
- openness is capable of having both spatial and visual aspects – in other words, the visual impact of the proposal may be relevant, as could its volume;
 - the duration of the development, and its remediability – taking into account any provisions to return land to its original state or to an equivalent (or improved) state of openness; and
 - the degree of activity likely to be generated, such as traffic generation.”
- 6.6 As set out in the submitted DAS documents when compared to the extant scheme the proposal would be larger on a number of measures including overall volume (1460 cubic m compared to 12293), gross internal area (3008.12 sqm compared to 3514.35), max height (+158.55m (AOD) compared to +158.1) and hardstanding (7846.76 sqm compared to 5131.18). The scheme is however smaller in terms of footprint (2,077sqm compared to 2,319sqm). The proposed scheme is also quite different in approach compared to the extant scheme with a greater spread of buildings across the site but with an increase in landscaping and openness within the centre of the site. It is therefore difficult to compare the two schemes. It must also be remembered that the assessment is not whether the proposed scheme is larger than the extant scheme but whether taking in to account all the factors

the proposal would have a greater impact on the Green Belt than the existing bunker development and collection of agricultural buildings.

- 6.7 A number of factors are considered to be relevant and these are discussed below.
- 6.8 In this instance the applicants have provided an assessment with regard to the existing and proposed buildings in terms of volumetric and footprint calculations, which demonstrates that the removal of existing structures and replacement with the ten detached dwellings results in an overall decrease in volume of buildings (from approximately 16,217cu.m to around 14,630.m) and footprint (from 4141.02m² to 2390m²). Whilst, volumetric and footprint calculations alone do not necessarily translate into an actual reduction in openness; they are informative in the overall consideration of the scheme. It is also important to note that some of the existing buildings will have been associated with agricultural use and therefore fall outside of the definition of previously developed land; however in terms of footprint there would still be a small reduction of 115m² if you exclude the agricultural buildings. In terms of proposed volume, whilst the amount of volume would be greater if the agricultural buildings were excluded, given that these structure form part of the redevelopment of the site these are still buildings which impact on openness and are relevant to the overall consideration.
- 6.9 In addition to a reduction in built structures, the proposed development would also bring about a significant reduction in the extent and spread of hardstanding and hard landscaping on the site with a reduction from 15050.68m² to 7846.76m² – a reduction of 47%. The layout concentrates the built form within the general footprint of the existing bunkers ensuring that the built form would not encroach beyond the parts of the site where there was built form. In addition whilst the spread of buildings would be greater than the extant scheme due to the reduction in hardstanding and the soft landscaped nature of the central part of the site the increase in openness throughout the site adequately counters this greater spread. Analysis by the applicant shows that the areas of the site covered by buildings, hard surface and the like would be reduced from 19,191m² to 10,236m² meaning a reduction from 67% coverage to only 37% coverage, with the equivalent to 0.9 hectares given back over to soft landscaping. Whilst it is acknowledged that the majority of this additional open land would be within the form of sub-divided private rear gardens (which reduces the benefit somewhat), there is nonetheless considered to be a net benefit to the openness of the Green Belt. The proposed boundary treatment proposed for the gardens would be a natural approach with the use of a bund, timber post and wire stock fencing and native hedging which would cause less impact to openness than standard close boarded fencing. It is also considered that the extent of private rear gardens can be appropriately secured by condition to ensure that the gardens do not extend fully up to the site boundary and also controls over the type and extent of boundary treatments.
- 6.10 Furthermore, the redevelopment proposals would secure cessation of the vehicle storage activities which have previously been lawfully carried out on

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

the site. The proposed residential use is considered to be an improvement over the vehicle storage which, both in terms of the intensity of activity/movements and the visual impact, is considered to be more harmful to the openness and purposes of the Green Belt.

- 6.11 From a visual impact point of view whilst the maximum height of the buildings would be higher than the extant and existing buildings the proposal has limited the impact by lowering the ground levels within the site ensuring that the building would only be marginally higher (+158.55 mAOD compared to existing +157.77mAOD and extant +158.1mAOD) and proposing a modern design with wooden cladding at first floor. The site will also continue to be well screened around its boundaries by the existing trees and a number of replacement trees. The result as demonstrated by the submitted Landscape and Visual Evidence and Appraisal (LVEA) report is that in the long term the visual impact would be neutral to minor beneficial compared to the existing built form.
- 6.12 The conclusions reached by the Inspector determining the 2007 appeal in respect of the Green Belt are noted and a consideration in this case. However, the situation now both in terms of the nature of development now proposed and changes in national policy since that decision, are such that it is not unreasonable to reach a different conclusion in this instance.
- 6.13 Overall, the proposal would bring about a reduction in the overall footprint, volume and number of buildings and structures present on the site. The scheme would also significantly reduce the extent of hardstanding, with large areas given back over to soft landscaping, albeit as part of sub-divided private curtilages. Development will also bring an end to the vehicle storage operations on the site which in themselves are considered harmful to the Green Belt. It is therefore considered that, on balance, in accordance with paragraph 149 of the Framework, the development of this brownfield site would not have a greater impact on the openness of the Green Belt than the existing development. Conditions are recommended to secure further details of the extent of the proposed garden areas, the extent and design of the proposed boundary treatments and also landscaping to ensure that the visual impact of the proposal are as expected and to limit the impact on openness of the proposed garden areas.
- 6.14 For these reasons, the development would not be inappropriate development and would therefore accord with Policy NHE5 of the Development Management Plan 2019, Core Strategy Policy CS3 and the NPPF. Given the specifics of the case where on balance the case is not considered to have a greater impact on the openness of the Green Belt than the existing development subject to the site being well contained and the residential curtilages and associated boundary treatments being sensitively laid out it is considered that it is necessary and reasonable in this case to retain control over future extensions and outbuildings to ensure that the development continues to have an acceptable impact on the openness of the green belt and visual amenities of the site and surrounding area.

Effect on the historic interest of the site and design considerations

- 6.15 Courtlands Farm is the former Banstead Anti-Aircraft Ammunition Depot of 1938. The depot is Locally Listed and Site of Archaeological Importance, with land surrounding the site also a Site of High Archaeological Potential.
- 6.16 The loss of all but 1 of the original eight Second World War ammunition store bunkers and some associated paraphernalia were considered in detail during the consideration of the 2016 application as well as the potential archaeological impacts. Under the 2016 application the loss of all but one of the bunkers was approved subject to the reinstatement of the bunker and an undertaking to enable public access to the bunker and the relocation of two air raid shelters. Following the granted of the 2016 permission demolition works were carried out which have removed all but the 1 retained bunker. The current application also proposes to retain the bunker in line with the agreed details under the 2016 application.
- 6.17 The Council's Conservation Officer has comments as follows: "The locally listed bunker is the sole surviving bunker of the former Banstead Anti Aircraft Ammunition Depot of 1938 and the proposal is to restore this. It is the only surviving bunker of its type in the London area, and would have been used in the Battle of Britain, the Blitz and against the Flying Bombs, covering supply for both South London and briefly the South East. Conditions 11 (retention) 12 (restoration) 13 (management) and 14 (air raid guardhouses) of 16/01013 should be carried forward to the present application. The information provided in the application is satisfactory for determination purposes though is now out of date and further details will be required by condition." Therefore subject to the reimposition of the conditions from the 2016 permission the impact on the locally listed heritage asset is considered to be acceptable.
- 6.18 In terms of archaeology this was considered under the 2016 application and as part of the already carried out demolition works the then applicant carried out archaeological recording (report by Archaeology South East). As this has already been carried out and submitted with this application the County Archaeologist has confirmed that there are no additional archaeological recording or investigations required under this current application.
- 6.19 The proposal is therefore, subject to the recommended conditions, considered to comply with the requirements of NHE9 and the NPPF.
- 6.20 In terms of design and impact on the wider landscape character of the area the site is accessed by a long sweeping access road and well screened by landscaping and trees on the boundary such that it does not have a strong physical or visual relationship with development in the surrounding area. Given this, there is no immediate context for the development to follow in terms of architectural and stylistic conventions, form or grain of development. There is therefore scope for the site to derive its own character, albeit, there within the parameters of the requirement to reflect and reinforce local distinctiveness in accordance with local and national policies.

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

- 6.21 The buildings would be arranged so that the 10 dwellings would be located around a central looping road. The houses would face on to the looping road with their rear gardens extending out towards the perimeter of the site. Inside the looping road would be a soft landscaped area with a pond. The frontages of the houses would include good sized areas of soft landscaping. The proposed boundary treatment is proposed to be a bund, with a timber and post and wire stock fence with native hedging and would measure 2m in height. The main access loop will be constructed to meet highways standards to enable access by refuse and maintenance vehicles, this will be dressed in a buff/golden rolled stone to provide a more rural aesthetic. The driveways themselves will use a permeable build up with a similar buff/golden rolled stone finish.
- 6.22 The design of the proposed dwellings would be contemporary in nature, with either low pitched asymmetrical roofs or mono-pitched roofs and modern detailing. The first floor would be timber clad while the ground floor would have facing brickwork. The use of this roof form assists in keeping the volume and bulk of the new buildings down and combined with the softer timber cladding finish enable the buildings to fit more sensitively within the site and woodland setting than a traditional build development. It should be noted that the design approach is one that has been found acceptable on the recently approved applications for Alvis House and the former Courtlands Farm site. In terms of light spill whilst the dwellings would include large areas of glazing however all of the large windows have large areas of overhang or are well recessed which limits the light spill.
- 6.23 The variation in roof designs and the spaced out nature of the buildings also helps to break up the bulk and massing of the scheme and limits its visual impact from wider viewpoints. As does the fact that the new dwellings are to be erected at a lower ground level. To further evidence the acceptability of the proposals in terms of a wider landscape impact the applicant has submitted a Landscape and Visual Evidence and Appraisal (LVEA). This considers the impact of the proposed higher dwellings and their design (including light spill) on the wider area taking in to account the topography of the surrounding area and the proposed tree works and replacement landscaping. The report demonstrates that whilst trees are being removed the impact on longer views and wider landscape is acceptable and the proposed replacement planting will ensure that, in the longer term, there is a neutral to minor beneficial impact. The report concludes that "It is not considered that the proposed development of the former munitions depot at Courtlands Farm will be out of character within the surrounding landscape or that it will differ significantly from the existing situation. The site is well contained by mature vegetation, both on the site's boundaries and within the surrounding landscape, and this, along with the size, scale, appearance and design of the proposed buildings, will limit any possible adverse landscape and visual effects of the development. It is considered that the proposals are compliant with policy by conserving the character, special qualities, distinctiveness, biodiversity and ecosystem service functions of the site and its surroundings."

- 6.24 I have considered the applicant's submission and concur with its conclusions. As noted above the site is previously developed land with a good degree of tree cover and higher ground level to the site's boundaries which will help to contain the proposed development. This in combination with the sensitively design dwellings and layout and modest quantum of development will result in an appropriate impact on the character of the site and wider landscape.
- 6.25 The comments of the Conservation Officer are noted where he has raised concerns regarding the larger scale and top heavy cantilevered design of the dwellings, light spill, the reduced amount of proposed landscaping and the suburbanising impact this would have on the area and local distinctiveness. However for the reasons set out above the scheme is considered to meet the policies of the DMP and would result in high quality development with a distinctive, high character which would be appropriate to the landscape and historic setting of the site and in my view a better designed scheme than the extant 2016 scheme.
- 6.26 As set out above conditions are recommended to secure further details of the extent of the proposed garden areas, the extent and design of the proposed boundary treatments and also landscaping to ensure that the visual impact of the proposal are as expected and to limit the impact on openness of the proposed garden areas. A condition is also recommended to secure further details of any proposed external lighting for the same reason.

Housing Mix and Standard of Accommodation

- 6.27 The submitted plans shows a total of 10 properties with a mix of 3 x 4 bed units and 7 x 5 bed market dwellings.
- 6.28 Policy DES4 relates to Housing Mix and states that all new residential developments should provide homes of an appropriate type, size and tenure to meet the needs of the local community. The proposed housing mix must on sites of up to 20 homes, at least 20% of market housing should be provided as smaller (one and two bedroom) homes. In this case, the proposal provides no smaller units. Whilst this is not in line with policy given the location of the site, in a rural setting well away from public facilities and in an area which is characterised by large, detached dwellings it is considered that a scheme with smaller dwellings is likely to have low market demand and would be at odds with the character of the area. It is also important to note that the extant scheme allowed permission for 9+ bedroom properties. As such a scheme with no smaller homes is considered to be acceptable in this case.
- 6.29 Policy DES5 requires that all new residential development must provide high quality adaptable accommodation and provide good living conditions for future occupants. New accommodation must meet the relevant nationally prescribed internal space standard for each individual unit unless the council considers that an exception should be made. Sufficient space must be included for storage, clothes drying and the provision of waste and recycling bins in the home. Adequate outdoor amenity space including balconies and terraces and /or communal outdoor space should be provided.

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

- 6.30 Each dwelling would have a floor area which is well in excess of the relevant standard in the Nationally Described Space Standards. All habitable rooms would have outlook either looking to the rear or front of their plots, with a number having secondary side facing windows, ensuring adequate outlook for the occupants. As such the units would provide good levels of sunlight and daylight to the main habitable rooms. The dwellings would all have ground floor patio areas and rear terrace areas. The proposed garden areas would be of an adequate size. The proposal would also have shared outdoor amenity space with the central landscaped area.
- 6.31 The terrace areas and outlook from any first floor windows is well located to prevent an unacceptable relationship between the proposed dwellings ensuring that the future occupants would not be unacceptable overlooked. The dwellings are also well spaced out to prevent overbearing impacts or loss of light.
- 6.32 The proposals is therefore considered to provide a good quality level of amenity for the future occupants.

Neighbour amenity

- 6.33 DMP Policy DES1 also requires new development to provide an appropriate environment for future occupants whilst not adversely impacting upon the amenity of occupants of existing nearby buildings, including by way of overbearing, obtrusiveness, overshadowing, overlooking and loss of privacy.
- 6.34 As discussed above, the site is well screened by landscaping and for the most part is physically divorced from neighbouring properties. The closest properties to the development are Alvis House, The Former Courtlands Farm and Courtlands Farm Bungalow to the west of the site which are served by the same shared access drive as the development site.
- 6.35 The Former Courtlands Farm and Alvis House are closest to the site. However, both would be located well away from the proposed dwellings. In the case of The Former Courtlands Farm site at the closest point, the eastern elevation of the replacement dwelling recently approved there (under ref. 21/02432/F) would be over 15m to the side garden boundary with the new house and a further 6m away from the side elevation more than 35m at the closest point from the new dwelling and the relationship would be side to side. There would be side facing windows but these would be a significant distance to the Former Courtlands Farm boundary and would not overlook key amenity areas.
- 6.36 Alvis House would be located over 20 metres away from the closest point of the nearest proposed dwelling and the relationship would almost be side to side. In addition there would be significant existing and new boundary landscaping between it and the new dwellings which would further reinforce the separation. The first floor balcony terrace area is designed with side facing solid walls preventing direct outlook towards Alvis House.

- 6.37 Courtlands Bungalow would be a considerable distance from the nearest of the proposed units such that the amenity of its occupants would not be materially altered. The dwelling is also significantly set back from the access road such that there would not be an unacceptable noise and disturbance impact arising from the vehicle movements to the new dwellings. At any rate, it is notable that the site was previously used for vehicle storage (a use which could lawfully be resumed), which would have generated significantly more vehicular movements.
- 6.38 All other neighbours along the Park Road frontage are considered to be sufficiently distant from the new units such that they would not experience any discernible change in amenity.
- 6.39 In terms of impact during construction the proposal would undoubtedly cause some disruption to the three nearby properties however the site is more than capable of containing all construction parking and activities within it and environmental health regulations exist if noise nuisance and other pollution issues became a problem during construction.
- 6.40 On this basis, the proposal would not have an unacceptable impact upon existing neighbouring properties and would accord with the provisions of DMP Policy DES1 and DES8.

Accessibility, parking and traffic implications

- 6.41 Policy TAP1 of the Development Management Plan 2019 requires new development to demonstrate that it would not adversely affect highways safety or the free flow of traffic, that it would provide sufficient off-street parking in accordance with published standards and that it would constitute development in a sustainable location
- 6.42 The development would be accessed by the existing private driveway/access road which serves the Courtlands Farm complex, with no changes proposed to the present arrangements other than a slight change to the access road alignment as it approaches the site. Each dwelling would be served by two off-street parking spaces and would have a garage providing further opportunity for parking for at least 1 further car. The net result is a scheme which provides 5 parking spaces more for the proposed dwellings than the required minimum standards. The scheme would also include 4 visitor parking spaces, 2 above the minimum requirement. This would also act as visitor parking for the retained bunker when open to the public. Given the nature of the heritage feature the amount of visitor parking is considered appropriate. In addition each plot proposes ample external hardstanding which could easily accommodate more than the allocated 2 parking spaces. The level of parking provision is therefore considered to be in line with Policy TAP1 and more than adequate for its location.
- 6.43 The submission includes tracking which shows that a refuse truck of the size that the Council uses would be able to enter the site and manoeuvre/navigate

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

within the site safely and efficiently to carry out bin collections. Neighbourhood services has confirmed they are happy with the proposal subject to the access road being to highway standards, which the applicant has confirmed it will. Therefore overall, the access and parking provision is considered to be appropriate.

- 6.44 The County Highway Authority has assessed the application and has raised no objection on highway safety/capacity grounds. However, they have advised of concerns regarding the accessibility of the proposal by modes other than private car. This matter therefore warrants further discussion.
- 6.45 The County Highway Authority response draws attention to the fact that the site is remote from key services and is not easily accessible by modes of transport other than the private car, with the nearest bus stop some distance from the site and no desirable or continuous cycleways/footpaths to the site. Reference is also made to a previously dismissed appeal on the site (for 14 units – 07/01743/OUT), in which the Inspector stated gave *“significant weight to the harm through housing in an unsustainable location”*.
- 6.46 These observations, and the previous Inspectors views, are undoubtedly material considerations in this case. Notwithstanding the CHA views, it is notable that the site is within 1km of Banstead Town Centre and as such future occupants would be an acceptable distance from a wide range of retail provision, services (health, dentist), community facilities and primary school provision. It is however accepted that, given the situation of the site, there would be a reliance on private car to access the town. Compared to the scheme before the Inspector at the previous appeal, the number of units has been reduced and as such the harm arising from this reliance on private car would be lessened by virtue of the fact that there would be fewer new residents on the site.
- 6.47 Both the Framework and local policy – chiefly Core Strategy Policy CS17 – seek to maximise accessibility to services and the use of sustainable travel modes. As the CHA note there are three dimensions to sustainable development - economic, social and environmental - hence the sustainability of the site should not be assessed purely in terms of transport mode and distance. On balance, whilst there would be some conflict with the accessibility requirements, given the scale of the development and the fact it is making use of vacant previously developed site, which will provide construction jobs and helps steer development away from greenfield land and will ensure the retention of a locally listed heritage asset and the historic uses of the site, which created more vehicle movements, it is not considered to be so harmful to the overall strategy for promoting and enabling sustainable development in the borough.

Flooding and Surface Water Drainage matters

- 6.48 The site according is located within Flood Zone 1 and is therefore at the least risk of fluvial flooding and does not require a site specific flood risk assessment or any further mitigation measures.

- 6.49 In terms of surface water flooding the additional drainage information submitted by the applicant has been considered by Surrey County Council as the Lead Local Flood Authority (LLFA). The LLFA, following the submission of additional information has concluded that it meets the requirements of national technical standards. They therefore raise no objection subject to a condition securing finalised details of the drainage strategy and implementation/verification.

Trees and Ecology

- 6.50 The existing tree stock provides screening and must continue to do so if the site is to be developed. With regard to the impact on trees the Council's Tree Officer has provided the following comments:

"My comments are based on a desk top assessment of the arboricultural report by Barrell Tree Consultancy 21075-AAA-CA. The report identifies the trees to be removed, the majority being low quality but three moderate quality specimens are to be removed. Despite the removal mature trees, there is still a diverse selection of trees that are to be retained. The report has identified replacement trees to be planted which overtime will contribute to the local canopy cover. The buildings are located far enough away from the retained trees allowing them to grow without being exposed to post development pressures, such as undertaking excessive works, or complete removal.

Based on the proposed layout I raise no objections subject to the following conditions being attached [Full tree protection details and detailed landscaping proposals]."

- 6.51 As set out above the Landscape and Visual Evidence and Appraisal (LVEA) demonstrates that whilst trees are being removed the impact on longer views and wider landscape is acceptable and the proposed replacement planting will ensure that, in the longer term, there is a neutral to minor beneficial impact. Based on the comments of the Tree Officer and the findings of the LVEA it is considered that the impact on trees is acceptable. It is noted that there are not full landscape details submitted with the application and as such a condition is recommended to secure further finalised details. It is considered that further planting should be encouraged along the site boundaries, particularly the southern and western boundaries where there are gaps in the existing trees. It is considered that this can be further explored during the assessment of the future landscaping scheme.
- 6.52 In terms of ecology the applicant has submitted a Preliminary Ecological Appraisal, a Bat Emergence Report and Ecological Impact Assessment. Surrey Wildlife Trust (SWT) has assessed the submitted information and has advised that the reports are sufficient to support this application. SWT advise that the methodology and conclusions of the reports are sound and indicate that subject to appropriate mitigation measures through a Construction Environmental Management Plan there should be no impact to protected habitats or species. The SWT advise that the CEMP should include precautionary measures for bat protection such as lighting management.

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

- 6.53 In terms of biodiversity net gain the Development Management Plan requires applications to provide it where practical. In this case the site is a previously developed site with limited ecological value. Therefore officers are of the view that net gain is a practical and achievable requirement. SWT has recommended a condition to secure a Landscape and Ecological Management Plan (LEMP) to secure further details of how a net gain in biodiversity will be achieved.
- 6.54 Therefore, subject to conditions to secure the recommended mitigation measures and enhancement measures the proposal is considered to be acceptable with regarding to ecology impacts and biodiversity.

Contamination

- 6.55 The Council's Environmental Protection Officer has reviewed the submitted documents and has identified the potential for ground contamination to the present on and/or in close proximity to the site due to the former use as farm land and as an ordnance store during the Second World War. As such a number of conditions are recommended to secure further investigation and if necessary mitigation. Subject to these conditions the proposal would be acceptable in relation to contamination.

Energy, Sustainability and Broadband

- 6.56 DMP Policy CCF1 relates to climate change mitigation and requires new development to meet the national water efficiency standard of 110litres/person/day and to achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations.
- 6.57 The submission includes a commitment in the DAS to highly sustainable dwellings which will include passive design, a fabric first approach, non-fossil fuel heating systems and total water usage of not more than 110 litres per person per day. This sets out a general approach to the scheme but does not refer specifically to the policy requirements of CCF1.
- 6.58 In the event that planning permission is to be granted, a condition could be imposed to seek updated information to ensure the scheme complies with the required standards and its implementation prior to the first occupation of development. Car charging points are also recommended to be secured by planning condition to provide appropriate facilities for electric cars. In this regard, there would be no conflict with DMP Policy CCF1.
- 6.59 A condition is also recommended to ensure that each dwelling is fitted with access to fast broadband services in accordance with policy INF3 of the DMP.

Crime

- 6.60 Policy DES1 requires that: “Creates a safe environment, incorporating measures to reduce opportunities for crime and maximising opportunities for natural surveillance of public places. Developments should incorporate measures and principles recommended by Secured by Design.”
- 6.61 Surrey Police recommend a condition in relation to Secure by Design to secure further details of security measures to be taken.

Community Infrastructure Levy (CIL) and request contributions

- 6.62 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, road, public transport and community facilities which are needed to support new development. This development would be CIL liable and the exact amount would be determined and collected after the grant of any planning permission.
- 6.63 Community Infrastructure Levy (CIL) Regulations were introduced in April 2010 which states that it is unlawful to take a planning obligation into account unless its requirements are (i) relevant to planning; (ii) necessary to make the proposed development acceptable in planning terms; and (iii) directly related to the proposed development.
- 6.64 As such only contributions that are directly required as a consequence of development can be requested and such requests must be fully justified with evidence including costed spending plans to demonstrate what the money requested would be spent on. No such contributions have been requested in this case.

CONDITIONS

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan Type	Reference	Version	Date Received
Elevation Plan	303449-SWH-XX-XX-DRC-0550-P02		25.03.2022
Location Plan	P001		29.12.2021
Site Layout Plan	P002		29.12.2021
Section Plan	P003		29.12.2021
Floor Plan	P004		29.12.2021
Section Plan	P005		29.12.2021
Floor Plan	P006		29.12.2021
Elevation Plan	P007		29.12.2021
Site Layout Plan	P102		29.12.2021
Site Layout Plan	P103		29.12.2021
Other Plan	P104		29.12.2021

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

Other Plan	P105	29.12.2021
Other Plan	P106	29.12.2021
Section Plan	P107	29.12.2021
Proposed Plans	P108	29.12.2021
Floor Plan	P120	29.12.2021
Floor Plan	P121	29.12.2021
Roof Plan	P122	29.12.2021
Floor Plan	P123	29.12.2021
Floor Plan	P124	29.12.2021
Roof Plan	P125	29.12.2021
Floor Plan	P126	29.12.2021
Floor Plan	P127	29.12.2021
Roof Plan	P128	29.12.2021
Elevation Plan	P130	29.12.2021
Elevation Plan	P131	29.12.2021
Elevation Plan	P132	29.12.2021
Elevation Plan	P133	29.12.2021
Elevation Plan	P134	29.12.2021
Elevation Plan	P135	29.12.2021
Arb / Tree Protection Plan	UNNUMBERED	29.12.2021

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

3. No development shall take place until the developer obtains the Local Planning Authority's written approval of finalised details of the proposed ground levels, including any changes to the garden areas, and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

Reason: To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with the existing site and wider landscape, to safeguard the visual amenities of the locality and protect the openness of the Green Belt with regard to Reigate and Banstead Development Management Plan 2019 Policy DES1 NHE9 and NHE5 and requirements of the NPPF.

4. No development shall take place until a plan indicating the residential curtilages of the dwelling houses and the finalised positions, design, materials and type of boundary treatment to be erected has been submitted to and approved in writing by the Local Planning Authority. The boundary treatment

shall be completed before the occupation of the development hereby permitted and no residential or associated domestic uses shall take place outside the residential curtilages agreed.

Reason: To preserve the visual amenity of the area and the openness of the Green Belt with regard to the Reigate and Banstead Borough Reigate and Banstead Development Management Plan 2019 Policy DES1 NHE9 and NHE5 and requirements of the NPPF.

5. No development shall commence including demolition and or groundworks preparation until a detailed, scaled finalised Tree Protection Plan (TPP) and the related finalized Arboricultural Method Statement (AMS) is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas of trees (RPA) shown to scale on the TPP, including the installation of service routings, type of surfacing for the entrance drive and location of site offices. The AMS shall also include a pre commencement meeting, supervisory regime for their implementation & monitoring with an agreed reporting process to the LPA. All works shall be carried out in strict accordance with these details when approved.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3, DES1 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard 5837:2012 Trees in relation to design, demolition and construction.

6. No development shall commence on site until a scheme for the hard and soft landscaping of the site including the retention of existing landscape features has been submitted to and approved in writing by the LPA. Landscaping schemes shall include details of hard and soft landscaping, including any tree removal/retention, planting plans, written specifications (including specialised urban planting pits, cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation and management programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to occupation or within the first planting season following completion of the development hereby approved or in accordance with a programme agreed in writing with the local planning authority.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

of planting shall be replaced within the next planting season by trees, shrubs of the same size and species.

Reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3 and DES1 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard 5837:2012 Trees in relation to design, demolition and construction.

7. The developer must either submit evidence that the building was built post 2000 or provide an intrusive pre-demolition and refurbishment asbestos survey in accordance with HSG264 supported by an appropriate mitigation scheme to control risks to future occupiers. The scheme must be written by a suitably qualified person and shall be submitted to the LPA and must be approved prior to commencement to the development. The scheme as submitted shall identify potential sources of asbestos contamination and detail removal or mitigation appropriate for the proposed end use. Detailed working methods are not required but the scheme of mitigation shall be independently verified to the satisfaction of the LPA prior to occupation. The development shall then be undertaken in accordance with the approved details.

Reason: To ensure that a satisfactory strategy is put in place for addressing contaminated land before development commences and to make the land suitable for the development without resulting in risk to construction workers, future users of the land, occupiers of nearby land and the environment with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 and the NPPF.

8. Prior to commencement of development a written comprehensive environmental desktop study report is required to identify and evaluate possible on and off site sources, pathways and receptors of contamination and enable the presentation of all plausible pollutant linkages in a preliminary conceptual site model. The study shall include relevant regulatory consultations such as with the Contaminated Land Officer and be submitted to the Local Planning Authority and is subject to the approval in writing of the Local Planning Authority and any additional requirements that it may specify. The report shall be prepared in accordance with the Environment Agency's Land Contamination: Risk Management Guidance (2020) and British Standard BS 10175.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

9. Prior to commencement of development, in follow-up to the environmental desktop study, a contaminated land site investigation proposal, detailing the extent and methodologies of sampling, analyses and proposed assessment

criteria required to enable the characterisation of the plausible pollutant linkages identified in the preliminary conceptual model, shall be submitted to the Local Planning Authority. This is subject to the written approval in writing of the Local Planning Authority, and any additional requirements that it may specify, prior to any site investigation being commenced on site. Following approval, the Local Planning Authority shall be given a minimum of two weeks written notice of the commencement of site investigation works. Please note this means a proposal is required to be submitted and approved prior to actually undertaking a Site Investigation.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

10. Prior to commencement of the development, a contaminated land site investigation and risk assessment, undertaken in accordance with the site investigation proposal as approved that determines the extent and nature of contamination on site and is reported in accordance with the standards of DEFRA's and the Environment Agency's Land Contamination: Risk Management Guidance (2020) and British Standard BS 10175, shall be submitted to the Local Planning Authority and is subject to the approval in writing of the Local Planning Authority and any additional requirements that it may specify. If applicable, ground gas risk assessments should be completed in line with CIRIA C665 guidance.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

11.
 - a. Prior to commencement of the development a detailed remediation method statement should be produced that details the extent and method(s) by which the site is to be remediated, to ensure that unacceptable risks are not posed to identified receptors at the site and details of the information to be included in a validation report, has been submitted to and approved in writing by the Local Planning Authority, and any additional requirements that it may specify, prior to the remediation being commenced on site. The Local Planning Authority shall then be given a minimum of two weeks written notice of the commencement of remediation works.
 - b. Prior to occupation, a remediation validation report for the site shall be submitted to the Local Planning Authority in writing. The report shall detail evidence of the remediation, the effectiveness of the remediation carried out and the results of post remediation works, in accordance with the approved remediation method statement and any addenda thereto, so as to enable future interested parties, including regulators, to have a single record of the remediation undertaken at the site. Should specific ground gas mitigation measures be required to be incorporated into a development the testing and

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

verification of such systems should have regard to CIRIA C735 guidance document entitled 'Good practice on the testing and verification of protection systems for buildings against hazardous ground gases' and British Standard BS 8285 Code of practice for the design of protective measures for methane and carbon dioxide ground gases for new buildings.

Reason: To demonstrate remedial works are appropriate and demonstrate the effectiveness of remediation works so that the proposed development will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

12. Unexpected ground contamination: Contamination not previously identified by the site investigation, but subsequently found to be present at the site shall be reported to the Local Planning Authority as soon as is practicable. If deemed necessary development shall cease on site until an addendum to the remediation method statement, detailing how the unsuspected contamination is to be dealt with, has been submitted in writing to the Local Planning Authority. The remediation method statement is subject to the written approval of the Local Planning Authority and any additional requirements that it may specify.

Note: Should no further contamination be identified then a brief comment to this effect shall be required to discharge this condition.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

13. No development shall commence until a Construction Environment Management Plan (CEMP) has been submitted to and approved in writing by the Local Planning Authority (LPA). The CEMP shall include, but not be limited to:

- a) Map showing the location of all of the ecological features
- b) Risk assessment of the potentially damaging construction activities
- c) Practical measures to avoid and reduce impacts during construction
- d) Location and timing of works to avoid harm to biodiversity features
- e) Responsible persons and lines of communication
- f) Use of protected fences, exclusion barriers and warning signs.
- g) Reptile and amphibian precautionary approach
- h) Pre-commencement bat external and internal inspection of buildings and bunker 4
- i) Preliminary ground level tree roost assessment prior to tree felling
- j) Pre-commencement badger walkover
- k) Hedgehog precautionary approach.

The development shall only be carried out in accordance with the agreed mitigation measures.

Reason: To ensure that any potential impact to protected species is adequately mitigated in accordance with the provisions of the National Planning Policy Framework and policy NHE2 of the Development Management Plan 2019.

14. No development shall commence until a Landscape and Ecological Management Plan (LEMP) has been submitted to and approved in writing by the Local Planning Authority (LPA). The LEMP should be based on the proposed impact avoidance, mitigation and enhancement measures specified in the submitted ecology reports and shall include, but not be limited to following:
- a) Description and evaluation of features to be managed
 - b) Ecological trends and constraints on site that might influence management
 - c) Aims and objectives of management
 - d) Appropriate management options for achieving aims and objectives
 - e) Prescriptions for management actions, together with a plan of management compartments
 - f) Preparation of a work schedule (including an annual work plan capable of being rolled forward over a five-year period)
 - g) Details of the body or organisation responsible for implementation of the plan
 - h) Ongoing monitoring and remedial measures
 - i) Legal and funding mechanisms by which the long-term implementation of the plan will be secured by the applicant with the management body(ies) responsible for its delivery.
 - j) Monitoring strategy, including details of how contingencies and/or remedial action will be identified, agreed and implemented so that the development still delivers the fully functioning biodiversity objectives of the originally approved scheme.
 - k) Sensitive lighting strategy
 - l) Biodiversity Net Gain Strategy

The agreed details shall be implemented before occupation of this development, unless otherwise stated in the agreed details or subsequently agreed in writing by the LPA, and maintained/monitored in accordance with the agreed details.

Reason: To provide enhancements to the biodiversity of the site in accordance with the provisions of the National Planning Policy Framework and Reigate and Banstead Development Management Plan 2019 policy NHE2.

15. The development hereby permitted shall not commence until details of the design of a surface water drainage scheme have been submitted to and approved in writing by the planning authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDS, NPPF and Ministerial Statement on SuDS. The required drainage details shall include:

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

- a) The results of infiltration testing completed in accordance with BRE Digest: 365 and confirmation of groundwater levels.
- b) Evidence that the proposed final solution will effectively manage the 1 in 30 & 1 in 100 (+ 40% allowance for climate change) storm events and 10% allowance for urban creep, during all stages of the development. The final solution should follow the principles set out in the approved drainage strategy. If infiltration is deemed unfeasible, associated discharge rates and storage volumes shall be provided using a maximum discharge rate equivalent to the pre-development Greenfield run-off.
- c) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels, and long and cross sections of each element including details of any flow restrictions and maintenance/risk reducing features (silt traps, inspection chambers etc.). Confirmation is required of a 1, unsaturated zone from the base of any proposed soakaway to the seasonal high groundwater level and confirmation of half-drain times.
- d) A plan showing exceedance flows (i.e. during rainfall greater than design events or during blockage) and how property on and off site will be protected from increased flood risk.
- e) Details of drainage management responsibilities and maintenance regimes or the drainage system.
- f) Details of how the drainage system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the drainage system is operational.

Reason: To ensure the design meets the national Non-Statutory Technical Standards for SuDS and the final drainage design does not increase flood risk on or off site in accordance with policy CCF2 of the Reigate and Banstead Borough Council Development Management Plan 2019.

16. Prior to the first occupation of the development, a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority. This must demonstrate that the surface water drainage system has been constructed as per the agreed scheme (or detail any minor variations), provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/areas, flow restriction devices and outfalls), and confirm any defects have been rectified.

Reason: To ensure the Drainage System is constructed to the National Non-Statutory Technical Standards for SuD and the implemented drainage design does not increase flood risk on or off site in accordance with policy CCF2 of the Reigate and Banstead Borough Council Development Management Plan 2019.

17. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) (or any order revoking and re-enacting that Order with or without modification), no demolition of the retained bunker, identified as building no. 4 on the approved drawing number: 916D P002, permitted by Class B of Part 11 of the Second

Schedule of the 2015 Order shall be undertaken without the prior written approval of the Local Planning Authority.

Reason: In the interests of safeguarding the identified historic fabric of the retained bunker with regard to Reigate and Banstead Borough Development Management Plan policy NHE9.

18. Before works commence a full specification and drawings for the restoration and repair of the retained bunker shall be submitted to and approved in writing by the LPA including reinstatement of the grass banks to the bunker, additional supporting walls required for this grass bank, a grille over the void between bunker and embankment, a metal roof covering to the main roof, treatment of the carbonation and any other repairs for the stabilising and full restoration of this bunker. All the repairs in the approved specification shall be carried out before any dwellings are occupied.

Reason: To preserve the historic interest of the site with regard to Reigate and Banstead Borough Development Management Plan policy NHE9.

19. Before works commence a full management plan shall be submitted to and approved in writing by the LPA and the retained bunker shall be retained, maintained and managed in accordance with this management plan unless otherwise agreed in writing by the LPA.

Reason: To preserve the historic interest of the site with Reigate and Banstead Borough Development Management Plan policy NHE9.

20. The two one person metal air raid shelters, also known as guard posts, shall be retained and relocated adjacent to the retained bunkers before works commence. Details of how these shelters shall be protected during their removal and details of their proposed new location shall be submitted to and approved in writing by the LPA before works commence.

Reason: To preserve the historic interest of the site with regard to Reigate and Banstead Borough Development Management Plan policy NHE9.

21. No development shall take place above slab level until written details of the materials to be used in the construction of the external surfaces of the buildings, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Development Management Plan 2019 policy DES1, NHE5 and NHE9.

22. The bin stores shown on the approved plans, unless otherwise agreed in writing, shall be erected and made ready for use (i.e. bins installed) prior to the first occupation of the development.

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

Reason: To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

23. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking and turning areas shall be retained and maintained for their designated purposes. This includes the garages which shall be retained to allow the parking for at least 1 car.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Access, Parking, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

24. The development hereby approved shall not be first occupied unless and until the cycle stores have been fitted in accordance with the approved plans for at least 2 bicycles to be stored within the garage of each dwelling. Thereafter the cycle stores shall be retained and maintained for its designated purpose.

Reason: In order that the development promotes more sustainable forms of transport, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17.

25. The development hereby approved shall not be occupied unless and until each of the proposed dwellings are provided with a fast charge socket (current minimum requirements - 7 kw Mode 3 with Type 2 connector - 230v AC 32 Amp single phase dedicated supply) in accordance with a scheme to be submitted and approved in writing by the Local Planning Authority and thereafter retained and maintained to the satisfaction of the Local Planning Authority.

Reason: In order that the development promotes more sustainable forms of transport, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17.

26. No external lighting shall be installed on the buildings hereby approved or within the site until an external lighting scheme, which shall include indication of the location, height, direction, angle and cowling of lights, and the strength of illumination, accompanied by a light coverage diagram, has been submitted to and agreed in writing by the local planning authority.

The external lighting shall be implemented in accordance with the approved scheme and be retained thereafter and maintained in accordance with the manufacturer's instructions.

Reason: To protect the visual amenity of the area and neighbouring residential amenities and protect biodiversity with regard to Reigate and Banstead Core Strategy 2014 Policy CS10 and policy DES1, DES5, DES9 and NHE2 of the Reigate and Banstead Development Management Plan 2019.

27. The development shall not be occupied until a scheme demonstrating compliance with the principles of 'Secured by Design' has been submitted to and approved in writing by the Local Planning Authority. The approved details shall be completed before the occupation of the development hereby permitted and shall be permanently maintained as such thereafter.

Reason: To ensure that the development provides a secure environment for future residents in accordance with Policy DES1 of the Reigate & Banstead Development Management Plan 2019.

28. The development hereby approved shall not be first occupied unless and until an Energy and Water Efficiency Statement has been submitted to and approved in writing by the Local Planning Authority. The Statement shall detail how the development will:
- a) Ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day
 - b) Achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations

The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

29. All units within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:
- a) A broadband connection accessed directly from the nearest exchange or cabinet
 - b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

30. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (as amended) (or any order revoking and re-enacting that Order with or without modification), no extensions permitted by Classes A, B or E of Part 1 of the Second Schedule of the 2015 Order shall be constructed (other than those expressly authorised by this permission).

Reason: To enable the LPA to retain control over the enlargement of dwellings or new outbuildings in this rural area and Green Belt Location and taking in to account the specific facts of the case where the site is previously developed land with regard Reigate and Banstead Borough Development Management Plan policy NHE5 and the requirements of the NPPF.

31. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (as amended) (or any order revoking and re-enacting that Order with or without modification), no gates, fences walls or other structures under Class A of Part 2 of the Second Schedule of the 2015 Order shall be constructed forward of the proposed houses, between them and the access road, other than those allowed by this permission.

Reason: To ensure the openness of the development internally is maintained in the interests of the visual and residential amenities of the locality with regard Reigate and Banstead Borough Development Management Plan policy NHE5 and the requirements of the NPPF.

INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.org.uk.
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : [Climate Change Information](#).
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, to contact the Council's Neighbourhood Services team to confirm the number and specification of recycling and refuse bins that are required to be supplied by the developer. All developer enquires on recycling and refuse bin ordering, collections and discussing waste matters is via our department email address RC@reigate-banstead.gov.uk . Please also note our website area for developers https://www.reigate-banstead.gov.uk/info/20062/recycling_and_refuse/392/fees_for_recycling_and_refuse_services/3.

4. The applicant is advised that the Borough Council is the street naming and numbering authority and you will need to apply for addresses. This can be done by contacting the Address and Gazetteer Officer prior to construction commencing. You will need to complete the relevant application form and upload supporting documents such as site and floor layout plans in order that official street naming and numbering can be allocated as appropriate. If no application is received the Council has the authority to allocate an address. This also applies to replacement dwellings. If you are building a scheme of more than 5 units please also supply a CAD file (back saved to 2010) of the development based on OS Grid References. Full details of how to apply for addresses can be found http://www.reigatebanstead.gov.uk/info/20277/street_naming_and_numbering
5. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
6. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
7. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to: <http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.
8. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
 - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
 - (e) There should be no burning on site;

Agenda Item 11

Planning Committee
6th July 2022

Agenda Item: 11
21/03311/F

(f) Only minimal security lighting should be used outside the hours stated above; and

(g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - www.ccscheme.org.uk/index.php/site-registration.

9. Environmental Health would like to draw the applicant attention to the specifics of the contaminated land conditional wording such as 'prior to commencement', 'prior to occupation' and 'provide a minimum of two weeks notice'. The submission of information not in accordance with the specifics of the planning conditional wording can lead to delays in discharging conditions, potentially result in conditions being unable to be discharged or even enforcement action should the required level of evidence/information be unable to be supplied. All relevant information should be formally submitted to the Local Planning Authority and not direct to Environmental Health.
10. The applicant site is situated on land that may have historically comprised military land. As a result there is the potential for a degree of soil contamination to be present beneath part(s) of the site. In addition there is the potential for the presence of Unexploded Ordnance (UXO) to be present beneath part(s) of the site. Groundworkers should be made aware of the above so suitable mitigation measures and personal protective equipment measures (if required) are put in place and used. Should significant ground contamination be identified or suspect/actual UXO identified the Local Planning Authority should be contacted promptly for further guidance and in relation to UXO the Local Police should also be contacted. CIRIA C681: UXO a Guide to the Construction Industry (Guidance Document) can provide further information on UXO matters relating to construction.
11. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above arboricultural tree and landscaping conditions. All works shall comply with the recommendations and guidelines contained within British Standard 5837. The planting of trees and shrubs shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate substantial sized trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Extra Heavy Standard size with initial planting heights of not less than 4m with girth measurements at 1m above ground level in excess of 14/16cm.
12. Neighbourhood Services - Please note that individual bin sets currently include:

- a. 140L green refuse bin (possible upgraded to 2x140L, as detailed on council website)
- b. 140L black/grey mixed recycling bin (possible upgrade to 240L, as detailed on council website)
- c. 55L paper box (sometimes with accompanying cardboard to the side)
- d. 23L food recycling caddy
- e. 240L garden waste bin (subscription service, as detailed on council website)

Note that the council requires 3-6 weeks' notice when ordering bin sets, please see the following link https://www.reigate-banstead.gov.uk/info/20062/recycling_and_refuse/392/fees_for_recycling_and_refuse_services/3

The construction of the driveway access must be to Surrey highway standard for the council vehicles to access, so that at least a 26t vehicle as shown below can access the site and leave the site safely, and without damage to property or vehicle. In particular note the height and width (with mirrors) of the collection vehicle in relation to adjacent obstacles or vegetation ie trees, poles etc. Please confirm the specifications will meet with these requirements.

The plans indicate there are no gates provided on the driveway. If there are plans for gates, then they should be automatic or trade button operated Monday to Saturday 6am to 4pm. The council will not accept a fob or key entry system, though the council does have a code entry requirement. Please advise accordingly.

REASON FOR PERMISSION

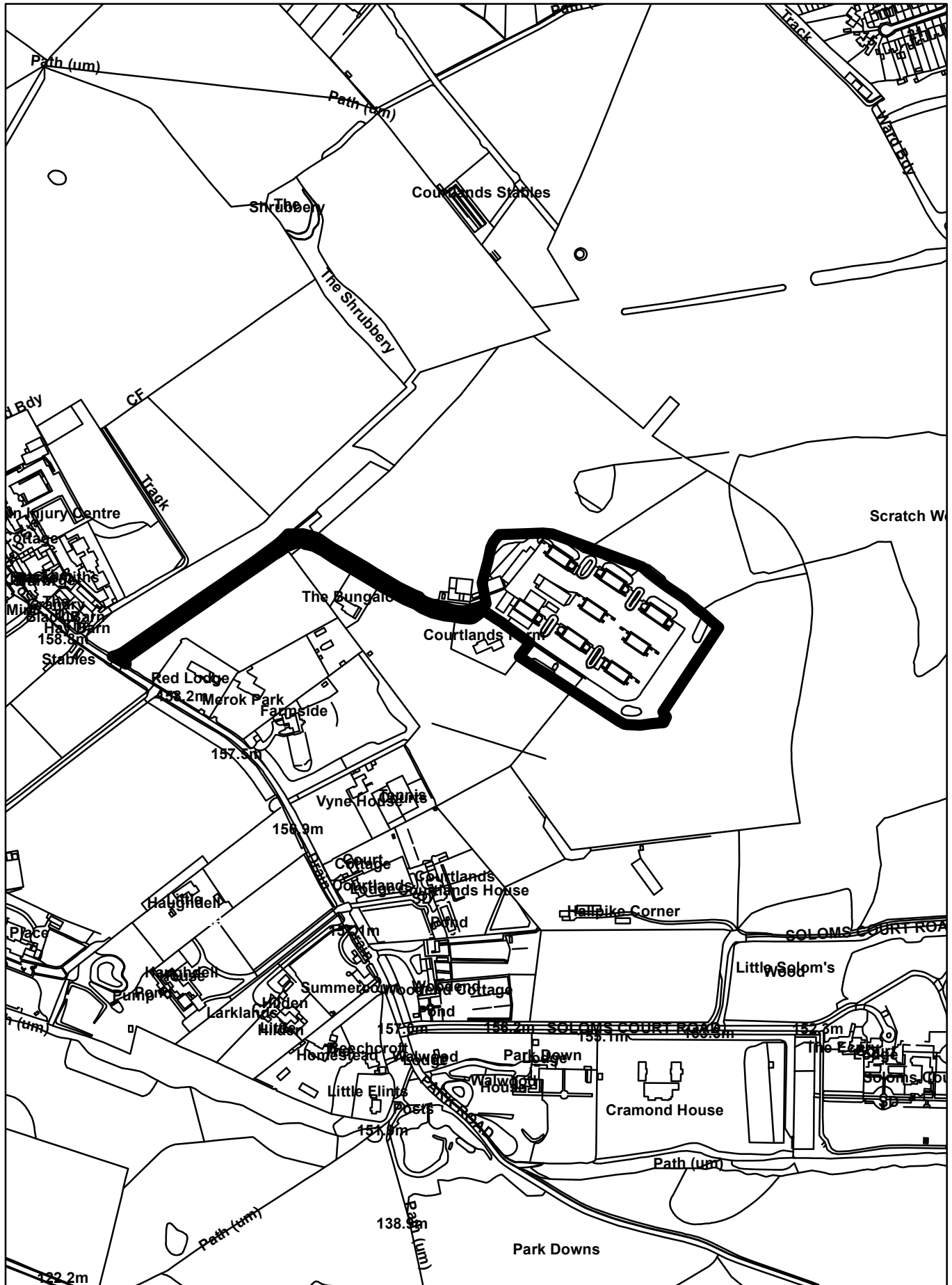
The development hereby permitted has been assessed against the relevant development plan policies set out in the report and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

Proactive and Positive Statements

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

Agenda Item 11

21/03311/F - Alvis House, Park Road, Banstead



PROPOSED MASTERPLAN				
HOUSE UNITS				
UNIT	TYPE	GEA	GIA	FOOTPRINT
1	B	348	301	195
2	A	376	330	202
3	B	348	301	195
4	A	376	330	202
5	A+	415	366	228
6	A+	415	366	228
7	A+	415	366	228
8	A	376	330	202
9	A	376	330	202
10	B	348	301	195
TOTAL		3793	3317.0	2077.0

TOTAL AREAS (M2)	
DEVELOPABLE AREA	28550.66 (7.05AD)
TOTAL FOOTPRINT AND HARDSTANDING	10236.86
PERCENTAGE OF HARDSTANDING	35.86

- HARD LANDSCAPING HIGHWAYS STANDARD
- PERMEABLE HARD LANDSCAPING

KEY PLAN

EXTRACT A



LEGEND

AP	Anchor Post	LB	Letter Box
AP	AP Wall	LP	Lamp Post
BS	Boundary	LPS	Lamp Post Short
B	Bollard	M	Mortice
BT	Biton Section Cover	MB	Manhole
BSF	Biton Man Cover	MBF	Manhole Man Cover
CBV	Cable Television Cover	MP	Open Wood Fence
CTV	Cable Television Point	PM	Parking Meter
CF	Close Board Fence	PF	Post and Rail Fence
CF	Chimble Fence	PF	Post and Rail Fence
CPS	Concrete paving slab	RF	Road Sign
DF	Down Pipe	SC	Step Cook
DPC	Damp Proof Course	SK	Skate Valve
DK	Drop Kerb	TS	Telephone Call Box
ED	Electricity Cover	TR	Tree Trunk
EP	Electricity Pole	UP	Up and Over Sign
FL	Finished Floor Level	UL	Up Light
FR	Fire Risert	US	Up to 10m
FP	Front Post	UP	Up Post
GP	Gate Post	UV	Up Valve
GU	Gully	WW	Water Meter
GS	Gas Service Box	WW	Water Meter
KS	Man Object		

NOTE: All heights are relative to Ordnance Datum and are based on GPS observations.
 NOTE: Whilst no physical access is possible to all areas and levels, the information is given in good faith. Any critical dimensions should be checked on site. Dimensions are not necessarily legal boundaries.
 Tree heights are by visual estimation and are not measured.

REPLACED GATES AND GATE POSTS TO LANDSCAPE ARCHITECTS DETAILS.

EXTRACT A

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Courlands Farm - Phase Two

Proposed Masterplan

Drawing No.	916 D
Revision	P103



LEGEND

AB Anchor Post	LB Letter Box
AV Air Valve	LP Lamp Post
BN Bench	LPS Lamp Post Short
BW Bin	MK Manhole
BE British Telecom cover	MS Service Manhole
BWF Barbed Wire Fence	OB Open Board Fence
CC Concrete Curbing	PF Post and Rail Fence
CD Chain Link Fence	PFH Post and Rail Fence
CF Chain Link Fence	PI Top of Pavement
CG Chain Link Fence	RF Foot and Rail Fence
CS Concrete paving slabs	RE Redding Eye
CK Column	RS Road Sign
CP Chimney	SC Step Sock
CS Concrete paving slabs	SD Drain Valve
DE Drop Kerb	T2B Telephone Call Box
EC Electricity Cover	TM Traffic Piling
ED Electricity Pole	TR Telephone Pole
EL Finished Floor Level	TS Traffic Signal
FE Fire Hydrant	TT Top of Mid Height
FF Fence Post	UL Utility to 10'
GF Gas Post	UV Utility Valve
GU Gully	VM Valve Manhole
GV Gas Service Valve	WW Wire Mesh Fence
HP High Point	

NOTE: Survey is relative to Ordnance Datum and is based on GPS observations.

NOTE: 1. Note: Whilst no physical access is possible to off-site and roof details, the information is given in good faith. Any critical dimensions should be checked after site clearance. Physical features such as fences are not necessarily legal boundaries.

NOTE: 2. The survey should be checked by an expert if identification is unclear.

NOTE: 3. The heights are by visual estimation and are not measured.

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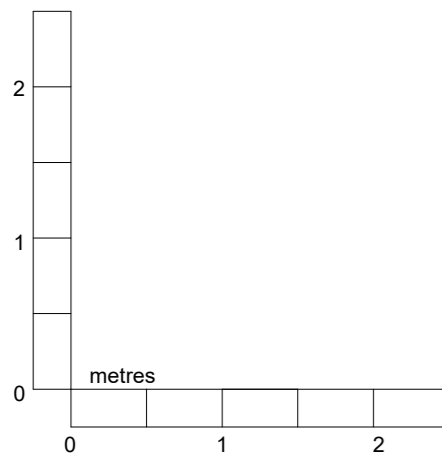
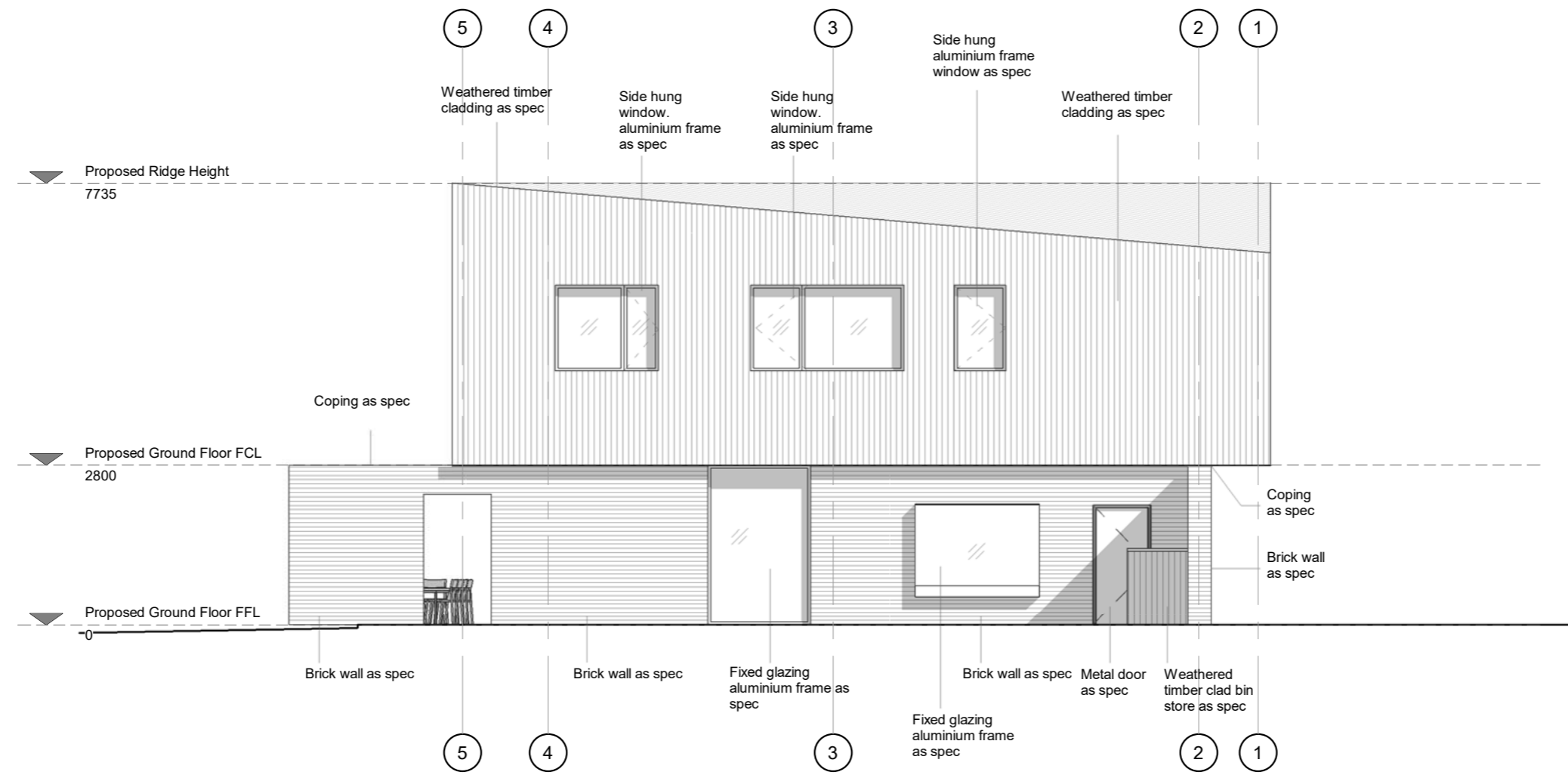
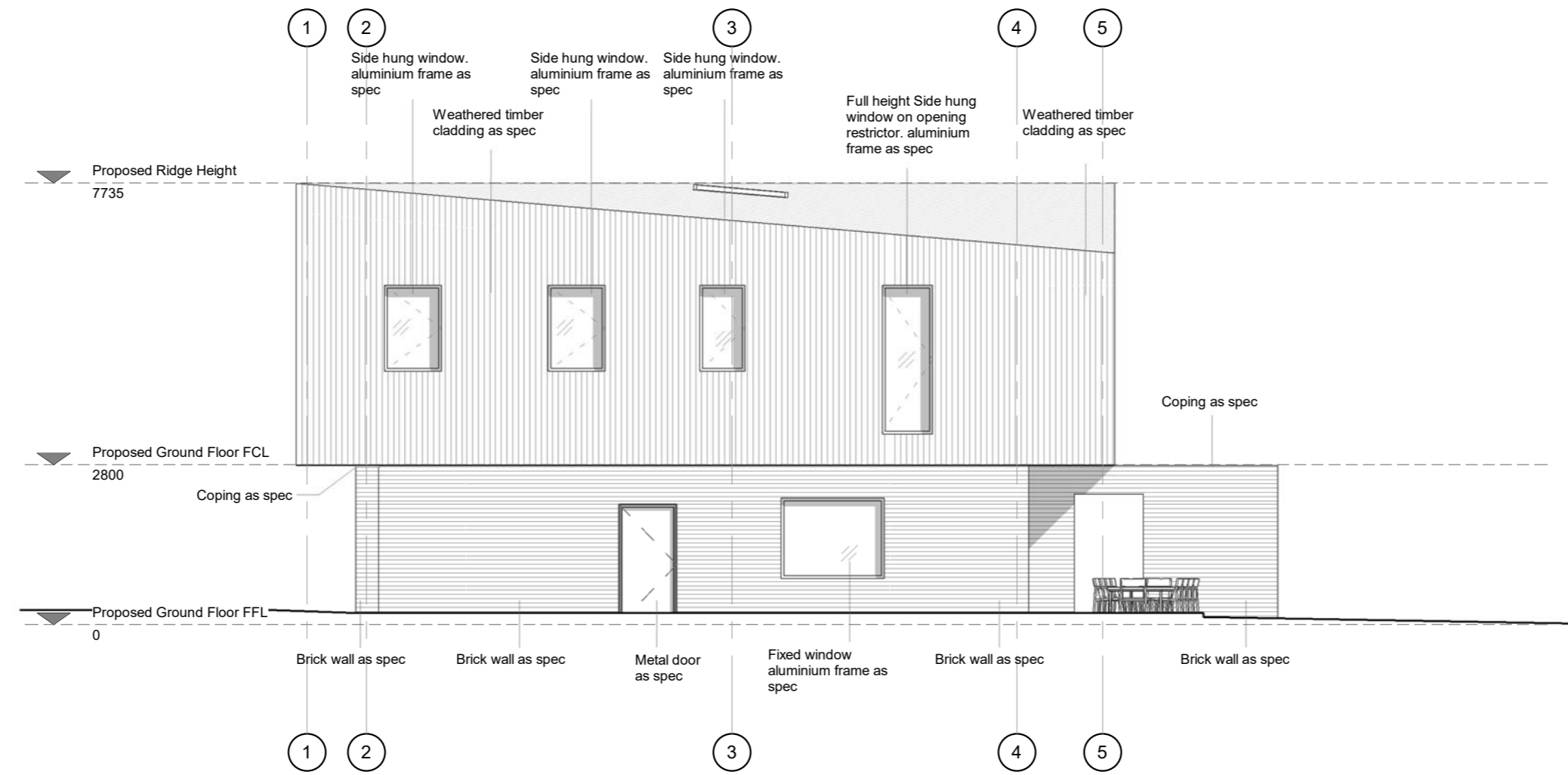
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Project: Courlands Farm - Phase Two

Drawing: Proposed Site Plan

Rev	Description	Date	Scale	Author
CC	CC	JAK	Sep 21	1:500

916 D P102



Notes:

These drawings relate to Houses 2, 4, 8 and 9.

Houses have identical layout and form, but heights are dependant on site levels.

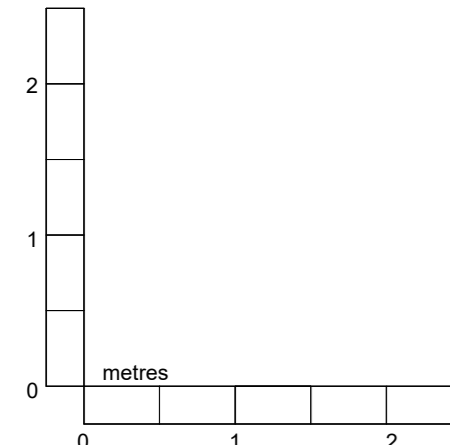
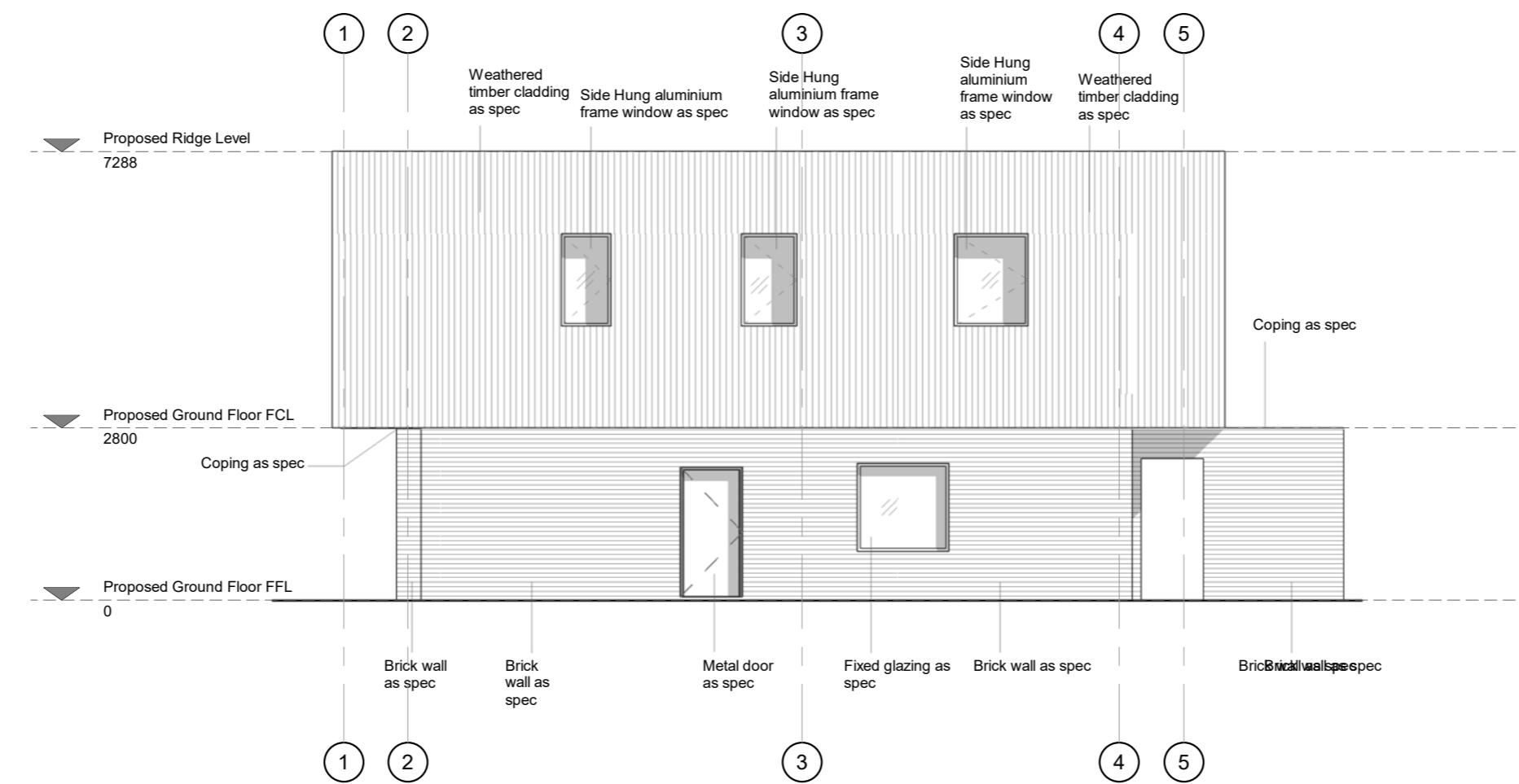
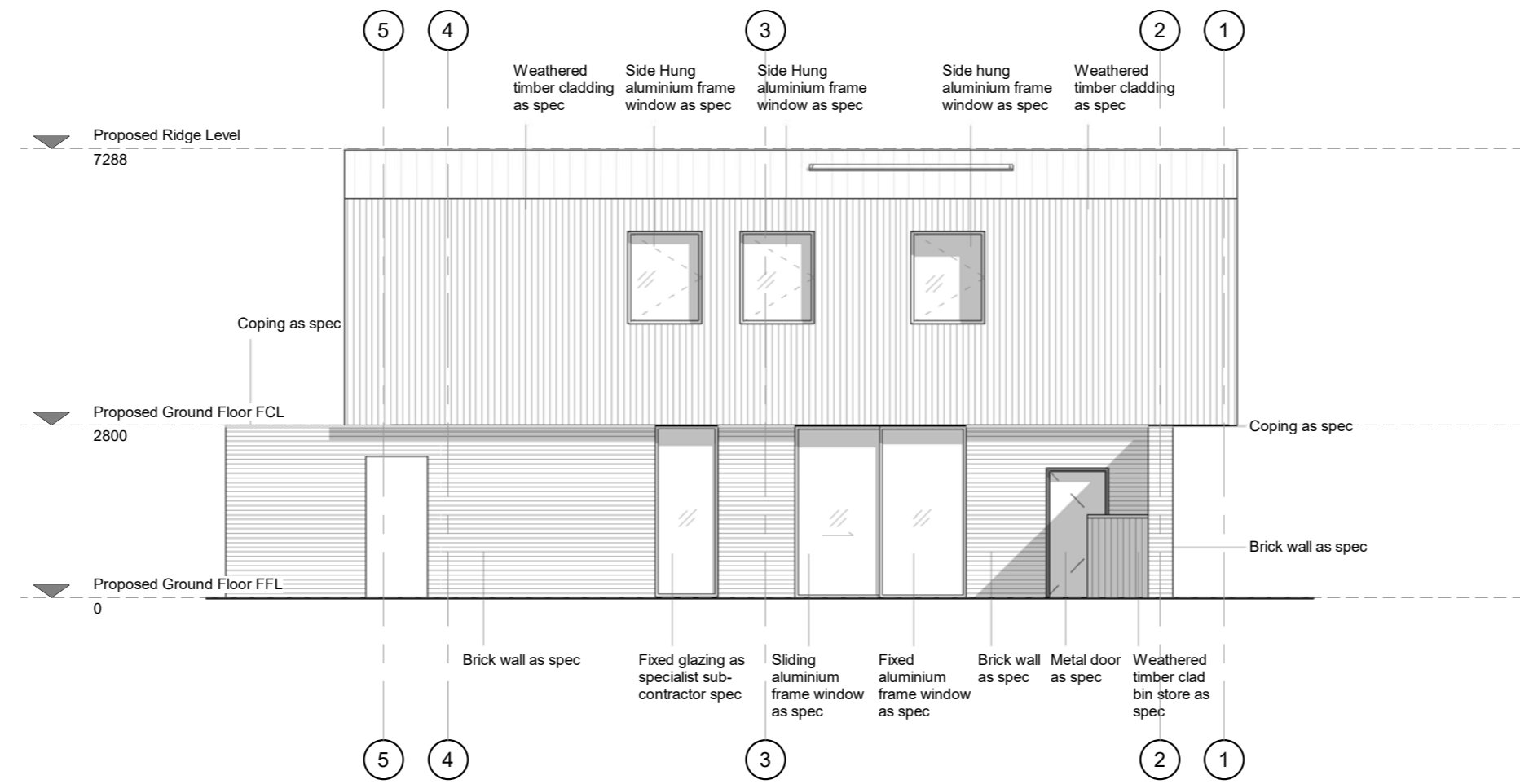
For ground floor levels and ridge heights please refer to masterplan drawing.

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PROJECT TITLE: Courtlands - Type A			
DRAWING TITLE: Side Elevations			
STATUS: Planning			
DRAWN BY: CC	CHECKED BY: JMO	DATE: Nov 21	SCALE A2: 1 : 100
JOB NO: 916D	DRAWING NO: P131	REVISION: -	

PLANNING ONLY



Notes:

These drawings relate to Houses 1, 3 and 10
Houses have identical layout and form, but heights are dependant on site levels.

For ground floor levels and ridge heights please refer to masterplan drawing.

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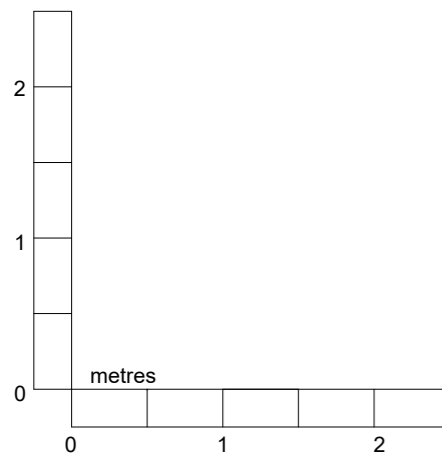
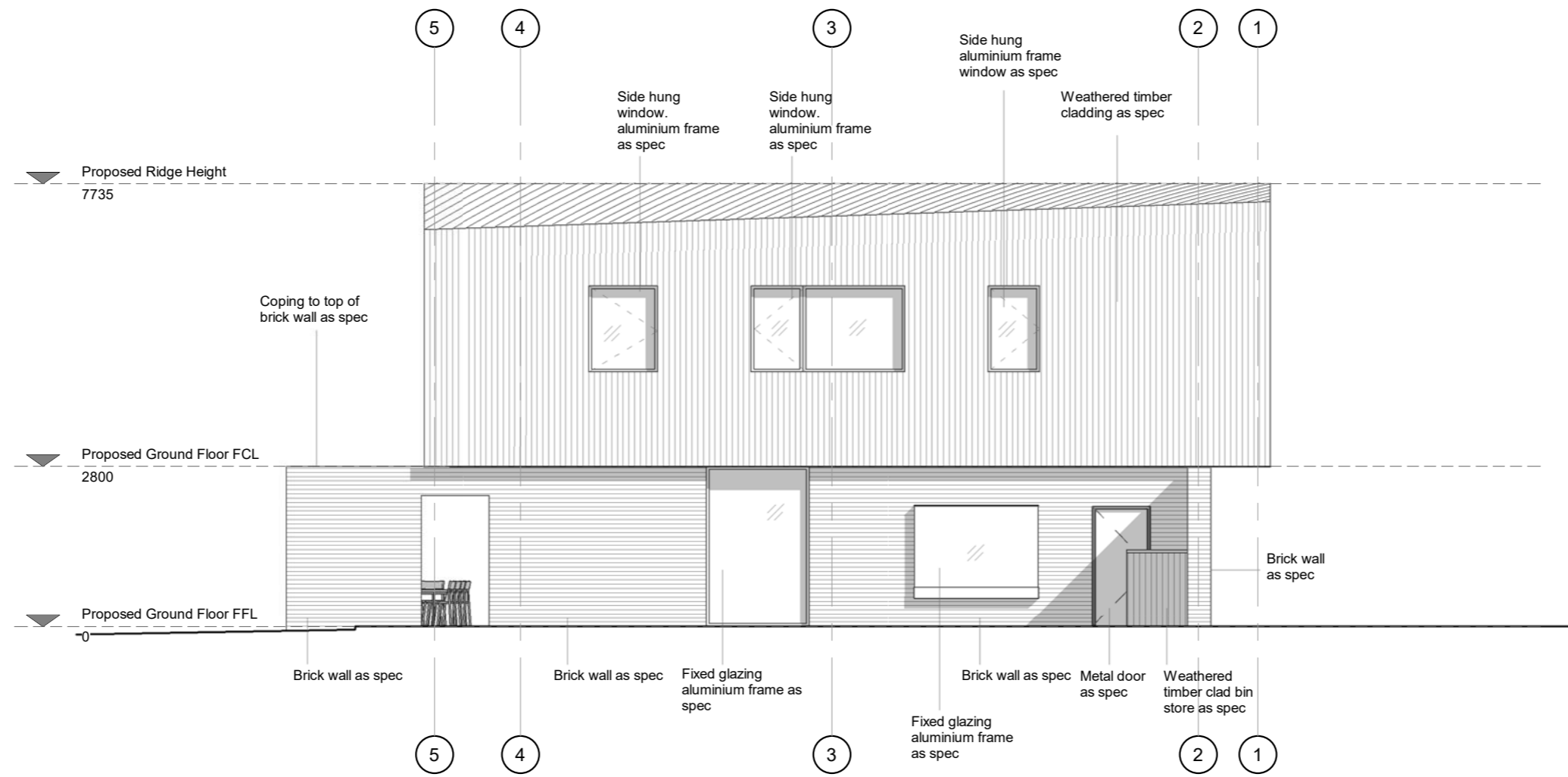
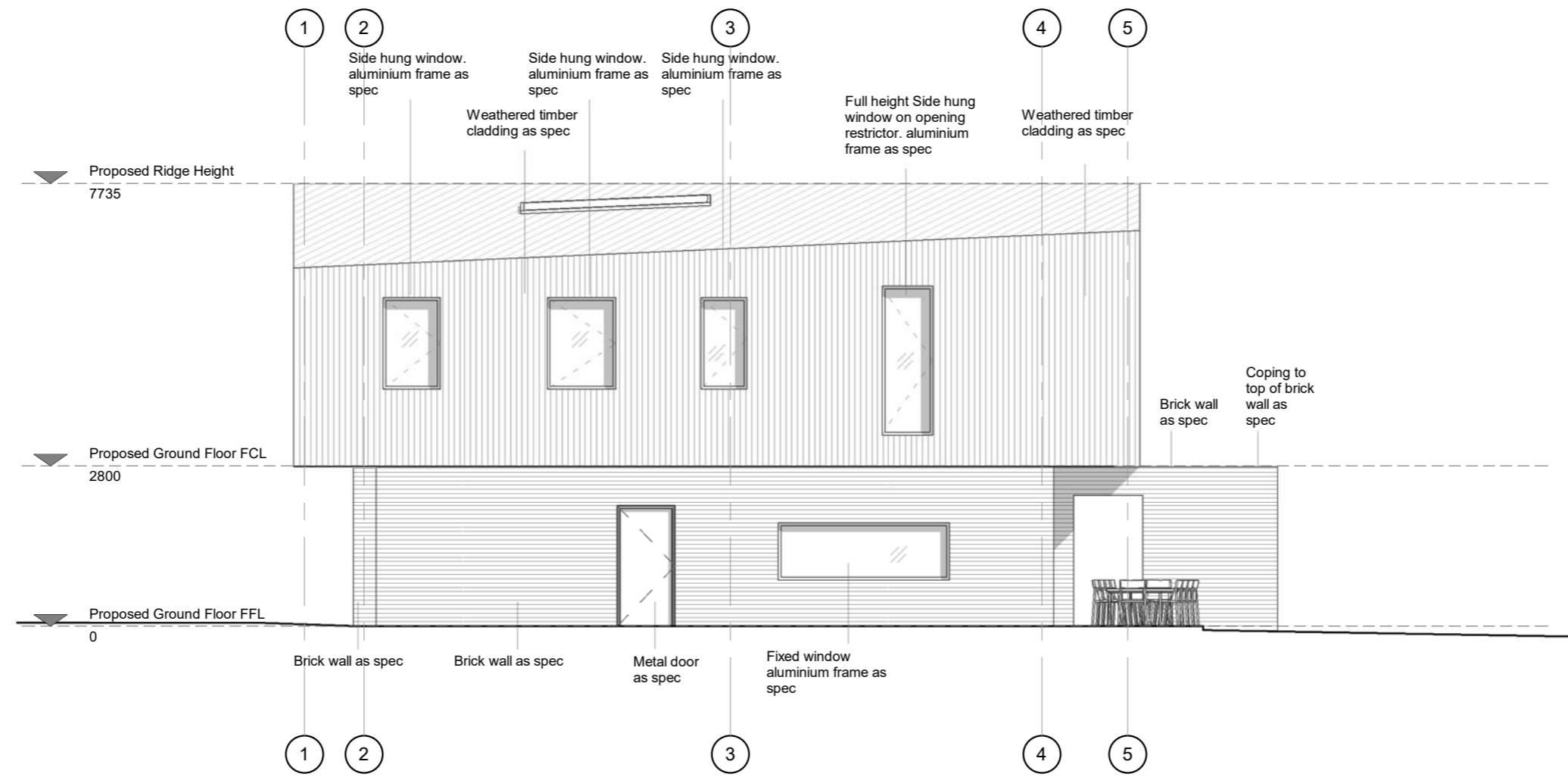
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Courtlands - House Type B

DRAWING TITLE:
Proposed Side Elevations

STATUS:
Planning

DRAWN BY: CC	CHECKED BY: JMO	DATE: Nov 21	SCALE A2: 1 : 100
JOB NO: 916D	DRAWING NO: P133	REVISION: -	

PLANNING ONLY



Notes:

These drawings relate to Houses 5, 6 and 7.

Houses have identical layout and form, but heights are dependant on site levels.

For ground floor levels and ridge heights please refer to masterplan drawing.

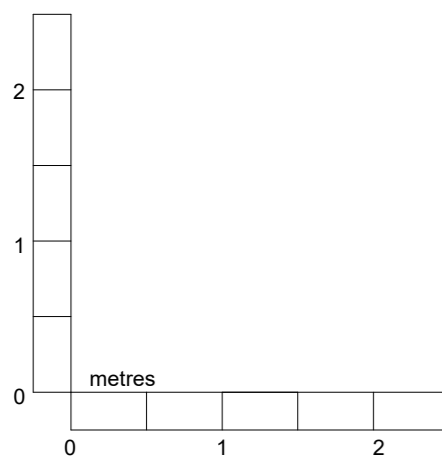
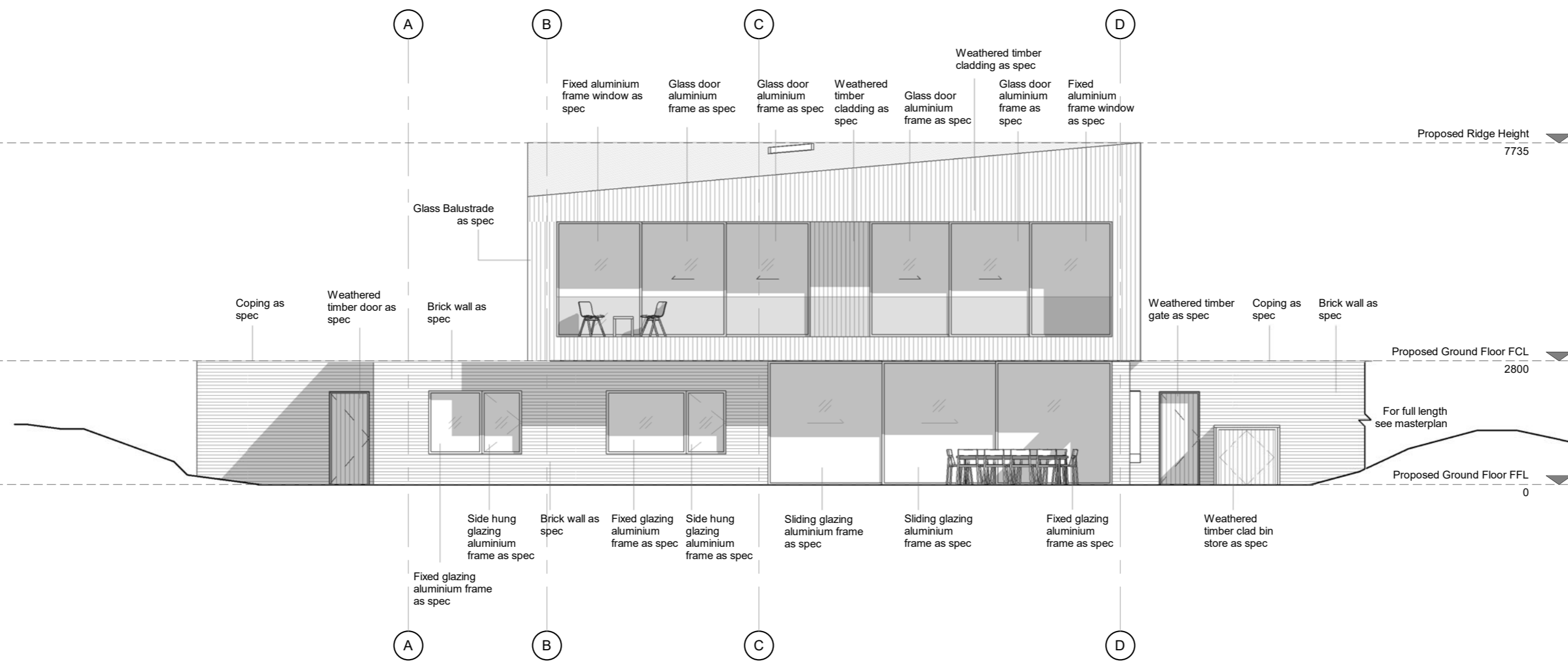
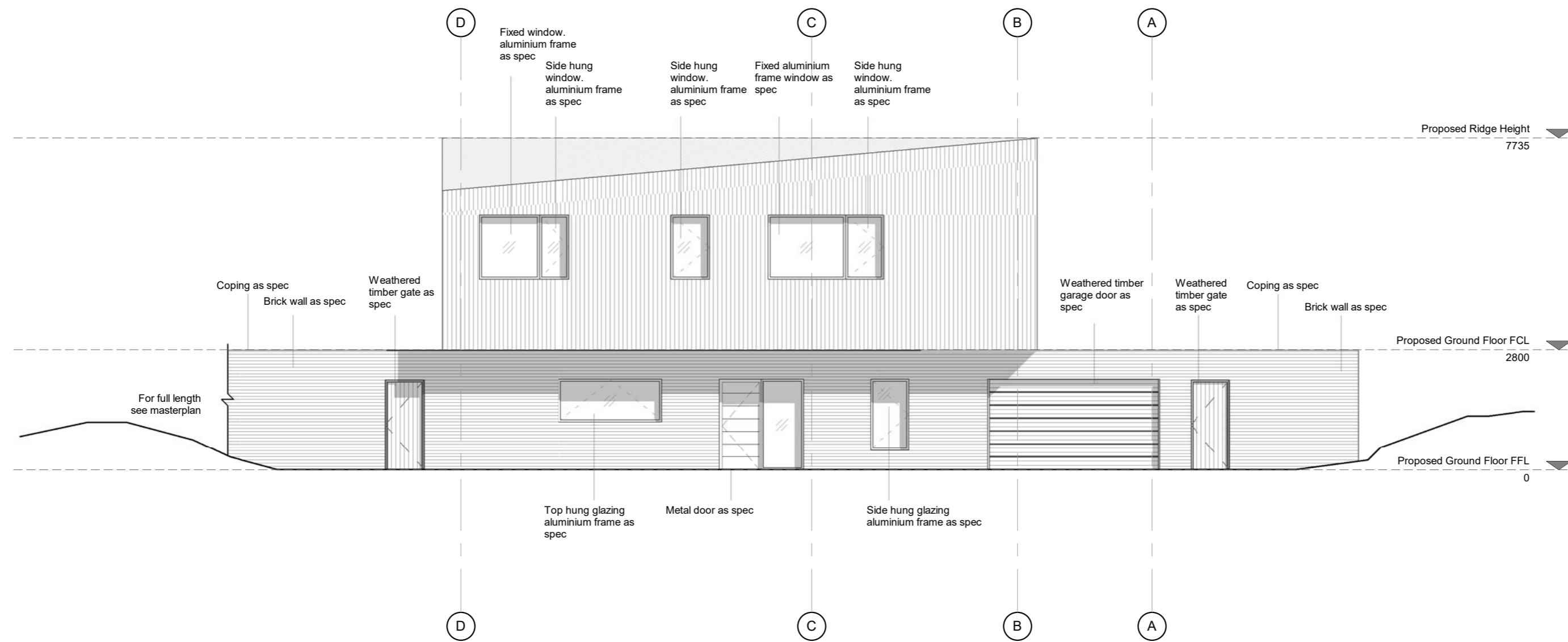
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PROJECT TITLE: Courtlands - Type A+		
DRAWING TITLE: Side Elevations		
STATUS: Planning		
DRAWN BY: CC	CHECKED BY: JMO	DATE: Nov 21
JOB NO: 916D	DRAWING NO: P135	SCALE A2: 1 : 100
		REVISION: -

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Notes:

These drawings relate to Houses 2, 4, 8 and 9.

Houses have identical layout and form, but heights are dependant on site levels.

For ground floor levels and ridge heights please refer to masterplan drawing.

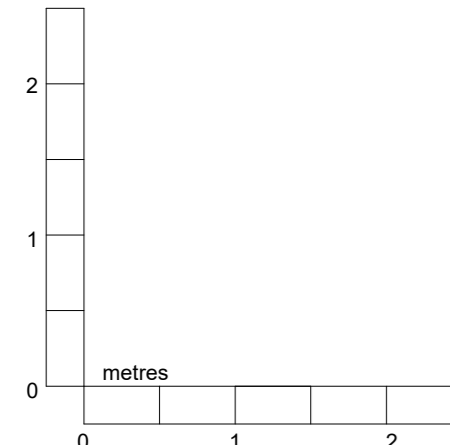
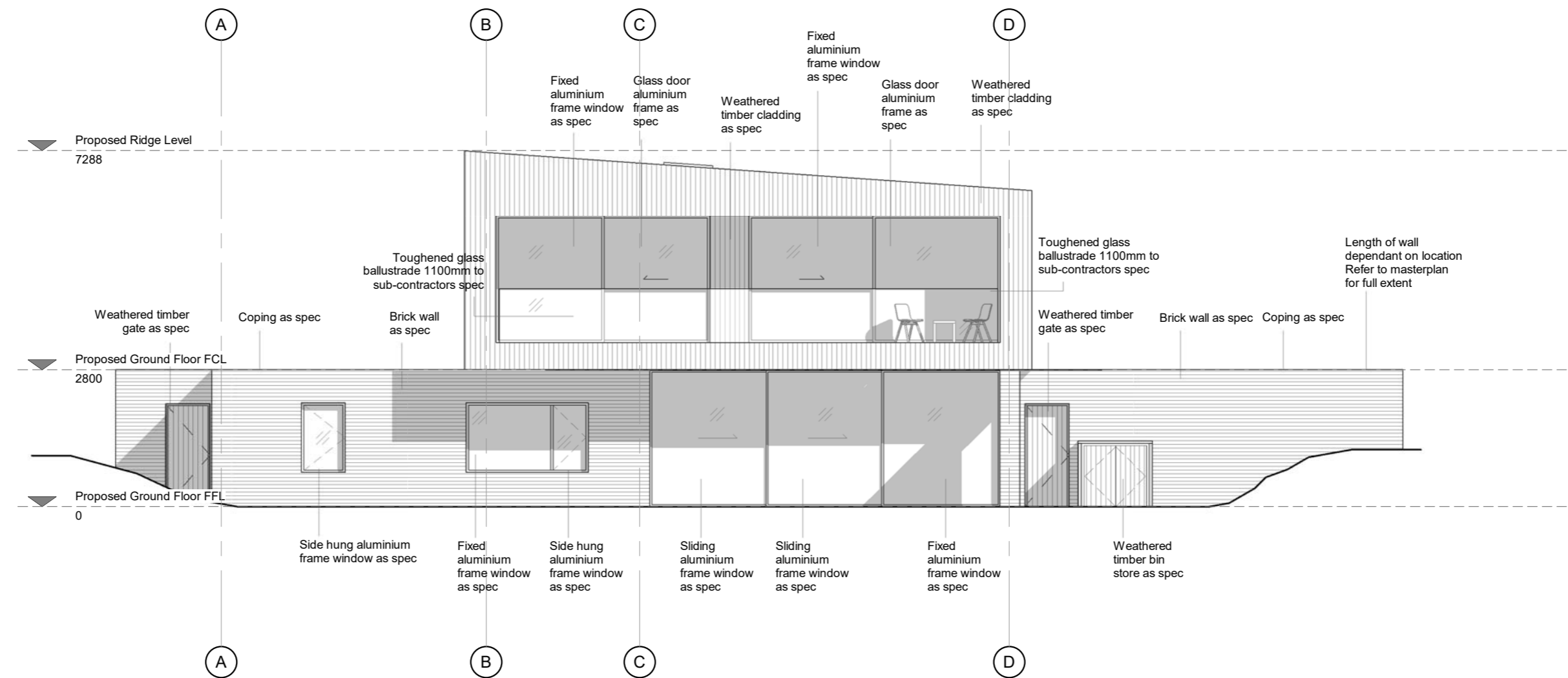
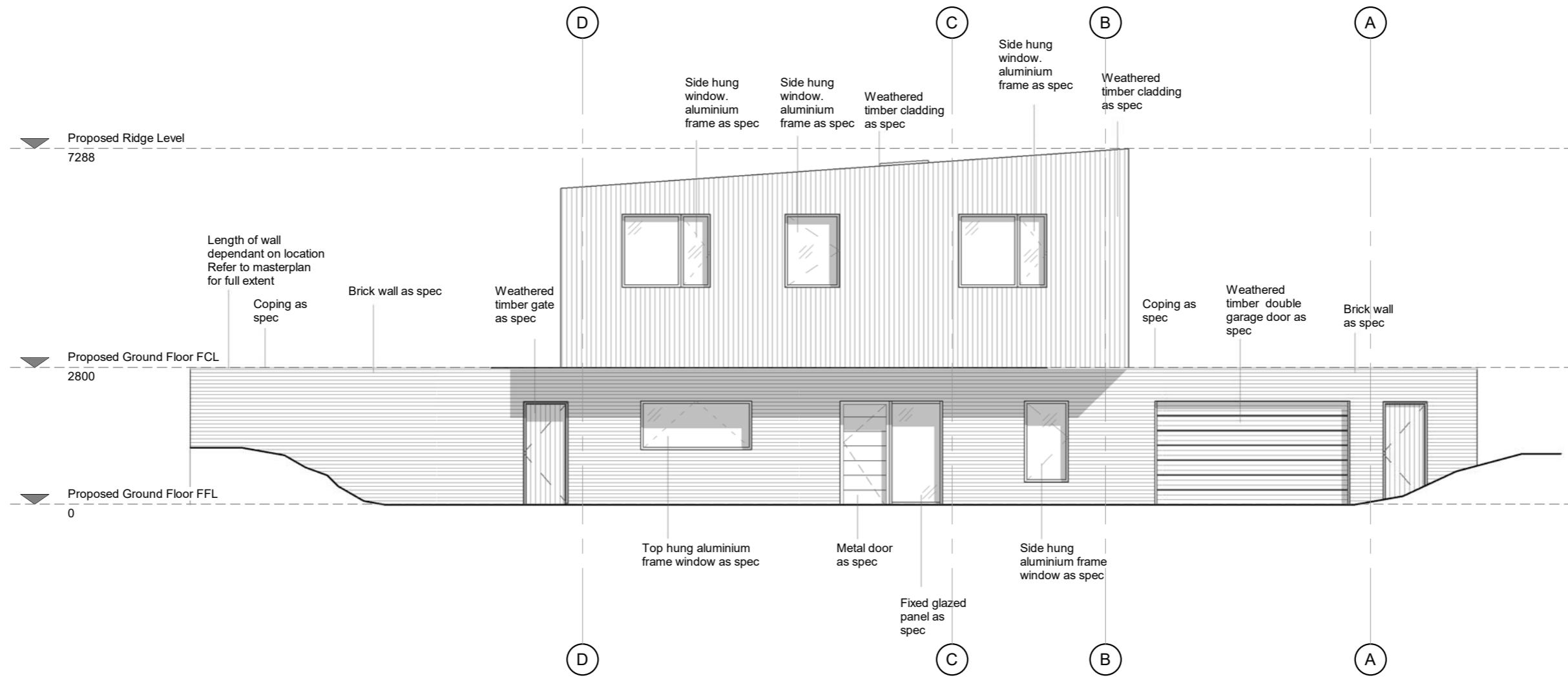
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PROJECT TITLE: Courtlands - Type A		
DRAWING TITLE: Front and Rear Elevation		
STATUS: Planning		
DRAWN BY: CC	CHECKED BY: JMO	DATE: Nov 21
JOB NO: 916D	DRAWING NO: P130	SCALE A2: 1 : 100
		REVISION: -

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Notes:

These drawings relate to Houses 1, 3 and 10
Houses have identical layout and form, but heights are dependant on site levels.

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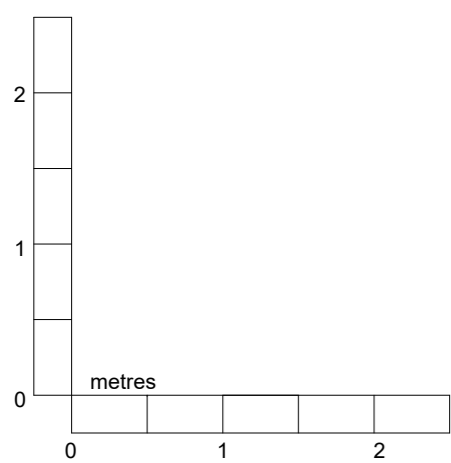
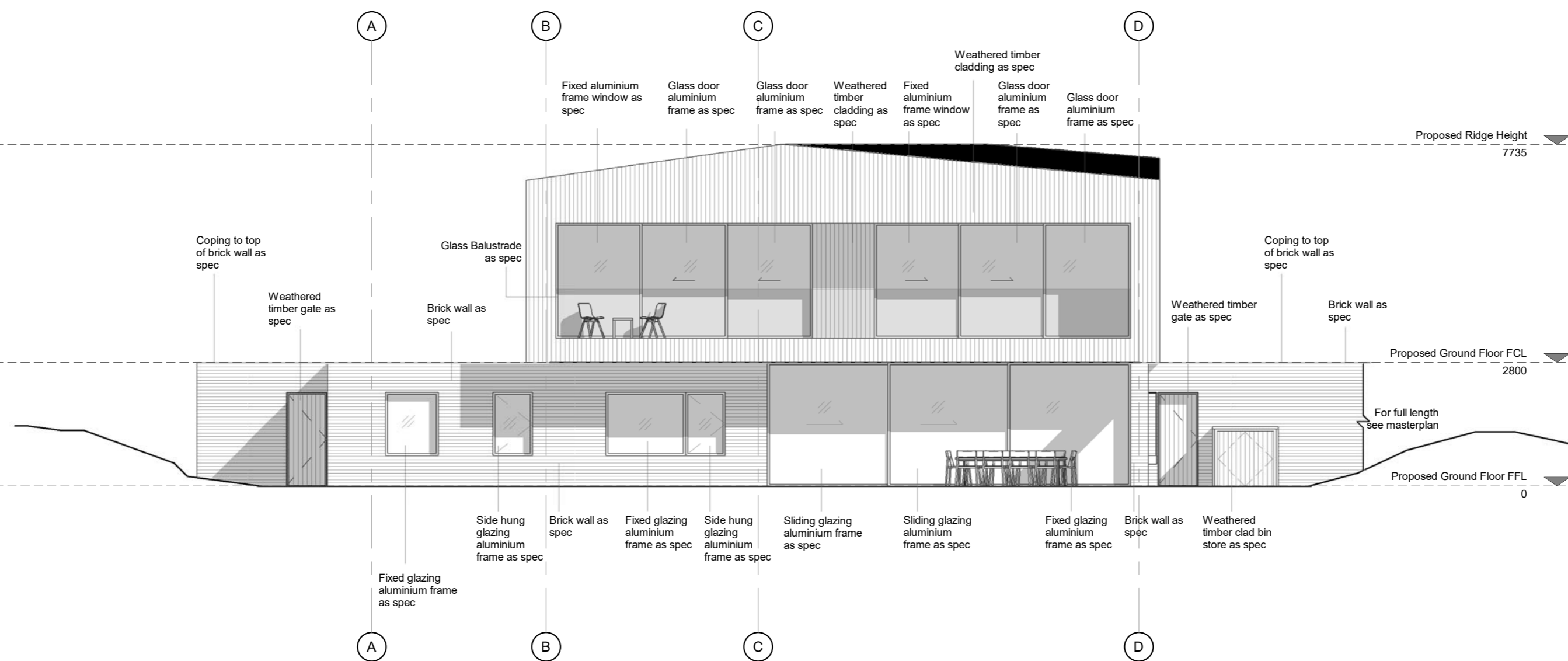
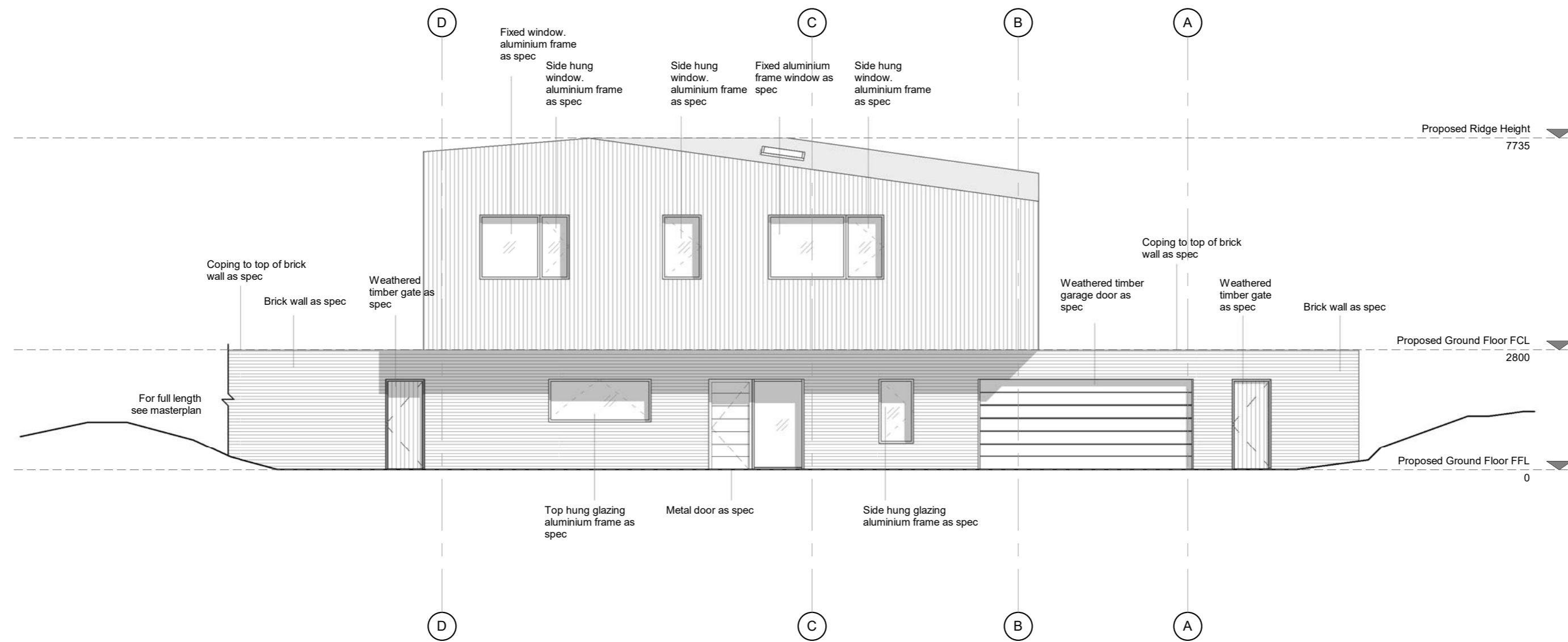
PROJECT TITLE:
Courtlands - House Type B

DRAWING TITLE:
Proposed Front and Rear Elevations

STATUS:
Planning

DRAWN BY: CC	CHECKED BY: JMO	DATE: Nov 21	SCALE A2: 1 : 100
JOB NO: 916D	DRAWING NO: P132	REVISION: -	

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Notes:

These drawings relate to Houses 5, 6 and 7.

Houses have identical layout and form, but heights are dependant on site levels.

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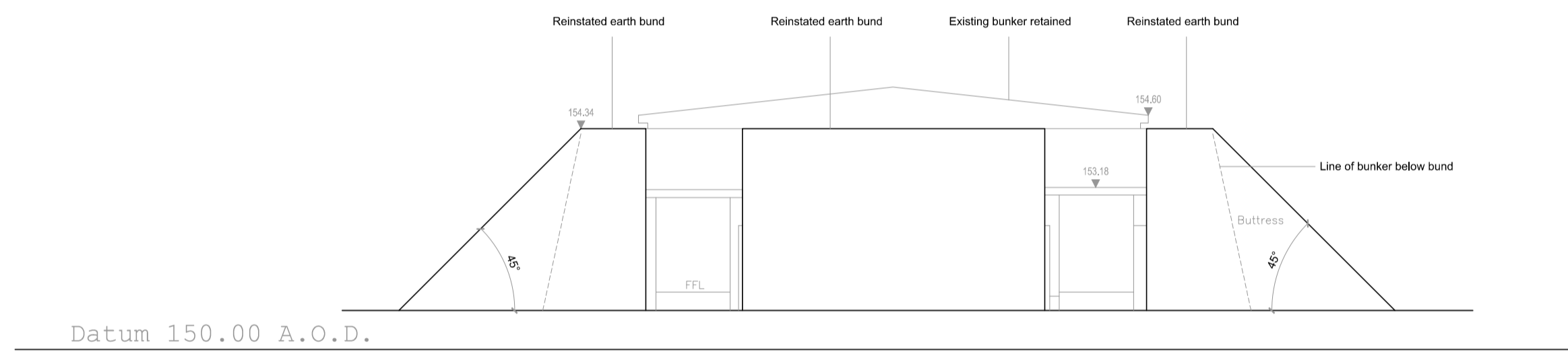
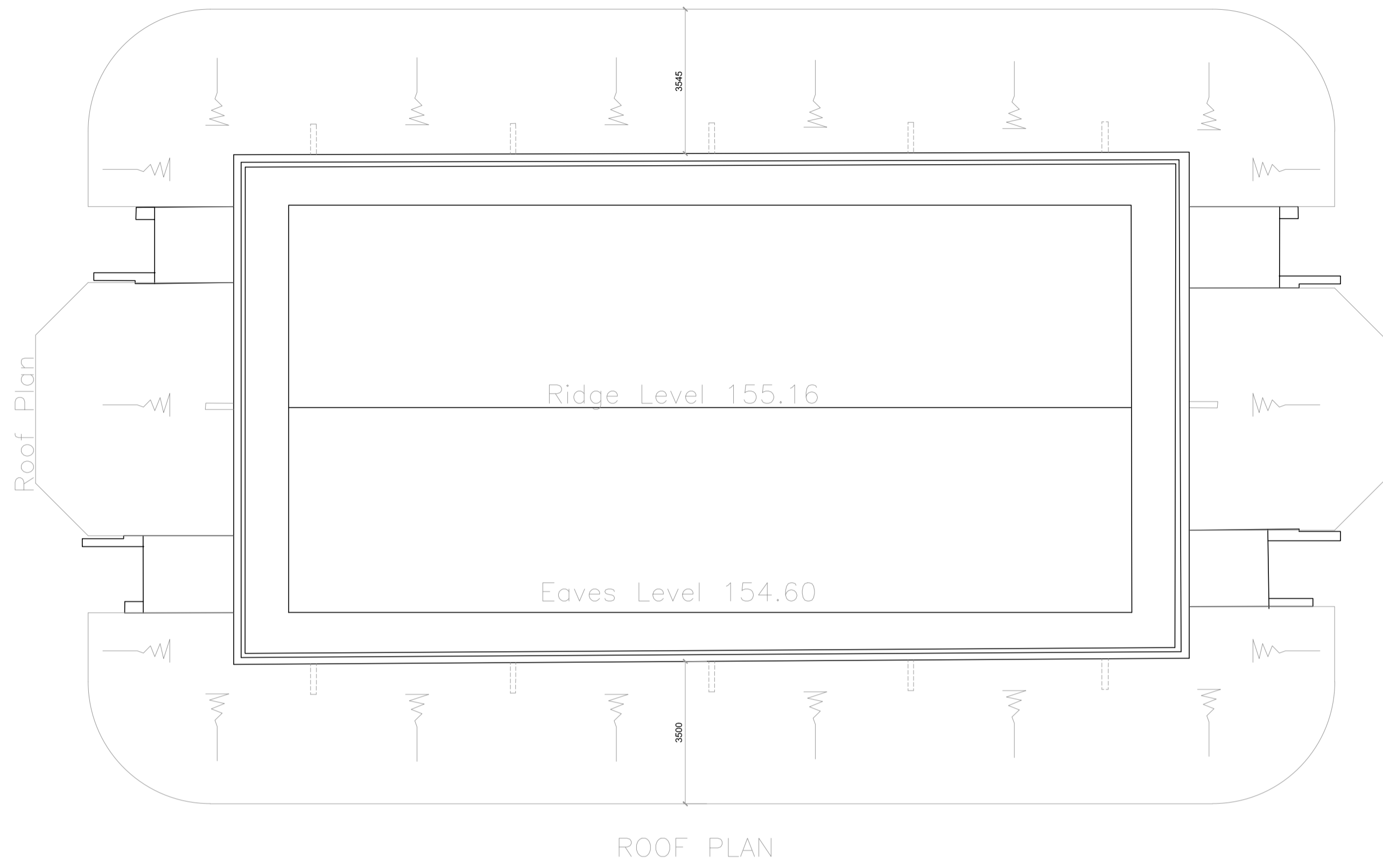
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DRAWING TITLE: Front and Rear Elevations

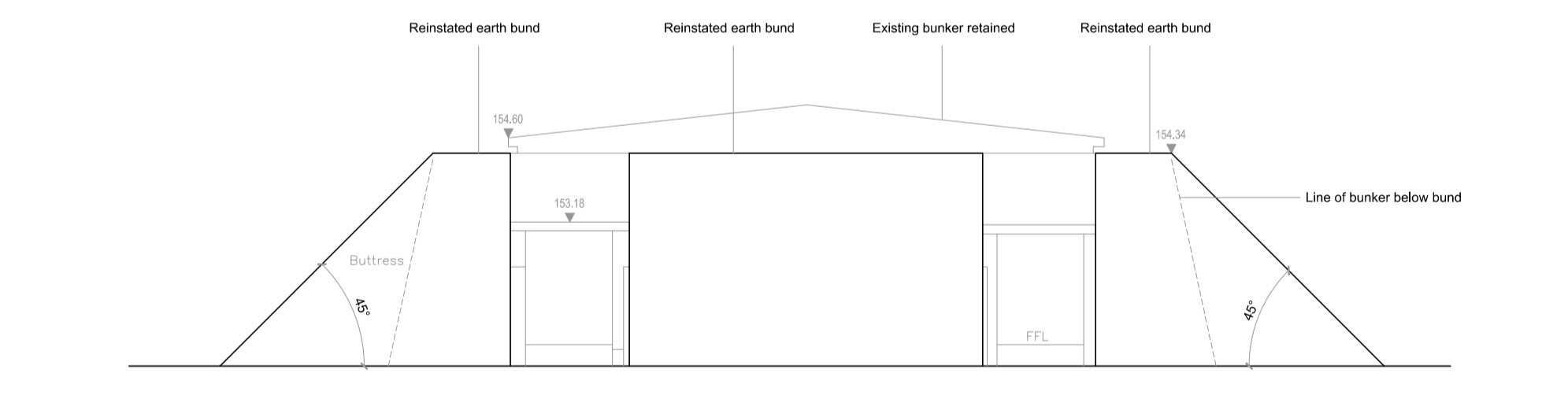
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JOB NO: 916D	DRAWING NO: P134	REVISION: -	

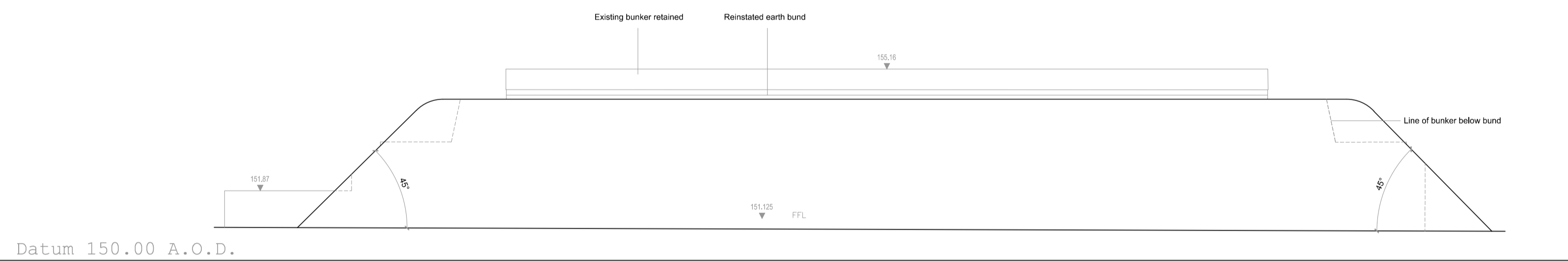
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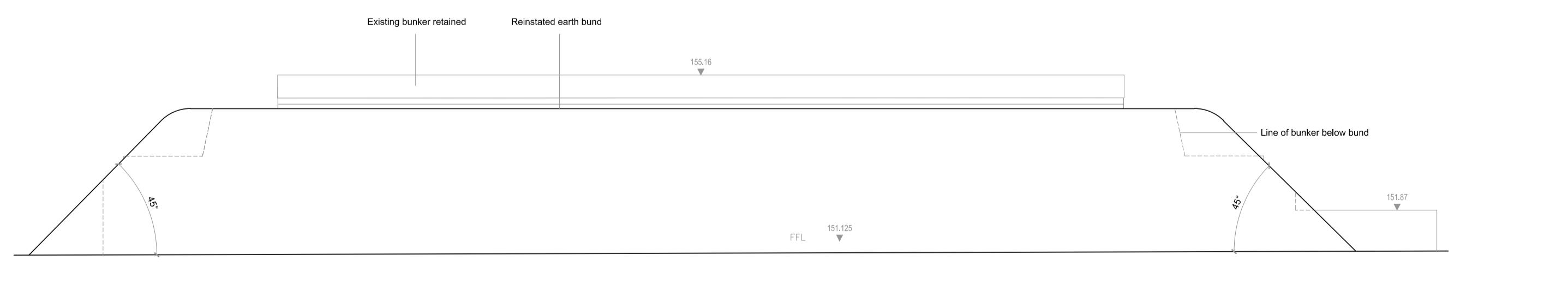
EAST ELEVATION



WEST ELEVATION



NORTH ELEVATION



SOUTH ELEVATION

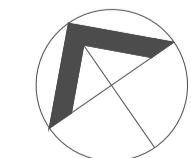
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Job title				
Courtlands				
Drawing title				
Proposed Bund Reinstatement				
Status				
PLANNING				
Drawn by	Checked by	Date	Scale at A1	Scale at A0
JMO	CC	August 2021	1:100	-
Job No.	Day No.	Revision		
916 D	P108	-		




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Agenda Item 12

Planning Committee
6th July 2022

Agenda Item: 12
22/00557/F

 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	6 th July 2022
	REPORT OF:	HEAD OF PLANNING
	AUTHOR:	Matthew Sheahan
	TELEPHONE:	01737 276514
	EMAIL:	Matthew.sheahan@reigate-banstead.gov.uk
AGENDA ITEM:	12	WARD: Redhill West And Wray Common

APPLICATION NUMBER:	22/00557/F	VALID:	09/03/2022
APPLICANT:	Mr A & D Lippett	AGENT:	Sawkings Harper Architects Ltd
LOCATION:	80 CROYDON ROAD REIGATE SURREY RH2 0NH		
DESCRIPTION:	Demolition of existing single-storey permanent structures (used as garages and storage) and the erection of 2No. self-built semi-detached 3-bedroom family dwellings with associated access, external amenity spaces, refuse storage and car and cycle parking.		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

SUMMARY

This is a full application for the demolition of existing structures on site and the erection of a pair of semi-detached dwellings. The dwellings would be located to the south-east side of Croydon Road, which is predominantly residential in character, comprises of a mix of semi-detached and detached dwellings and flats. Reigate fire station is located diagonally opposite the site.

The proposed dwellings would be traditional in their appearance, seeking to reflect the Victorian/ Edwardian architecture that is dominant in the area, particularly with respect to the pitched roof form, bay windows, dormers in the roof and materials palette. This would be an acceptable approach, reflective of the character of the area, representing an improvement to the existing site, which at present makes a negative contribution to the character of the street scene. The height and scale of the dwellings would be commensurate to that of neighbouring buildings and would not appear incongruous in this regard.

The introduction of dwellings would represent a change in relationship with neighbouring dwellings, however it is not considered that there would be sufficient harm to their amenity to justify refusal. The height, depth and scale would not generate loss of light to habitable rooms, nor would it result in unacceptable overshadowing to neighbouring rear gardens. Whilst some overlooking may occur from proposed rear

Agenda Item 12

Planning Committee
6th July 2022

Agenda Item: 12
22/00557/F

windows this would be normal for a residential area. The siting of the proposed dwellings would not result in an overbearing relationship occurring.

Sufficient levels of parking would be provided to accord with residential parking standards. Space would be provided within the site to allow for vehicles to exit on to the highway in a forward gear, and the County Highway Authority has raised no objection on highway safety grounds subject to conditions.

Additional benefits with regards to landscaping, ecology, cycle storage and electric vehicle charging points can be secured by condition.

In view of the above the application is considered to be acceptable on design, character, neighbour amenity and highway/ transport grounds, and is therefore recommended for approval subject to conditions.

RECOMMENDATION(S)

Planning permission is **GRANTED** subject to conditions.

Consultations:

County Highway Authority: No objection has been raised on highway safety, capacity and policy grounds and have raised no objection subject to conditions and informatives requiring details of the proposed belmouth access to be submitted and approved, the provision of parking as per the submitted plans, the submission of a construction transport management plan, the provision of electric vehicle charging points.

Contaminated Land Officer: No objection subject to conditions

Representations:

Letters were sent to neighbouring properties on the 10th March 2022. One response have been received raising the following issues:

Issue	Paragraph
Overdevelopment	Paragraph 6.2-6.5
Overlooking and loss of privacy	Paragraph 6.6-6.9
Overshadowing	Paragraph 6.6-6.9
Health fears	Paragraph 6.21
Noise and disturbance	Paragraph 6.24
Loss of/harm to trees	Paragraph 6.12
Inadequate parking	Paragraph 6.13-6.17
Inconvenience during construction	Paragraph 6.13-6.17
Increase in traffic and congestion	Paragraph 6.13-6.17
Hazard to highway safety	Paragraph 6.13-6.17

1.0 Site and Character Appraisal

- 1.1 The site is located on the south-east side of Croydon Road, to the north-east of Reigate. The site is predominantly residential in character, consisting largely of detached, semi-detached and flatted schemes. Residential development is largely comprised of Victorian/ Edwardian era dwellings, though there are a number of later 20th Century developments.
- 1.2 The site currently comprises a number of storage buildings used by the applicant, as well as hardstanding for the parking of vehicles.
- 1.3 More widely, the site is less than 1km from Reigate Town Centre and 0.5km from Reigate station, as well as Reigate College. The site is sustainably located in close proximity to a range of commercial and retail services.

2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: Formal pre-application advice was not sought from the Local Planning Authority prior to the submission of the application.

Agenda Item 12

Planning Committee
6th July 2022

Agenda Item: 12
22/00557/F

2.2 Improvements secured during the course of the application: Improvements have been sought to reduce the size of the proposed dormer windows and increase the amount of soft landscaping to the front of the properties.

2.3 Further improvements could be secured: Further improvements could be sought by way of conditions. A condition restricting the hours of use would be included.

3.0 Relevant Planning and Enforcement History

There is no planning or enforcement history related specifically to the application site. The following applications are related to land to the rear of 80 Croydon Road:

02/00852/OUT	Demolition of existing garages and erection of 1 no. two storey detached dwelling Amended plans received 25/6/02 showing access drive and siting of proposed building	Refused 8 th July 2002
03/01317/OUT	Demolition of existing garages & erection of 1 no. 2 storey detached dwelling - outline application. Drawing no: 1082	Approved 30 th September 2003

4.0 Proposal and Design Approach

4.1 This is a full application for the demolition of existing structures on site, which include garages and storage buildings, and the erection of a pair of semi-detached 3-bedroom dwellings. The dwellings would be of matching appearance and traditional in their design, being pitched roof/ gable ended properties, with a smaller more subservient central gable to the rear. Two modest pitched roof dormers would feature to the front elevations, with four small rooflights to the rear (two per dwelling). A further small rooflight would be inserted in to the side of the rear gable. It is also proposed to insert PV panels to the rear.

- 4.2 Regarding materials it is proposed to utilise slate for the roof, yellow blend facing brick of Flemish bond with soldier brick courses above the windows, sash windows, white painted fascia's/ bargeboards to the dormer windows and gable ends.

- 4.3 The dwellings would be 10m in height, with a depth of 12.6m at the ground floor, reducing to 9.9m at the first floor. The footprint of the dwellings would be set back from the front boundary by 5.5m, approx.. 3m behind 78 Croydon Road. The depth of rear garden would be 10m. The dwellings would be set 1m from the shared side boundaries.

- 4.4 Internally each property would provide three bedrooms, including a bedroom within the roof space. Externally parking would be located to the front alongside a manoeuvring space allowing vehicles to enter and exit the site safely. Refuse storage and secure cycle storage would be located to the rear of each property.

- 4.5 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:
Assessment;
Involvement;
Evaluation; and
Design.

- 4.6 Evidence of the applicant's design approach is set out below:

Assessment	<p>The site is identified as being located within the built-up area of Reigate. The surrounding area is characterised as being residential, Victorian in character with a mix of style in terms of appearance whilst acknowledging that traditional forms and detailing are predominant. Gable ends, bay and dormer windows are considered prevalent, with a mix of brickwork, render and hanging tiles. There is a balanced mix of slate and clay tiles used for the roofs.</p> <p>The site is identified as being in a highly accessible location, in close proximity to a range of public transport options including bus stops and Reigate Railway station being within walking distance. Reigate town centre is approx..13mins walk away.</p>
Involvement	It is not stated that any community involvement or consultation has taken place.
Evaluation	The statement does not include any evidence of other development options being considered.

Agenda Item 12

Planning Committee
6th July 2022

Agenda Item: 12
22/00557/F

Design	The scheme has been designed to respond to the pattern and character of development in the surrounding area in terms of quantum of development, layout, scale, architectural form and materials.
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4.7 Further details of the development are as follows:

Site Area	0.033Ha
Existing Use	Residential (C3) and ancillary storage
Proposed Use	Residential (Use Class C3) 2x3 bed dwellinghouses
Site Density	60dph
Parking Standard	Accessibility level – High – 2 spaces required (1 per dwelling)
Proposed Parking Spaces	2

5.0 Policy Context

5.1 Designation

Urban Area
Regeneration Area

5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)
CS10 (Sustainable Development),

5.3 Reigate & Banstead Development Management Plan 2019

Design	DES1, DES2, DES5, DES6, DES8 DES9
Housing Mix	DES4
Transport, access and parking	TAP1
Climate Change resilience and flooding	CCF1
Infrastructure	INF1

5.4 Other Material Considerations

National Planning Policy Framework
National Planning Practice Guidance
National Design Guide
Supplementary Planning Guidance Surrey Design

Local Distinctiveness Design Guide
A Parking Strategy for Surrey
Parking Standards for Development

Other

Human Rights Act 1998
Community Infrastructure Levy
Regulations 2010

6.0 Assessment

6.1 The application seeks planning permission for the demolition of existing single-storey permanent structures (used as garages and storage) and the erection of 2No. self-built semi-detached 3-bedroom family dwellings with associated access, external amenity spaces, refuse storage and car and cycle park.

- Principle of the development
- Design and character assessment
- Neighbouring Amenity
- Amenity for future occupiers and housing mix
- Trees and landscaping
- Transport matters
- Ecology
- Sustainability, Infrastructure and Climate change
- Environmental Health
- CIL
- Affordable Housing

Principle of the development

6.2 The application site is within the urban area, where there is a presumption in favour of sustainable development and where the principle of residential development is accepted. The area is comprised of a mix of residential, commercial and industrial uses. There is therefore no objection to the principle of residential development in this location. Consideration of the application therefore rests on whether the application is acceptable with regards to design and character, impact on neighbouring amenity, amenity for future occupiers, highway matters, trees and landscaping and ecology.

Design and character assessment

6.3 The proposed dwellings have been designed to have a traditional appearance that would be very much in keeping with the character and style of residential properties immediately adjacent to the site and within the wider surrounding area. The pitched roof/ gable ended form, bay windows, brick detailing and materials palette would correspond with the predominant Victorian/ Edwardian vernacular of Croydon Road, which constitutes the more attractive properties in the road. The proposed dormer windows to the front elevation would be visually prominent however they are well designed and of a scale that would be proportionate to that of the dwellings. They would not be dissimilar to those

Agenda Item 12

Planning Committee
6th July 2022

Agenda Item: 12
22/00557/F

found on other properties and dormer windows are not an uncommon feature in the road.

- 6.4 The scale of the dwellings and their siting within the plot would be acceptable. They would be set behind the neighbouring property at 78 Croydon Road and forward of 84 Croydon Road, thus following the staggered principal elevation which characterises the road in parts, which is not uniform in this respect. The height of the dwellings would not appear dominant within the street and would be lower than number 78 to the south-west. Whilst they would be higher than number 80 to the north-east this property is not typical with regard to building heights, and they would be consistent with other properties in the road.
- 6.5 In light of the above it is considered that the design of the dwellings and the impact upon the character of the area would be acceptable and would comply with Policies DES1 and DES2 of the Development Management Plan 2019 and Character and Local Distinctiveness Design Guide.

Impact on neighbouring amenity

- 6.6 The adjacent neighbour to the south-west, 78 Croydon Road, is taller than the proposed dwellings. A single side facing window would face the application site at the second floor level, however this appears to be obscure glazed and likely serving a bathroom and therefore it is not felt that there would be harm to this window. The dwellings would not extend beyond the rear elevation of number 78 at ground or first floor level. Whilst the proposed first floor would be deeper it would not intersect a 45 degree line as measured from the neighbours' upper floor rear windows in the rear elevation in the horizontal plane. This would pass the light assessment test as set out within the Councils supplementary planning guidance. The level of built form would be sufficiently minimal and away from the boundary so as to avoid being overbearing. Whilst rear bedroom windows would increase overlooking to the rear this would not be untypical in a residential area such as this. A first floor side window is proposed however this would serve a bathroom and would be conditioned to be obscure glazed.
- 6.7 Turning to 84 Croydon Road, this is a smaller property of mid to late 20th Century in age. This property features a first floor side window that would face the site however this serves a bathroom, not a habitable room, therefore there would not be concern with regard to loss of light to a habitable room. The proposed development would be 1m from the shared boundary. The single storey ground floor element would be 3m in height, and the first floor would be set behind the neighbours' rear building line with a distance of 3.4m between flank elevations. In view of this, whilst the presence of dwellings where at present there are none would be an obvious change, it is not the view that the dwellings would be overbearing and would form quite a typical relationship for a road such as this. Again, whilst the rear facing bedrooms would overlook the neighbouring garden to a degree this is not unusual in a residential area. Rear facing windows and the rear conservatory of the neighbour would not be harmfully impacted in regard to light. The rear garden faces south-east so would receive considerable amount of light throughout the day. A first floor side

window is proposed however this would serve a bathroom and would be conditioned to be obscure glazed.

- 6.8 30C Doods Road is located to the south-east of the site and is accessed via Wilmots Close. This is a two storey pitched roof dwelling with a large window that would face the rear of the proposed dwellings, which appears to serve a bedroom. This property was granted outline consent in 2003. There would be a separation distance of 21m between the proposed first floor and the rear of 30C Doods Road, which is generally considered acceptable to avoid mutual overlooking between opposing windows, as well as any overbearing impact. Whilst some views may be afforded of the rear gardens this would not be harmful and would not be different to the level of view afforded to a number of neighbouring properties.
- 6.9 In light of the above it is considered that the proposed dwellings would not result in significant harm to the amenity of neighbouring properties and would therefore comply with Policy DES1 of the DMP 2019 in this regard.

Amenity for future occupiers and housing mix

- 6.10 It is a fundamental objective of planning policy and stated within the National Planning Policy Framework 2021 that we provide high quality housing that is well designed and built to a high standard. The advice is amplified further by policies DES2 and DES5 of the Development Management Plan, which requires developments to demonstrate that dwellings have been designed to ensure that a good standard of amenity for all existing and future occupants and meet the minimum relevant nationally described space standards and be arranged to ensure that habitable rooms are arranged to have an acceptable outlook and where possible receive direct sunlight.
- 6.11 Each property would provide three bedrooms, one of which would be in the roof. A two storey three bedroom dwelling would need to provide a minimum of 84qm of residential floor space (102sqm for a total of 6 people). Each dwelling would provide approx.. 106sqm, thus exceeding the maximum requirement. Each habitable room would be well served by windows providing ample light and reasonable outlook. Externally each dwelling would be provided with a rear garden some 10m in depth. This size of garden would be sufficient for dwellings of this size and would be consistent with other dwellings in the road, where garden areas do vary. There would be ample space for storage for things such as bicycles and refuse bins, and the drying of clothes. The proposal would therefore be acceptable with regard to amenity for future occupants and therefore would comply with Policies DES2 and DES5 of the DMP 2019.

Trees and Landscaping

- 6.12 The existing site is relatively devoid of trees and landscaping, however there are a number of trees within neighbouring gardens that would potentially be impacted by the proposed development. In order to secure their protection during the course of development a condition requiring the submission of full tree protection details for approval prior to commencement of development

Agenda Item 12

Planning Committee
6th July 2022

Agenda Item: 12
22/00557/F

would be included in any decision to approve. A further condition to secure landscaping for the site would also be included should permission be granted. The site does not currently contribute to the generally attractive frontages of properties along Croydon Road. The front would be turned over to hard landscaping in the form of block pavements, which would be softened by planters to the front of each house, along the south-west side and partially across the front boundary in front of the parking spaces. The rear garden would be a mix of patio areas, which would step up by 0.5m to the rear lawn. This sort of rear garden would be appropriate for a residential area of this kind, of which there is significant variance. Subject to compliance with the aforementioned conditions the proposal would be acceptable with regard to trees and would comply with Policies DES1 and NHE3 of the DMP 2019.

Transport Matters

- 6.13 The site is located partly in both a high and medium accessible location (approx.. 50/50) as defined within Annex 4 of the DMP. In line with the Councils' residential parking standards either 1 or 2 spaces would be required per dwelling, depending on which is applied. The DMP does not provide guidance for when this eventuality occurs, therefore judgement needs to be applied. It is proposed that two spaces would be located in front of unit 2, parallel to the road, with a manoeuvring/ turning area in front of unit 1. The County Highway Authority (CHA) has reviewed this arrangement and is satisfied that this would be preferable to having a single space in front of each dwelling, which would require stopping in the road and potentially reversing off the highway. This would cause the stopping up of traffic and increases the potential to create highway safety issues. The proposed arrangement would allow vehicles to exit the highway safely and exit the site in a foreword gear, increasing visibility on a road that suffers from parking on the highway. The applicant has submitted tracking plans to demonstrate that turning can be achieved, which has been reviewed by the CHA, who have raised no objection. Conditions have been recommended, including requiring the submission of a construction transport management plan for approval prior to commencement of development. In order to create the proposed spaces the existing post-box would be moved to the north corner of the site. In light of the above 1 space per dwelling is considered to be acceptable, particularly taking account of the sustainable location in close proximity to Reigate town centre and the railway station. It would allow additional space for manoeuvring to take place, creating a safer environment for both drivers and pedestrians.
- 6.14 It is noted that the existing site accommodates a number of buildings used for storage, as well as the parking of vehicles. These are within the ownership of the applicant, and any vehicles would be relocated to within the curtilage of 30C Doods Road, within the applicant's ownership, not on to the road, therefore there would not be a concern regarding the potential exacerbation of on-street parking.
- 6.15 Provision for the secure storage of bicycles would be provided to the rear garden and a condition requiring the provision of an electric vehicle charging point for each dwelling would be included in the event of planning permission

being granted. This would meet the Councils' aims of encouraging more sustainable forms of travel and reduced car dependency in accordance with Policy TAP1 of the DMP 2019.

- 6.16 Regarding refuse, storage is provided to the rear of each dwelling. Bins would then be presented at the kerbside for collection, as is typically the case for properties along Croydon Road.
- 6.17 In light of the above considerations the scheme is considered to comply with Policy TAP1 of the DMP 2019 with regard to highway safety, capacity and policy grounds and would therefore be acceptable.

Ecology

- 6.18 Whilst detailed ecological information has not been provided at this stage, it is noted from the submitted planning statement that it is intended to retain many of the existing trees and hedges within the site. Policy NHE2 of the Development Management Plan 2019 requires new development to:
- a. retain and enhance other valued priority habitats and features of biodiversity importance; and
 - b. be designed, wherever possible, to achieve a net gain in biodiversity. Where a development will impact on a priority habitat or species, or protected species, and mitigation cannot be provided on site in an effective manner, developers may be required to offset the loss by contributing to appropriate biodiversity projects elsewhere, in a location agreed with the Council.
- 6.19 In order to improve the ecological potential of the site and increase the level of biodiversity, a condition requiring the submission of an ecological appraisal prior to commencement of development above ground level. Subject to compliance with this condition the development would comply with Policy NHE2 of the Development Management Plan 2019.

Sustainability, Infrastructure and Climate change

- 6.20 Policy CCF1 of the Councils Development Management Plan 2019 seeks to ensure that all new development contributes to reducing carbon emissions. New development will be encouraged to incorporate passive and active energy efficiency measure and climate change resilience measures and renewable energy technologies. In order that the proposed development contributes to achieving these aims, in the event that planning permission were to be granted, conditions requiring demonstration that it will meet the national water efficiency standard of 110litres/person/day and achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations would be attached. A further condition requiring the provision of broadband connection, in accordance with Policy INF3 of the DMP 2019, would also be attached to any grant of planning permission.

Agenda Item 12

Planning Committee
6th July 2022

Agenda Item: 12
22/00557/F

Environmental Health

- 6.21 The proposal would involve the demolition and removal of existing storage buildings. Given the age of the buildings it is possible that these may contain asbestos. In order to ensure that these buildings are demolished safely and that material is disposed of in an appropriate manner a condition requiring a remedial mitigation scheme, to be written by a suitably qualified person, to be submitted to the LPA for approval prior to commencement of the development shall be included in the event of permission being granted. The scheme shall identify potential sources of asbestos contamination and detail removal or mitigation appropriate for the proposed end use of the site. Detailed working methods are not required but the scheme of mitigation shall be independently verified to the satisfaction of the LPA prior to occupation to ensure the remedial mitigation plan has been complied with. The development shall then be undertaken in accordance with the approved details. Subject to this condition being satisfied the scheme would comply with policy DES9 of the DMP 2019.

CIL

- 6.22 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, roads, public transport and community facilities which are needed to support new development. This development would be CIL liable although, the exact amount would be determined and collected after the grant of planning permission.

Affordable Housing

- 6.23 Core Strategy Policy CS15 and the Council's Affordable Housing SPD require financial contributions towards affordable housing to be provided on housing developments of 1-9 units. However, the 2019 NPPF makes clear such contributions should not be sought from developments of 10 units or less.

In view of this the Council is not presently requiring financial contributions from applications such as this resulting in a net gain of 10 units or less. The absence of an agreed undertaking does not therefore warrant a reason for refusal in this case.

Other Matters

- 6.24 It is noted that objection has been raised on the grounds of increased noise and disturbance and inconvenience during the construction phase. Whilst development can cause disturbance this is temporary in nature. Separate noise legislation is in place to deal with excessive disturbance and it would be expected that works would be carried out with the hours outlined in informative 3 below.

CONDITIONS

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan type	Reference	Version	Received
Street Scene	2205	P2	26.04.2022
Elevation Plan	2200	P2	26.04.2022
Floor Plan	2100	P2	26.04.2022
Location Plan	0001	P1	09.03.2022
Site Layout Plan	1000	P1	09.03.2022
Floor Plan	1100	P1	09.03.2022
Elevation Plan	1200	P1	09.03.2022
Street Scene	1205	P1	09.03.2022
Section Plan	1305	P1	09.03.2022
Proposed Plans	2002	P1	09.03.2022
Site Layout Plan	2000	P1	09.03.2022
Proposed Plans	2001	P1	17.06.2022
Section Plan	2305	P1	09.03.2022

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

3. Notwithstanding the submitted plan 2000 Rev P1 the development shall not be occupied until the proposed belmouth vehicular access to Croydon Road has been constructed in accordance with a scheme to be submitted to and approved in writing with the Local Planning Authority, all to be permanently retained.

Reason: in order that the development should not prejudice highway safety nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework (2021) as well as Policy TAP1 for Parking, Access, and Servicing and Policy DES8 Construction Management of the Reigate and Banstead Local Plan Development Management Plan September 2019.

4. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with approved plan numbered 2000 Rev P1 for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking

Agenda Item 12

Planning Committee
6th July 2022

Agenda Item: 12
22/00557/F

and turning areas shall be retained and maintained for their designated purposes.

Reason: in order that the development should not prejudice highway safety nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework (2021) as well as Policy TAP1 for Parking, Access, and Servicing and Policy DES8 Construction Management of the Reigate and Banstead Local Plan Development Management Plan September 2019.

5. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for bicycles to be parked. Thereafter the parking area shall be retained and maintained for its designated purposes.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

6. No development shall commence until a Construction Transport Management Plan, to include details of:
 - (a) parking for vehicles of site personnel, operatives and visitors
 - (b) loading and unloading of plant and materials
 - (c) storage of plant and materials
 - (e) provision of boundary hoarding behind any visibility zones
 - (h) measures to prevent the deposit of materials on the highway
 - (k) on-site turning for construction vehicles

has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and DES8 Construction Management of the Reigate and Banstead Local Plan Development Management Plan September 2019.

- 7 The development hereby approved shall not be occupied unless and until each of the proposed dwellings are provided with a fast charge socket (current minimum requirements - 7 kw Mode 3 with Type 2 connector - 230v AC 32 Amp single phase dedicated supply) in accordance with a scheme to be submitted and approved in writing by the Local Planning Authority and thereafter retained and maintained to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other

highway users, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

8. The developer must either submit evidence that the building was built post 2000 or provide an intrusive pre-demolition and refurbishment asbestos survey in accordance with HSG264 supported by an appropriate mitigation scheme to control risks to future occupiers. The scheme must be written by a suitably qualified person and shall be submitted to the LPA and must be approved prior to commencement to the development. The scheme as submitted shall identify potential sources of asbestos contamination and detail removal or mitigation appropriate for the proposed end use. Detailed working methods are not required but the scheme of mitigation shall be independently verified to the satisfaction of the LPA prior to occupation. The development shall then be undertaken in accordance with the approved details.

Reason: To ensure that a satisfactory strategy is put in place for addressing contaminated land before development commences and to make the land suitable for the development without resulting in risk to construction workers, future users of the land, occupiers of nearby land and the environment with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 and the NPPF.

9. The remedial mitigation scheme must be written by a suitably qualified person and shall be submitted to the LPA and must be approved prior to commencement to the development. The scheme as submitted shall identify potential sources of asbestos contamination and detail removal or mitigation appropriate for the proposed end use of the site. Detailed working methods are not required but the scheme of mitigation shall be independently verified to the satisfaction of the LPA prior to occupation to ensure the remedial mitigation plan has been complied with. The development shall then be undertaken in accordance with the approved details.

Reason: To ensure that a satisfactory strategy is put in place for addressing contaminated land before development commences and to make the land suitable for the development without resulting in risk to construction workers, future users of the land, occupiers of nearby land and the environment with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 and the NPPF.

10. Prior to commencement of development a written comprehensive environmental desktop study report is required to identify and evaluate possible on and off site sources, pathways and receptors of contamination and enable the presentation of all plausible pollutant linkages in a preliminary conceptual site model. The study shall include relevant regulatory consultations such as with the Contaminated Land Officer and be submitted to the Local Planning Authority and is subject to the approval in writing of the Local Planning Authority and any additional requirements that it may specify.

Agenda Item 12

Planning Committee
6th July 2022

Agenda Item: 12
22/00557/F

The report shall be prepared in accordance with the Environment Agency's Land Contamination: Risk Management Guidance (2020) and British Standard BS 10175.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

11. Prior to commencement of development, in follow-up to the environmental desktop study, a contaminated land site investigation proposal, detailing the extent and methodologies of sampling, analyses and proposed assessment criteria required to enable the characterisation of the plausible pollutant linkages identified in the preliminary conceptual model, shall be submitted to the Local Planning Authority. This is subject to the written approval in writing of the Local Planning Authority, and any additional requirements that it may specify, prior to any site investigation being commenced on site. Following approval, the Local Planning Authority shall be given a minimum of two weeks written notice of the commencement of site investigation works. Please note this means a proposal is required to be submitted and approved prior to actually undertaking a Site Investigation.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

12. Prior to commencement of the development, a contaminated land site investigation and risk assessment, undertaken in accordance with the site investigation proposal as approved that determines the extent and nature of contamination on site and is reported in accordance with the standards of DEFRA's and the Environment Agency's Land Contamination: Risk Management Guidance (2020) and British Standard BS 10175, shall be submitted to the Local Planning Authority and is subject to the approval in writing of the Local Planning Authority and any additional requirements that it may specify. If applicable, ground gas risk assessments should be completed in line with CIRIA C665 guidance.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

13. a. Prior to commencement of the development a detailed remediation method statement should be produced that details the extent and method(s) by which the site is to be remediated, to ensure that unacceptable risks are not posed to identified receptors at the site and details of the information to be included

in a validation report, has been submitted to and approved in writing by the Local Planning Authority, and any additional requirements that it may specify, prior to the remediation being commenced on site. The Local Planning Authority shall then be given a minimum of two weeks written notice of the commencement of remediation works.

b. Prior to occupation, a remediation validation report for the site shall be submitted to the Local Planning Authority in writing. The report shall detail evidence of the remediation, the effectiveness of the remediation carried out and the results of post remediation works, in accordance with the approved remediation method statement and any addenda thereto, so as to enable future interested parties, including regulators, to have a single record of the remediation undertaken at the site. Should specific ground gas mitigation measures be required to be incorporated into a development the testing and verification of such systems should have regard to CIRIA C735 guidance document entitled 'Good practice on the testing and verification of protection systems for buildings against hazardous ground gases' and British Standard BS 8285 Code of practice for the design of protective measures for methane and carbon dioxide ground gases for new buildings.

Reason: To demonstrate remedial works are appropriate and demonstrate the effectiveness of remediation works so that the proposed development will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

14. Unexpected ground contamination: Contamination not previously identified by the site investigation, but subsequently found to be present at the site shall be reported to the Local Planning Authority as soon as is practicable. If deemed necessary development shall cease on site until an addendum to the remediation method statement, detailing how the unsuspected contamination is to be dealt with, has been submitted in writing to the Local Planning Authority. The remediation method statement is subject to the written approval of the Local Planning Authority and any additional requirements that it may specify.

Note: Should no further contamination be identified then a brief comment to this effect shall be required to discharge this condition.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

15. The materials to be used in the construction of the external surfaces of the development shall be constructed in accordance with the materials as specified within the application and there shall be no variation without prior approval and agreement in writing with the Local Planning Authority.

Agenda Item 12

Planning Committee
6th July 2022

Agenda Item: 12
22/00557/F

Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

16. The first floor windows in the side elevations of the development hereby permitted shall be glazed with obscured glass and shall be non-opening unless the parts of the window which can be opened are more than 1.7 metres above the floor of the room in which the window is installed and shall be maintained as such at all times.

Reason: To ensure that the development does not affect the amenity of the neighbouring property by overlooking with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

17. No development shall commence including groundworks until a detailed Tree Protection Plan (TPP) shall be submitted to and approved in writing by the local planning authority. The TPP shall contain details of the specification and location of tree protection (barriers and/or ground protection) and any construction activity that may take place within the protected root areas of trees/hedges shown, where retained on the TPP. The tree protection measures shall be installed prior to any development works and will remain in place for the duration of all construction works. The tree protection barriers/ground protection shall only be removed on the completion of all construction activity, including hard landscaping. All works shall be carried out in strict accordance with these details when approved.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with Reigate and Banstead Development Management Plan 2019 policies NHE3, DES1 and DES3 and the recommendations within British Standard 5837:2012 Trees in relation to design, demolition and construction.

18. The development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected has been submitted to and approved in writing by the Local Planning Authority. The boundary treatment shall include wildlife friendly access and be completed before the occupation of the development hereby permitted.

Reason: To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the Reigate and Banstead Development Management Plan 2019 policy DES1 and NHE3.

19. No development shall commence on site until a scheme for the landscaping of the site including the retention of existing landscape features has been submitted to and approved in writing by the local planning authority. Landscaping schemes shall include details of hard and soft landscaping, including any tree removal/retention, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant

sizes and proposed numbers/densities and an implementation and management programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to first occupation or within the first planting season following completion of the development hereby approved or in accordance with a programme agreed in writing with the local planning authority.

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837. Trees in relation to construction.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason: To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with Reigate and Banstead Borough Development Management Plan 2019 policies NHE3, DES1 and DES3, and the recommendations within British Standards including BS8545:2014 and British Standard 5837:2012.

20. No development above ground level shall commence until a scheme to provide positive biodiversity benefits, informed by a preliminary ecology appraisal, has been submitted to and approved in writing by the local planning authority (LPA). This should be designed alongside the soft landscaping proposals for the site. The biodiversity enhancement measures approved shall be carried out and maintained in strict accordance with these details or as otherwise agreed in writing by the LPA, and before occupation of this development.

Reason: To provide enhancements to the biodiversity of the site in accordance with the provisions of the National Planning Policy Framework and Reigate and Banstead Development Management Plan 2019 policy NHE2.

21. The development hereby permitted shall not commence until details of the design of a surface water drainage scheme has been submitted to and approved in writing by the local planning authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDs, NPPF and Ministerial Statement on SuDs. The required drainage details shall include:

- a) Evidence that there is no risk of contamination through the infiltration SuDs
- b) Evidence that the proposed final solution will effectively manage the 1 in 30 and 1 in 100 (+40% allowance for climate change) storm events and 10% allowance for urban creep, during all stages of the

Agenda Item 12

Planning Committee
6th July 2022

Agenda Item: 12
22/00557/F

- development (Pre, Post and during), associated discharge rates and storage volumes shall be provided using a maximum discharge rate of 37.8l/s.
- c) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels and long and cross sections of each element including details of any flow restrictions and maintenance / risk reducing features (silt traps, inspections chambers etc)
 - d) Details of how the drainage system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the drainage system is operational
 - e) Details of drainage management responsibilities and maintenance regimes for the drainage system
 - f) A plan showing exceedance flows (i.e. during rainfall greater than design events or during blockage) and how property on and offsite will be protected. This should include details of how surface water run-off entering the site from the bunded northern boundary will be intercepted.

The development shall be completed in accordance with the approved details and thereafter maintained.

Reason: To ensure the design meets the technical standards for SuDs and the final drainage design does not increase flood risk on or off site in accordance with, Policy CS10 of the Core Strategy 2014, Policy CCF2 of the Development Management Plan 2019 and the NPPF.

22. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:
- a) A broadband connection accessed directly from the nearest exchange or cabinet,
 - b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

23. The development hereby approved shall not be first occupied unless and until an Energy and Water Efficiency Statement has been submitted to and approved in writing by the Local Planning Authority. The Statement shall detail how the development will:
- a) Ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day,

- b) Achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations.

The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.info.
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : Climate Change Information.
3. You are advised that the Council will expect the following measures to be included in the above CMS condition to control noise, pollution and parking:
 - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
 - (e) There should be no burning on site;
 - (f) Only minimal security lighting should be used outside the hours stated above; and
 - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - www.ccscheme.org.uk/index.php/site-registration.

Agenda Item 12

Planning Committee
6th July 2022

Agenda Item: 12
22/00557/F

4. The developer is advised that as part of the detailed design of the highway works required by the above conditions, the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
5. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
6. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
7. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to: <http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.
8. Notwithstanding any permission granted under the Planning Acts, no signs, devices or other apparatus may be erected within the limits of the highway without the express approval of the Highway Authority. It is not the policy of the Highway Authority to approve the erection of signs or other devices of a non-statutory nature within the limits of the highway.
9. The permission hereby granted shall not be construed as authority to obstruct the public highway by the erection of scaffolding, hoarding or any other device or apparatus for which a licence must be sought from the Highway Authority Local Highways Service.
10. Environmental Health would like to draw the applicant attention to the specifics of the contaminated land conditional wording such as 'prior to commencement', 'prior to occupation' and 'provide a minimum of two weeks' notice'. The submission of information not in accordance with the specifics of the planning conditional wording can lead to delays in discharging conditions, potentially result in conditions being unable to be discharged or even enforcement action should the required level of evidence/information be unable to be supplied. All relevant information should be formally submitted to the Local Planning Authority and not direct to Environmental Health.
11. The use of a suitably qualified arboricultural consultant is essential to provide acceptable submissions in respect of the arboricultural tree condition above.

All works shall comply with the recommendations and guidelines contained within British Standard 5837.

12. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above landscaping condition. The planting of trees and native hedging shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate structural landscape trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of [Semi-Mature/Advanced Nursery] stock / [Extra Heavy Standard/Heavy Standard] size with initial planting heights of not less than [6m/4.5m/4m/3.5m] with girth measurements at 1m above ground level in excess of [20/25cm/16/18cm/14/16cm/12/14cm].
13. With regard to Condition 17 (Landscaping) and Condition 16 (boundary treatment) the Council is encouraging the developer to incorporate planting and measures to encourage biodiversity and wildlife and allow wildlife to move into and out of gardens and, in particular, include Hedgehog friendly gravel boards where appropriate. Details of the 'wildlife friendly' measures should be provided with the submission of the details for approval.
14. Building works involving work on an existing wall shared with another property; or new building on the boundary with a neighbouring property; or excavating near a neighbouring building, may fall within the scope of the Party Wall, etc. Act 1996 and may require that you serve a Statutory Notice on all affected owners. Further guidance is available from <https://www.gov.uk/guidance/party-wall-etc-act-1996-guidance>
15. The developer is advised that if the buildings to be demolished are found to contain asbestos within the building fabric they should produce a mitigation plan to reduce any potential risks to construction workers and future occupiers. For further information see the Council's website or contact Environmental Protection at the Council. http://www.reigate-banstead.gov.uk/info/20060/environmental_health/712/asbestos
16. The applicant is advised to seek the advice of Royal Mail with regard to the relocation of the existing post box and any permission that may be required to carry out these works.

REASON FOR PERMISSION

The development hereby permitted has been assessed against the NPPF and Development Management Plan policies DES1, DES2, DES4, DES5, DES6, DES8, DES9, NHE3, TAP1, CCF1, INF1 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

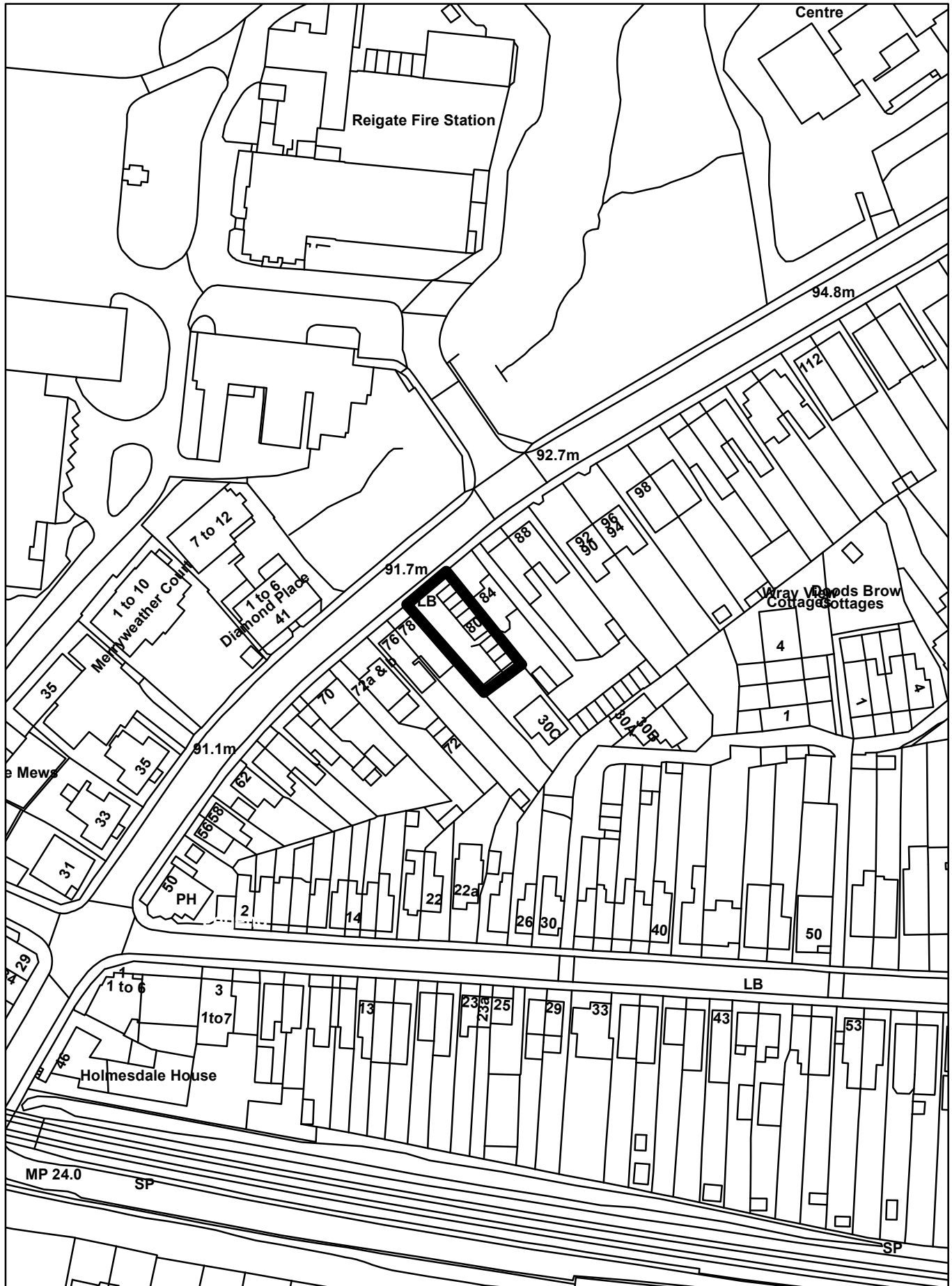
Agenda Item 12

Planning Committee
6th July 2022

Agenda Item: 12
22/00557/F

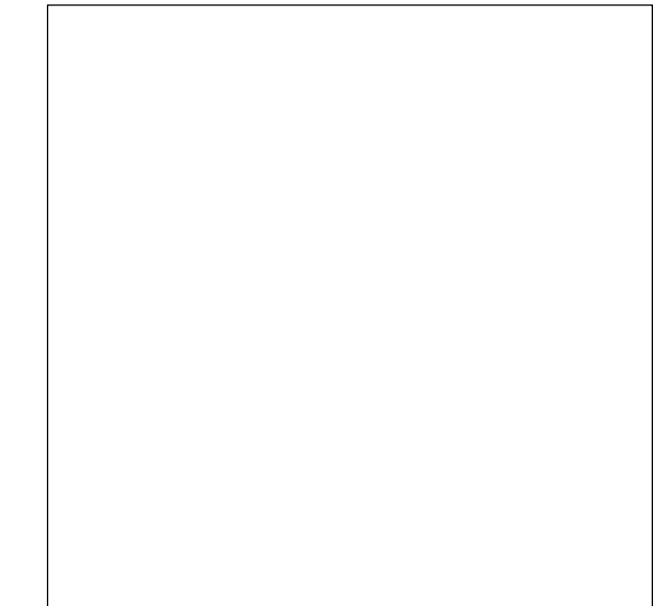
Proactive and Positive Statements

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.



SCALE @ A1: 1:50
SCALE @ A3: 1:100

A1
A3



NOTES

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REV.	DATE	NOTES
D1	15/02/2022	Draft Issue to Energy Assessor
D2	25/02/2022	Draft Issue to Client.
P1	04/03/2022	Planning Issue

PROJECT
80 Croydon Road, Reigate

CLIENT
A & D Lippett

SAWKINGERS
architects

SawKings Harper Architects
1 The Quarry,
Betchworth,
Surrey RH3 7BY

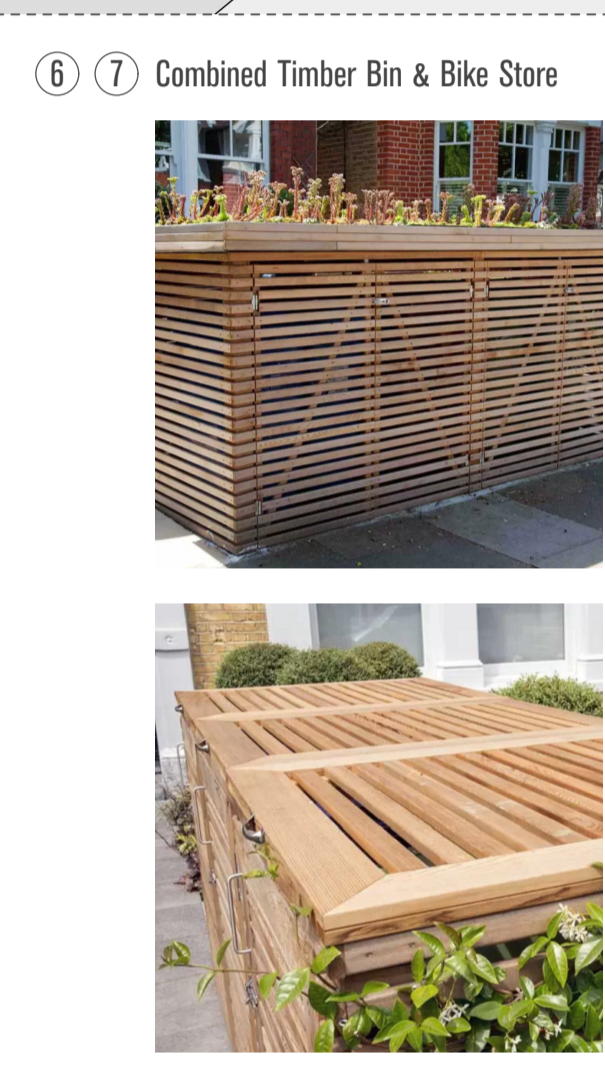
T 01737 845580
E admin@sawkingsharper.com
W www.sawkingsharper.com

RIBA Chartered Practice

STATUS	DRAWN BY	CHECKED BY
PLANNING	PT	-
JOB NO.	DRAWING NO.	REV
A4260	2000	P1
DRAWING TITLE		
Proposed Site Plan		



- ① New Hardstanding Finish: Paving
- ② Parking Spaces; 1No. per unit
- ③ Royal Mail Post Box Relocated
- ④ Existing Timber Fence; to be repaired and replaced as required
- ⑤ New 1.8m Closed Board Timber Fence
- ⑥ Timber Refuse Store (per unit):
1 x 140l Waste Bin
1 x 140l Mixed Recycling Bin
1 x 23l Food Bin
1 x 55l Paper & Card Recycling Box
- ⑦ Timber Cycle Store; 2No. spaces per unit
- ⑧ New Soft Landscaping
- ⑨ Shrub Planters
- ⑩ Obscure Glazed Window



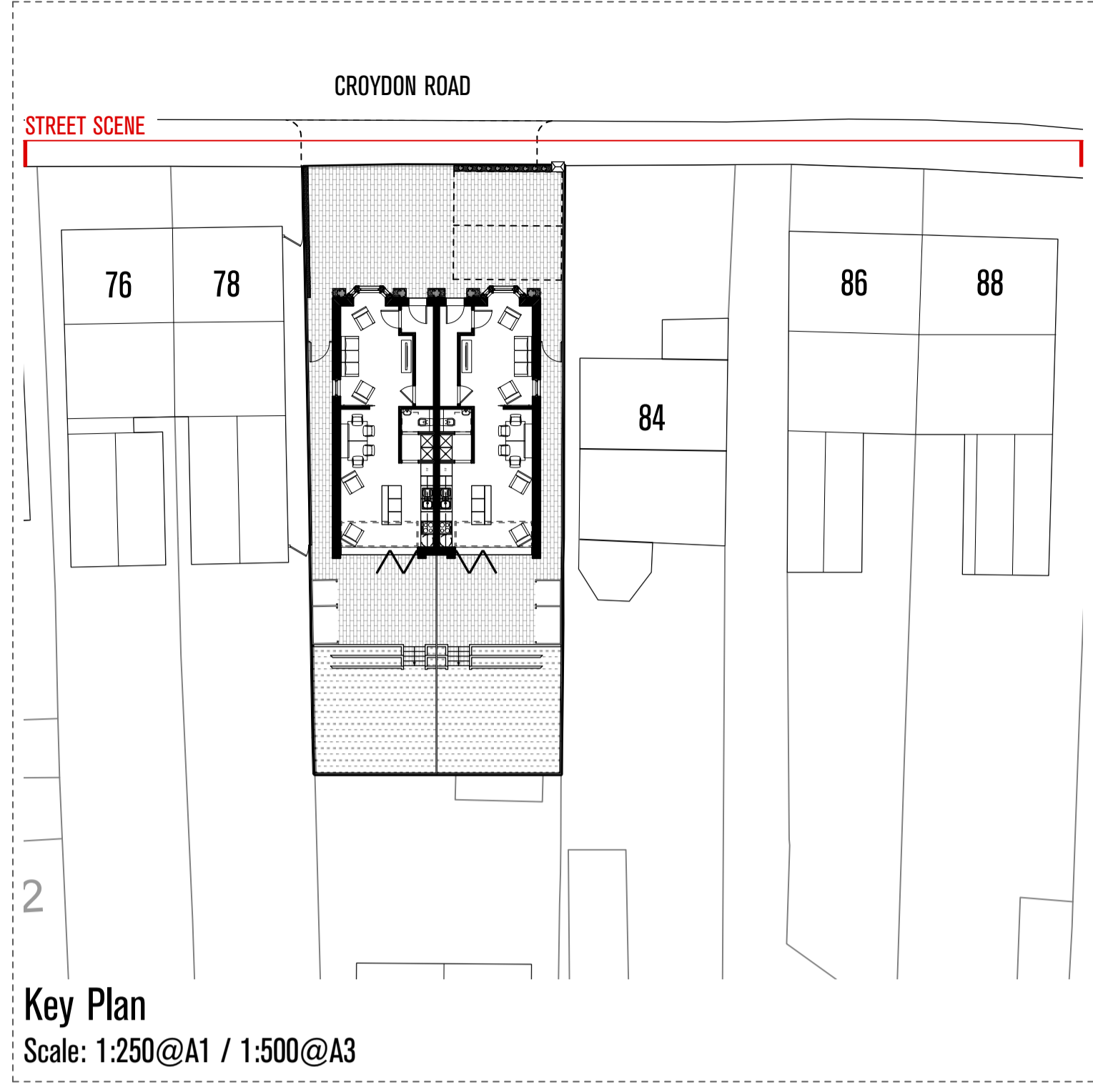
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Full Street Scene

Scale: 1:100 @ A1
1:200 @ A3



Key Plan

Scale: 1:250@A1 / 1:500@A3

SCALE @ A1: ON SHEET	A1 A3
SCALE @ A3: ON SHEET	

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REV.	DATE	NOTES
D1	25/02/2022	Draft Issue to Client.
P1	04/03/2022	Planning Issue
P2	26/04/2022	Drawing amended to Planning Officer's comments.

PROJECT
80 Croydon Road, Reigate

CLIENT
A & D Lippett

SAWKINGS HARPER
 architects

Sawkings Harper Architects
 1 The Quarry,
 Betchworth,
 Surrey RH3 7BY

T 01737 845580
 E admin@sawkingsharper.com
 W www.sawkingsharper.com

RIBA Chartered Practice

STATUS PLANNING	DRAWN BY PT	CHECKED BY -
JOB NO. A4260	DRAWING NO. 2205	REV P2
DRAWING TITLE Proposed Street Scene		



80 Croydon Road Street Scene (Close-Up)

Scale: 1:50 @ A1
1:100 @ A3

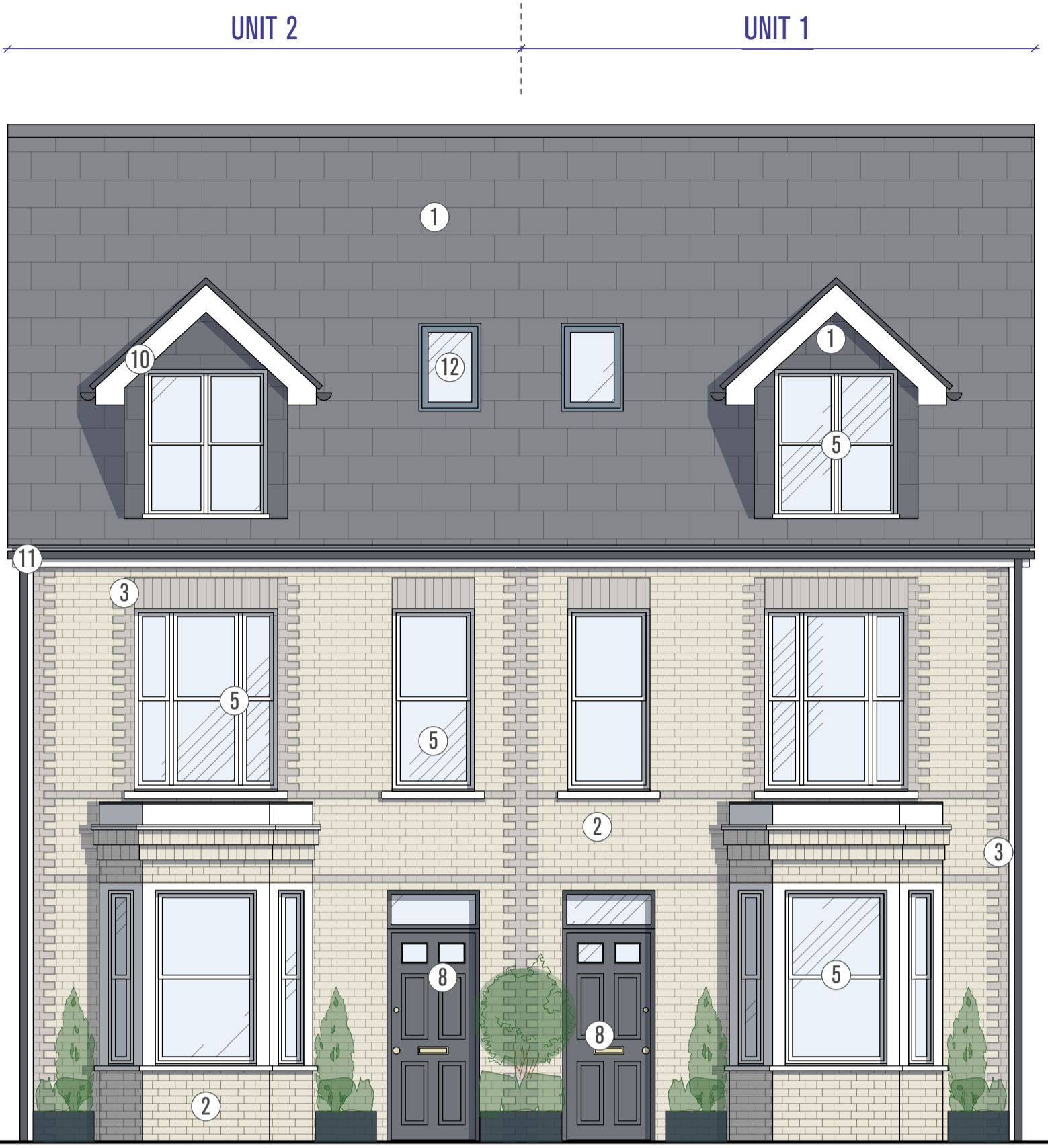
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- ① Slate Roof Tiles
- ② Brickwork Flemish Bond & Soldier Course, Yellow Blend Facing Brick
- ③ Traditional Brick Quoining / Details: Beige Grey Blend
- ④ Soldier Course Brickwork, Stacked Bond
- ⑤ Double-Glazed Traditional Sash Windows, White
- ⑥ Double-Glazed Windows, Anthracite Grey
- ⑦ Double-Glazed Windows, Anthracite Grey, Obscure Glazing
- ⑧ Timber Effect Door
- ⑨ Double-Glazed Sliding-Folding Doors, Anthracite Grey
- ⑩ White Painted Fascias, Soffits and Bargeboards
- ⑪ Black Rainwater Gutters and Downpipes
- ⑫ Anthracite Grey Skylights
- ⑬ Proposed PV Location

SCALE @ A1: 1:50
SCALE @ A3: 1:100

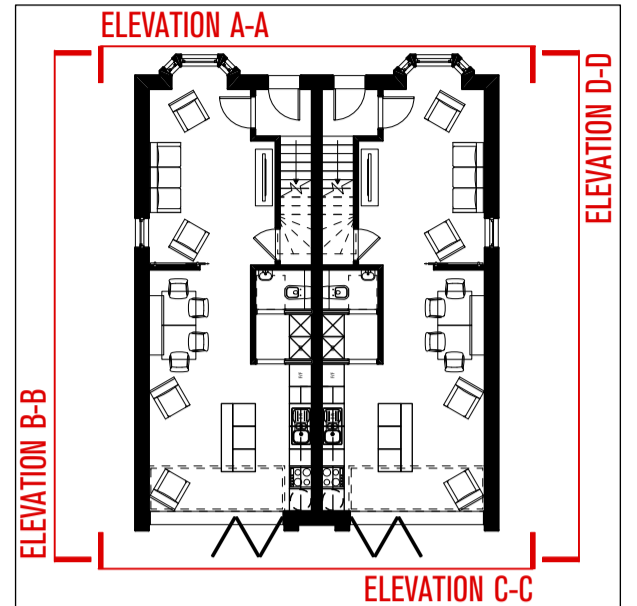
A1
A3



Elevation A-A (Front; 80 Croydon Road)



Elevation B-B (Side; Unit 1)



NOTES

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D2	25/02/2022	Draft Issue to Client.
P1	04/03/2022	Planning Issue
P2	26/04/2022	Drawing amended to Planning Officer's comments.

PROJECT
80 Croydon Road, Reigate

CLIENT
A & D Lippett

SAWKINGS HARPER architects

Sawkins Harper Architects
1 The Quarry,
Betchworth,
Surrey RH3 7BY

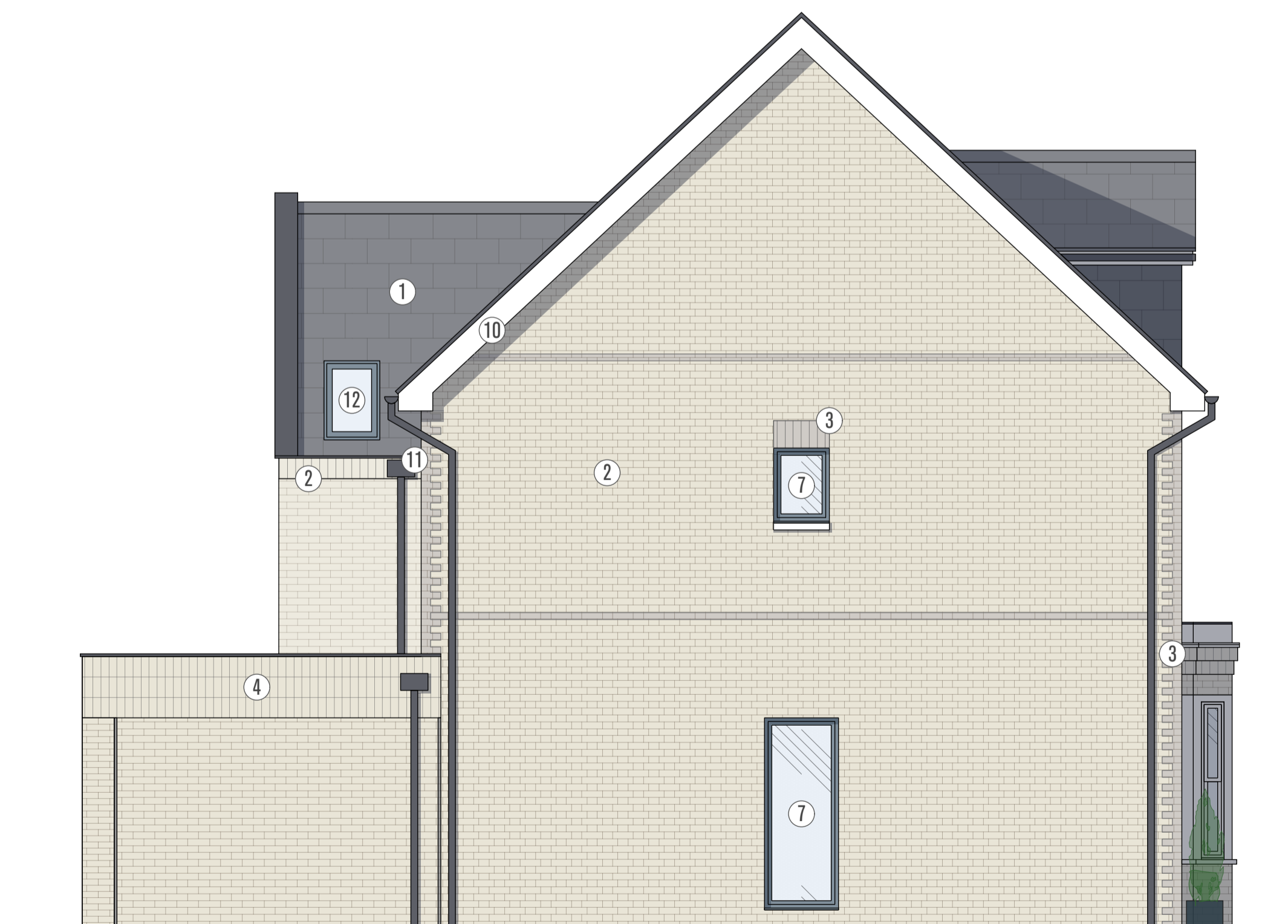
T 01737 845580
E admin@sawkingsharper.com
W www.sawkingsharper.com

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STATUS	DRAWN BY	CHECKED BY
PLANNING	PT	-
JOB NO.	DRAWING NO.	REV
A4260	2200	P2
DRAWING TITLE		
Proposed GA Elevations		



Elevation C-C (Rear)



Elevation D-D (Side; Unit 2)

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